Application for Planning Approval

Land Use Planning and Approvals Act 1993

APPLICATION NO.

SA2024/011

LOCATION OF AFFECTED AREA

80 POSSUM ROAD, BRIDGEWATER

DESCRIPTION OF DEVELOPMENT PROPOSAL

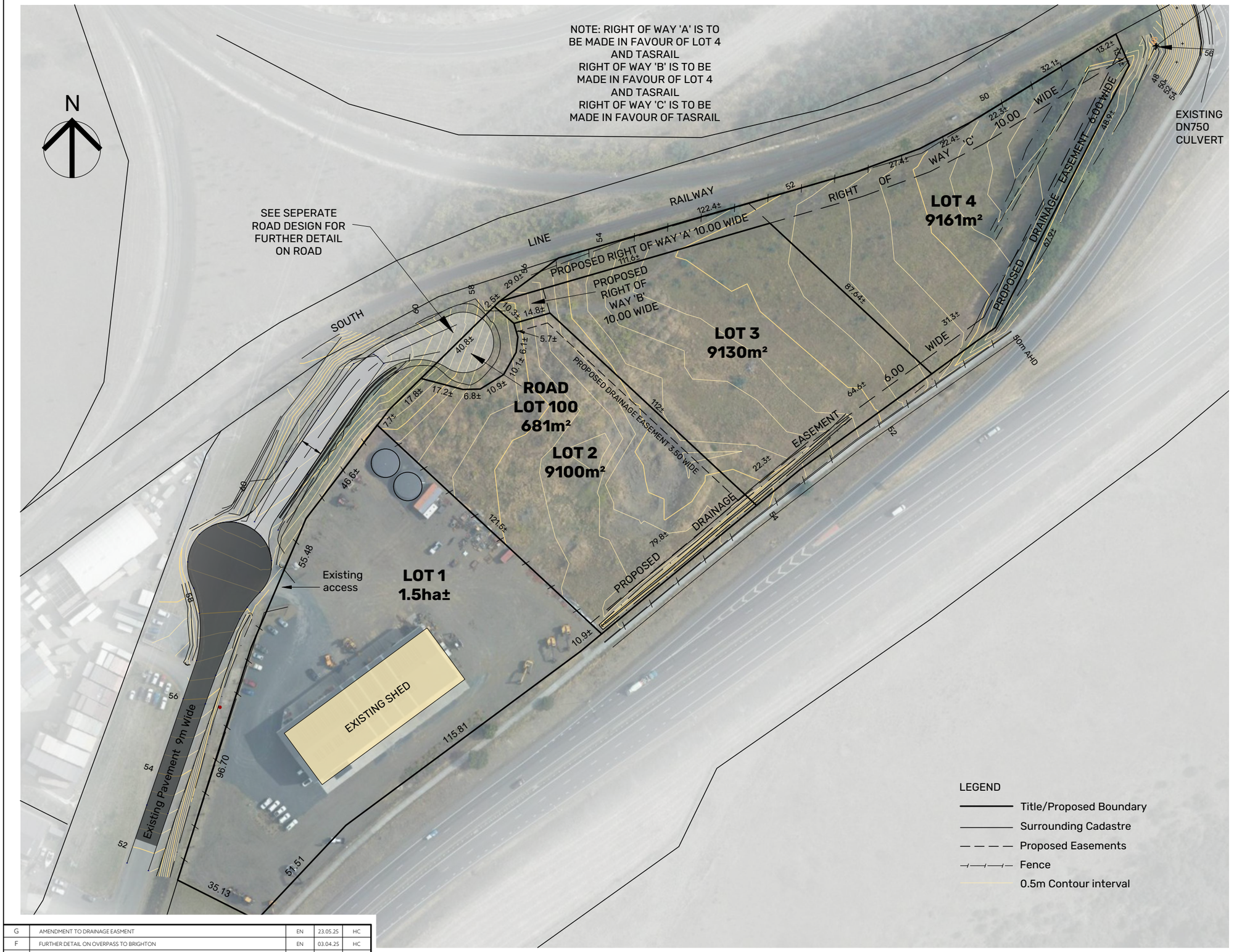
SUBDIVISION (3 LOTS PLUS BALANCE)

A COPY OF THE DEVELOPMENT APPLICATION MAY BE VIEWED AT www.brighton.tas.gov.au AND AT THE COUNCIL OFFICES, 1 TIVOLI ROAD, OLD BEACH, BETWEEN 8:15 A.M. AND 4:45 P.M, MONDAY TO FRIDAY OR VIA THE QR CODE BELOW. ANY PERSON MAY MAKE WRITTEN REPRESENTATIONS IN ACCORDANCE WITH S.57(5) OF THE LAND USE PLANNING AND APPROVALS ACT 1993 CONCERNING THIS APPLICATION UNTIL 4:45 P.M. ON **15/09/2025**. ADDRESSED TO THE CHIEF EXECUTIVE OFFICER AT 1 TIVOLI ROAD, OLD BEACH, 7017 OR BY EMAIL AT development@brighton.tas.gov.au. REPRESENTATIONS SHOULD INCLUDE A DAYTIME TELEPHONE NUMBER TO ALLOW COUNCIL OFFICERS TO DISCUSS, IF NECESSARY, ANY MATTERS RAISED.

GILLIAN BROWNE
Acting Chief Executive Officer

SCAN ME

Brighton
going places



PLAN OF SUBDIVISION

Owners
R.G. Hazell Pty. Ltd.
David Hazell Pty Ltd

Title References
FR 159938/4

Address
80 Possum Road Bridgewater

Council
Brighton Council

Tasmanian Planning scheme
Brighton Local Provisions Schedule

Zone
19.0 General Industrial

Code Overlay
9 Attenuation Code/Bridgewater Quarry
13 Bushfire-prone Area
BRI-S4.0, BRI-S10.0,

PID
3511316

Point of interest GDA2020 MGA55
59752E, 5270805N

Schedule of Easements
Proposed Rights of Way as shown.
Proposed Drainage Easement 6.00 wide

NOTES

This plan has been prepared only for the purpose of obtaining preliminary subdivision approval from the Council and the information shown hereon should be used for no other purpose.
All measurements and areas are subject to final survey.

All lots shown on plan cannot be serviced by water and sewer. On site treatment for sewer is required.

Water tanks are required for firefighting purposes.

The Site is covered in its entirety by the Codes listed above and have not been shown for clarity.

REV	AMENDMENTS	DRAWN	DATE	APPR.
G	AMENDMENT TO DRAINAGE EASMENT	EN	23.05.25	HC
F	FURTHER DETAIL ON OVERPASS TO BRIGHTON	EN	03.04.25	HC
E	CHANGED SW OUTLET DIAMETER AND REMOVED VARIOUS NOTES	EN	07.03.25	MK
D	EXTENDED DETAIL	EN	13.08.24	HC
C	ROAD LOT EXTENDED TO MATCH ROAD DESIGN	EN	01.08.24	HC
B	ROAD WIDTH INCREASED TO 11m	EN	18.07.24	CT
A	TURNING CIRCLE ADDED	EN	30.11.23	CT
0	PLAN OF SUBDIVISION	EN/MK	05.09.23	HC

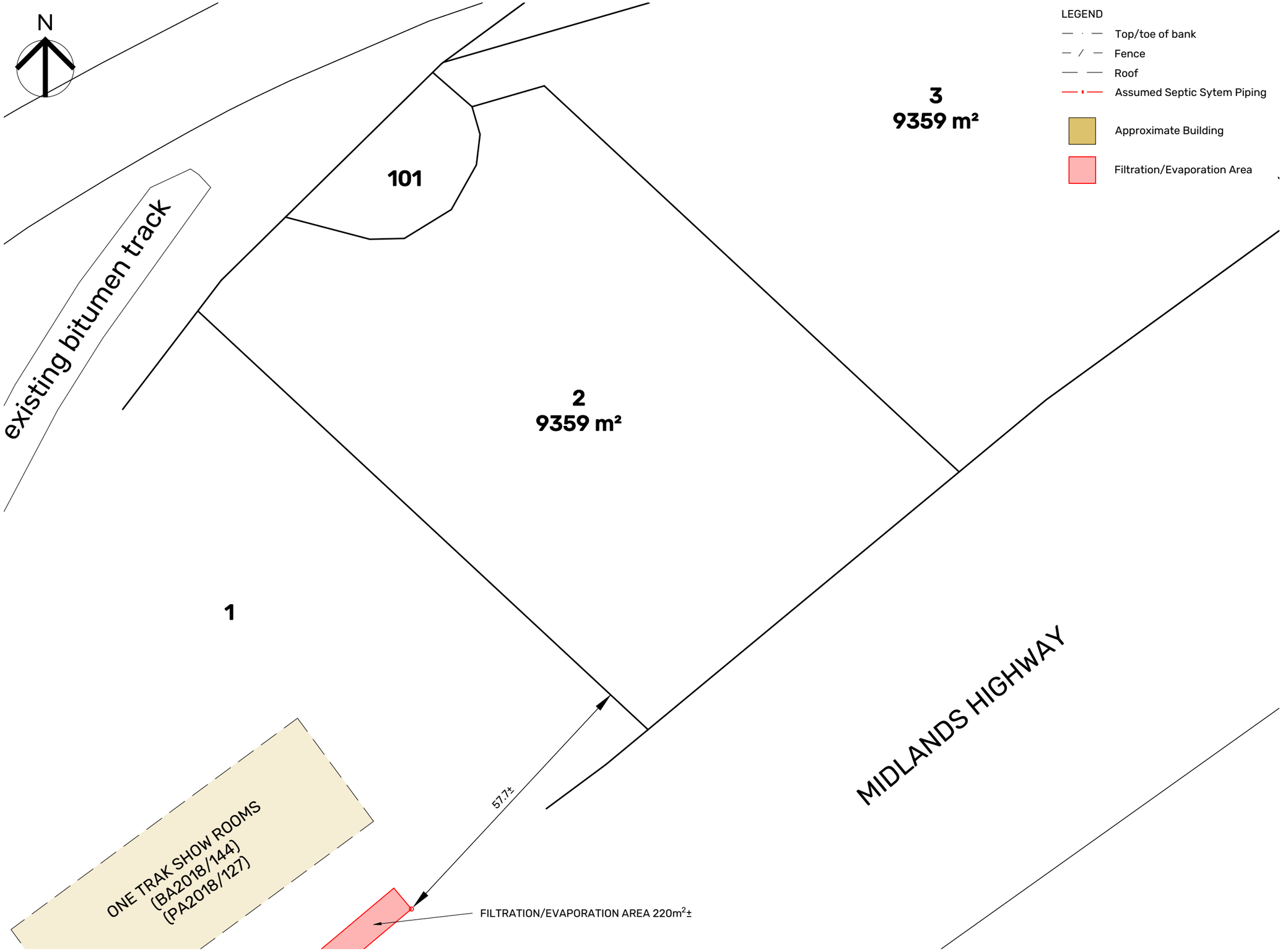
NOTES:

SURVEYOR	GEOCIVIL
EN	EN
DRAWN	CHECKED
EN/MK	HC
DATE	
23/05/2025	

PLAN OF SUBDIVISION
80 POSSUM ROAD, BRIDGEWATER
for DAVID HAZELL

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
FAX: +61 03 6234 5085
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie

SCALE	PAPER
1:1500	(A3)
JOB NUMBER	DRAWING
46981NG-1g	



LEGEND

- Top/toe of bank
- Fence
- Roof
- Assumed Septic Sytem Piping
- Approximate Building
- Filtration/Evaporation Area

NOTES

Plan is to show separation from existing septic to proposed boundary only.

Some services have been plotted from council records, and as such are approximate only.

Prior to any demolition, excavation, final design or construction on this site, a comprehensive site investigation should be undertaken to locate all above and below ground service infrastructure.

All coordinates within this file, although stated to the nearest 0.001 metre, are approximate only and are only within 0.015m of the stated coordinate (horizontally and vertically).

The boundaries shown on this plan are compiled from SP@@@@@ and, as such, are approximate only.

If any works are to be conducted on or near the boundary a re-establishment survey will be required.

Any DTM modeling that is to be done from the accompanying 3D digital file must be done using only the layer 'TRIANGLE_1 SURFACE' to ensure that surface matches that verified by PDA Surveyors, Engineers & Planners. No responsibility is taken for the use or interpretation of this data in any other format.

Some feature levels are not shown on this plan for clarity. These can be found turned on in model space or on the OFF Levels layer.

REV	AMENDMENTS	DRAWN	DATE	APPR.
E				
D				
C				
B				
A				

SCALE: 0 3.75 7.5 15 37.5m

SURVEYOR	GEOCIVIL
-	-
DRAWN	CHECKED
EN	HC
DATE	
11/08/2025	

SERVICES PLAN
80 POSSUM ROAD
BRIDGEWATER
for R.G HAZELL PTY & DAVID HAZELL PTY

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
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EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Huonville, Launceston, Swansea, Devonport & Burnie

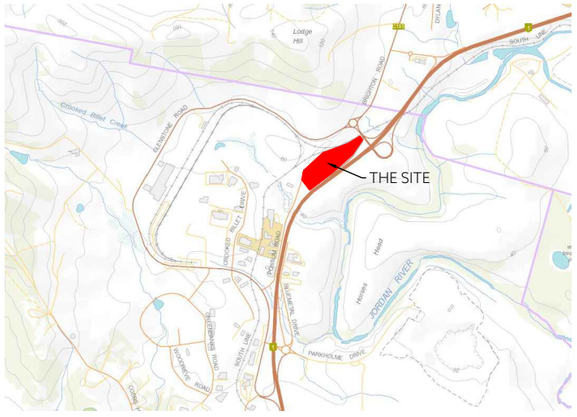
SCALE	PAPER
1:750	(A3)
JOB NUMBER	DRAWING
46981NG-	

DRAWING LIST		
DRAWING NUMBER	**DRAWING TITLE**	**REVISION**
001	COVER SHEET	A
002	PROJECT NOTES	A
003	TYPICAL DETAILS	A
100	OVERALL SITE PLAN	A
101 - 102	ROADS & STORMWATER DETAIL PLAN	A
200	ROAD 1 LONG SECTION	A
201	ROAD 1 CUL DE SAC PLAN & LONG SECTION	A
202	ROAD 1 KERB RETURN PLAN & LONG SECTION	A
210 - 212	V-DRAIN LONG SECTION	A
300 - 302	ROAD 1 CROSS SECTIONS	A
310 - 311	V-DRAIN CROSS-SECTIONS	A
400 - 403	STORMWATER LONG SECTIONS	A
700	TRUCK TURNING PATHS FOR LOT 2	A
900	STREET LIGHTING DETAIL PLAN	A

BRIGHTON COUNCIL SUBMISSIONS - SA3035/00011		
REV	**DATE ISSUED**	**DESCRIPTION**
PRELIM	14/08/2024	-
A	-	-

ONETRAK
80 POSSUM ROAD, BRIDGEWATER
4 LOT INDUSTRIAL SUBDIVISION

DIAL BEFORE YOU DIG www.1100.com.au



PROJECT STATUS						
REV	**DATE ISSUED**	**DRAWN BY**	**AMENDED BY**	**CHECKED BY**	**COUNCIL SUBMISSION**	**TASWATER SUBMISSION**
PRELIM	14/08/2024	RD	-	MW	14/08/2024	N/A
A	8/08/2025	FM	-	MW	-	-

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: COVER SHEET

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
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Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: NTS		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	001	A

DATE/TIME: Friday, 8 August 2025 2:48:45 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG

GENERAL NOTES:

1. NO ATTEMPT HAS BEEN MADE TO LOCATE ALL SERVICES. ONLY THOSE SERVICES CONSPICUOUS DURING FIELD SURVEYS ARE SHOWN. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON THE SITE, THE RELEVANT AUTHORITY(S) SHOULD BE CONTACTED FOR POSSIBLE LOCATION OF FURTHER UNDERGROUND SERVICE AND DETAILED LOCATIONS OF ALL SERVICES.

2. ALL DIMENSIONS AND LOT SIZES SUBJECT TO FINAL SURVEY.

3. REFER IPWEA - LGAT SPECIFICATIONS, TASMANIAN STANDARD DRAWINGS ISSUED - 03 DECEMBER 2020

   TSD-G01.v3 - TRENCH REINSTATEMENT FLEXIBLE PAVEMENTS
   TSD-G02.v3 - URBAN ROADS - TYPICAL SERVICE LOCATIONS
   TSD-R01.v3 - RURAL ROADS UNSEALED
   TSD-R02.v3 - RURAL ROADS SEALED
   TSD-R03.v3 - RURAL ROADS - TYPICAL DRIVEWAY ACCESS
   TSD-R04.v3 - RURAL ROADS - TYPICAL DRIVEWAY PROFILE
   TSD-R05.v3 - TRUCK ACCESS TO RURAL PROPERTIES 'TYPE A'
   TSD-R06.v3 - URBAN ROADS - TYPICAL SECTIONS & PAVEMENT WIDTHS
   TSD-R09.v3 - URBAN ROADS - DRIVEWAYS
   TSD-R11.v3 - URBAN ROADS - FOOTPATHS
   TSD-R12.v3 - SUB SOIL DRAINS - CONSTRUCTION DETAILS
   TSD-R13.v3 - SUBSOIL DRAINS PIT CONNECTION - TYPE FD
   TSD-R14.v3 - CONCRETE KERBS & CHANNELS DIMENSIONS
   TSD-R15.v3 - CONCRETE KERBS & CHANNELS CONSTRUCTION DETAILS
   TSD-R16.v3 - CONCRETE KERBS & CHANNELS VEHICULAR CROSSINGS
   TSD-R18.v3 - CONCRETE KERBS & CHANNELS ACCESS RAMPS
   TSD-R26.v3 - DELINEATORS
   TSD-R28.v3 - W-BEAM - INSTALLATION DETAILS
   TSD-SW01.v3 - PIPE INSTALLATION ANCHOR BLOCKS
   TSD-SW02.v3 - MANHOLES - 100-600 DIA. PIPES - GENERAL ARRANGEMENTS
   TSD-SW03.v3 - MANHOLES - 100-600 DIA. PIPES - BENCHING DETAILS
   TSD-SW09.v3 - SIDE ENTRY PITS - 'TYPE 3'
   TSD-SW11.v3 - SIDE ENTRY PITS - KERB TRANSITIONS
   TSD-SW15.v3 - STORMWATER - 'GP'
   TSD-SW17.v3 - OUTLET HEADWALLS 300 - 600 DIA PIPES
   TSD-SW25.v3 - STORMWATER PROPERTY CONNECTIONS TO MAINS
   TSD-SW26.v3 - SADDLE CONNECTION TO STORMWATER DRAIN
   TSD-SW27.v3 - REPAIRS/ NEW CONNECTION TO STORMWATER DRAIN
   TSD-SW28.v3 - GUIDELINES FOR SEDIMENT CONTROL
   TSD-RF04.v3 - NATURE STRIP DETAILS

4. CONSTRUCTION TO COMPLY WITH WSAA SEWERAGE CODE OF AUSTRALIA (MELBOURNE RETAIL WATER AGENCIES EDITION) - WSA 02-2014-3.1 v3 AND TASWATER SUPPLEMENT TO THE CODE.

5. ALL WORKS TO BE CONSTRUCTED IN ACCORDANCE WITH THE TASMANIAN SUBDIVISION GUIDELINES (VERSION 1.0 , DATED OCTOBER 2013) UNLESS OTHERWISE NOTED.

6. ALL CONNECTIONS TO EXISTING SEWER AND WATER MAINS TO BE CARRIED OUT BY TASWATER OR APPROVED CONTRACTOR AT DEVELOPER'S COST UNLESS APPROVED OTHERWISE.

SURVEY NOTES:

1.	DATUM:	XXXX
2.	PLANAR OR GRID:	GDA 2020 GRID
3.	SITE CONTROL:	XXXX
4.	SURVEYED BY:	E.N
5.	ACCURACY OF SURVEY:	XXXX
6.	ACCURACY/STATUS OF EXISTING & FUTURE PROPERTY BOUNDARIES:	XXXX

ROADS & STORMWATER NOTES:

1. ALL STORMWATER LOT CONNECTIONS TO BE DN150 UNLESS OTHERWISE NOTED.
2. ALL STORMWATER LINES DN300 PVC-U SN8 OR EQUIVALENT UNLESS NOTED OTHERWISE.
3. ALL FOOTPATHS TO BE N25 CONCRETE 100mm THICK, SL72 CENTRAL, 100 x 20 FCR BASE.
4. 2 x 65 DIA WEEPHOLES TO BE PLACED IN ALL STORMWATER SIDE ENTRY PITS AND MANHOLES WITH NO SUBSOIL DRAIN CONNECTIONS.
5. ALL STORMWATER BRANCHES TO TERMINATE AT PROPERTY BOUNDARIES WITH AN INSPECTION OPENING RAISED TO SURFACE WITH AN APPROVED COVER 1m INSIDE PROPERTY BOUNDARY.
6. ALL LOT CONNECTIONS TO BE CONSTRUCTED IN THE POSITION SHOWN UNLESS APPROVED BY THE SUPERINTENDENT.
7. FINAL POSITION AND WIDTH OF ALL DRAINAGE EASEMENTS IS TO BE DETERMINED FOLLOWING CONSTRUCTION.
8. EXTENT OF ROADWORKS SHOWN SHADED.
9. ALL FILL AREAS TO BE PLACED AND COMPACTED PRIOR TO TRENCH EXCAVATION.
10. PROVIDE TRAFFICABLE LIDS TO CONNECTIONS IN TRAFFICABLE AREAS INCLUDING DRIVEWAYS.
11. ALL DRIVEWAYS TO HAVE MAXIMUM GRADE 20% OUTSIDE LOT BOUNDARIES AND 25% INSIDE LOT BOUNDARIES AND CONSTRUCTED AS PER TSD-R09 V3.
12. PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF ROD EYES TO SURFACE COVER LID.

STORMWATER LONG SECTION NOTES:

1. ALL EXCAVATION OVER 1.5m IN DEPTH MUST BE CARRIED OUT IN ACCORDANCE WITH WORKPLACE SAFETY STANDARDS CODE OF PRACTICE FOR EXCAVATION WORK.
2. COMPACTED FCR BACKFILL UNDER ROADS & DRIVEWAYS.
3. ALL ANCHOR BLOCKS (CONCRETE BULKHEADS) ARE TO BE KEYED INTO UNDISTURBED, COMPETENT MATERIAL TO ENSURE THE MIGRATION OF BEDDING AND BACKFILL MATERIAL IS REDUCED AND THE INTEGRITY OF THE PIPE IS MAINTAINED.
4. TRENCHES >10% ARE TO HAVE CONTINUOUS DRAINAGE PATH INSTALLED TO ENSURE WATER THAT HAS MIGRATED INTO THE TRENCH IS CARRIED TO THE RETICULATED SYSTEM. REFER TSD-SW01.
5. ALL FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
6. FILL IN EXCESS OF 300mm DEPTH TO BE COMPACTED TO 95% STANDARD DENSITY (AS 1289E4.01) IN 150mm MAX LAYERS.
7. PROVIDE MINIMUM 150mm CLEARANCE FROM TOP OF ROD EYES TO SURFACE COVER LID.

NATURE STRIP NOTES:

1. APPLY 100mm THICK 20mm SCREENED LOAM TO NATURE STRIP & APPLY GRASS SEED AS PER TSD-RF04-v3.
2. NATURE STRIPS TO BE WATERED & MAINTAINED INCLUDING WEED REMOVAL & MOWING THROUGHOUT THE MAINTENANCE PERIOD.
3. FOR FILL BATTERS, BENCH AS SHOWN AND PLACE APPROVED FILL IN 300mm LAYERS COMPACTED TO 95% MAX DRY DENSITY. FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
4. MATERIAL TO BE PLACED AND COMPACTED UNIFORMLY IN LAYERS ACROSS THE EMBANKMENT NO GREATER THAN 300mm TO ACHIEVE 95% MDD. FOR FILL BATTERS, BENCH AS SHOWN.

ENGINEERED FILL NOTES:

1. FILL MATERIAL FOR NEW ROAD AND FILL EMBANKMENTS MUST NOT CONTAIN ORGANIC OR OTHER MATERIALS THAT DECOMPOSE OR OTHERWISE LEAD TO LONG TERM SETTLEMENT AND TO BE APPROVED BY SUPERINTENDENT BEFORE USE.
2. MATERIAL TO BE PLACED AND COMPACTED UNIFORMLY IN LAYERS UNDER NEW ROAD NO GREATER THAN 150mm TO ACHIEVE 98% MDD AND ACROSS ROAD EMBANKMENT NO GREATER THAN 300mm TO ACHIEVE 95% MDD. FOR FILL BATTERS, BENCH AS SHOWN.
3. ROAD EMBANKMENT TO BE FULLY CONSTRUCTED PRIOR TO TRENCHING FOR WATER AND SEWER SERVICES.
4. FILL TO BE PLACED & COMPACTED PRIOR TO TRENCH EXCAVATION.
5. SITE FILLING THAT EXCEEDS A DEPTH OF 300mm MUST BE PLACED IN ACCORDANCE WITH THE AS3798 GUIDELINES FOR COMMERCIAL AND RESIDENTIAL DEVELOPMENTS 1996. UPON COMPLETION OF THE WORKS THE CLIENTS GEOTECHNICAL ENGINEER MUST CONFIRM IN WRITING THAT THE WORKS HAVE BEEN CARRIED OUT IN ACCORDANCE WITH AS3798.

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED: RD | REVIEWED: MW
DRAWN: RD | REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: GENERAL NOTES
-

PDA
SURVEYORS, ENGINEERS & PLANNERS
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PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
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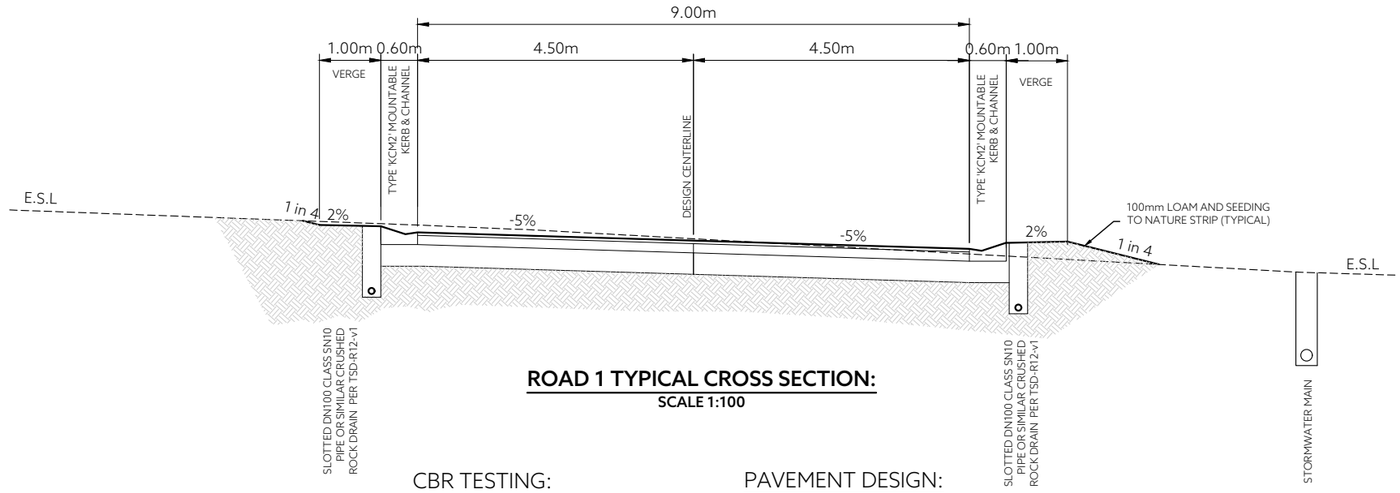
Also at: Kingston, Launceston & Burnie
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: NTS		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	002	A

DATE/TIME: Friday, 8 August 2025 2:48:46 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



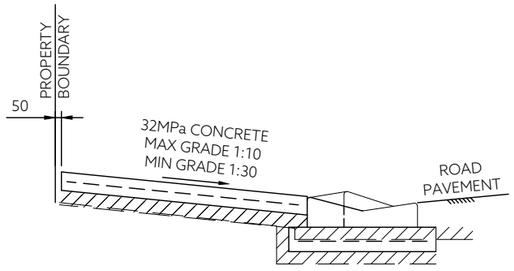
ROAD 1 TYPICAL CROSS SECTION:
SCALE 1:100

CBR TESTING:
SOAKED CBR TEST RESULTS TAKEN AT SUBGRADE LEVEL WILL BE REQUIRED TO BE SUBMITTED TO COUNCIL TO CONFIRM PAVEMENT DESIGN PRIOR TO SUBGRADE APPROVAL BEING PROVIDED

PAVEMENT DESIGN:
50 THICK HOTMIX SEAL
BASE - 150mm THICK 20 FCR CLASS 2
SUBBASE - 350mm THICK 40 FCR CLASS 3
ON APPROVED SUBGRADE

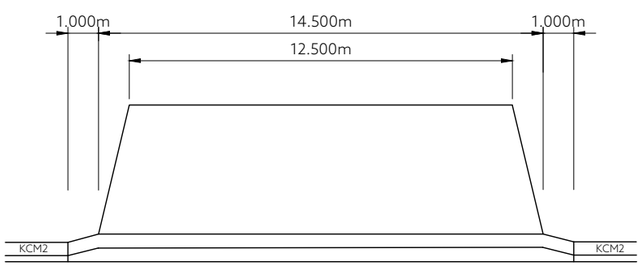
PAVEMENT THICKNESS IS BASED ON AUSTROADS GUIDE TO PAVEMENT TECHNOLOGY PART 2: FIGURE 8.4

DESIGN SUBGRADE CBR - 5
DESIGN ESA - 1×10^7

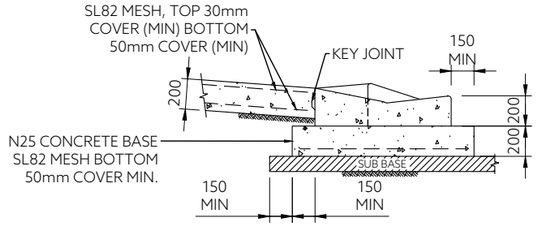


NOTE:
1. T.O.K. DENOTES TOP OF KERB
2. WHERE THERE ARE EXPANSIVE SOILS AN ADDITIONAL LAYER OF REINFORCEMENT MAY BE REQUIRED AT 60mm COVER FROM THE BOTTOM OF THE SLAB.
3. FOR STEEP TERRAIN CONTACT THE COUNCIL FOR GUIDANCE.

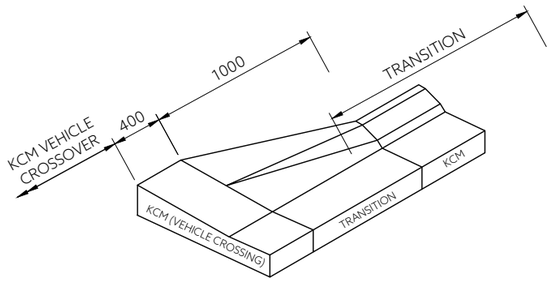
TYPICAL INDUSTRIAL DRIVEWAY CROSS SECTION:
SCALE 1:20
SEE TSD-R16-V3



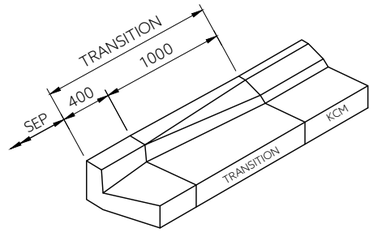
TYPICAL INDUSTRIAL DRIVEWAY DETAIL:
SCALE 1:50



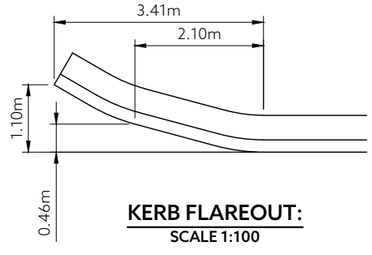
TYPE KCRB & (HEAVY VEHICLES) EXTRUDED ON REINFORCED BASE:
NOT TO SCALE
(TYPES KCS & KCM SIMILAR)



MOUNTABLE KERB TRANSITION AT DRIVEWAY CROSSOVER:
NOT TO SCALE



MOUNTABLE KERB TRANSITION AT SIDE ENTRY PITS:
NOT TO SCALE



KERB FLAREOUT:
SCALE 1:100

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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW
REV	AMENDMENTS	DRAWN	DATE	APPR.

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
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JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: TYPICAL DETAILS
-

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CONTRACT NO.	SCALE		PAPER
------	AS SHOWN (A3)		
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	003	A

N

LEGEND:

NEW STORMWATER
EX. FENCE
TOP/TOE BANK
ASPHALT
CONCRETE DRIVEWAY

NOTES:

FOR PROJECT NOTES SEE DRG 46891NG-002

STORMWATER OUTFALL TO DISCHARGE TO EXISTING HEADWALL

SOUTH RAIL LINE

SOUTH RAIL LINE

RIGHT O F WAY 10.00 WIDE

RIGHT O F WAY 10.00 WIDE

LOT 4

LOT 3

LOT 2

LOT 1

ROAD 1

POSSUM ROAD

MIDLANDS HIGHWAY

FR156586/2

FR164750/1

FR6195/2

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JOB MANAGER: CRAIG TERRY	
ISSUED DATE: 25/07/2024	

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: OVERALL SITE PLAN

-

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 2000		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	100	A

DATE/TIME: Friday, 8 August 2025 2:48:46 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG

N

SOUTH RAIL LINE

RIGHT O F WAY 10.00 WIDE

RIGHT O F WAY 10.00 WIDE

12.50

R0.46

R0.62

7.40

SW1/6

SW1/7

LOT 3

3m WIDE GRASS LINED V-DRAIN FOR OVERLAND FLOWS. SEE SHEET 210-212 & 310-311 FOR DETAILS

FOR CUL DE SAC DETAILS SEE DRG 46891NG-201

120

110

100

90

12.50

ROAD 1

9.00

40

80

20

10

0

LOT 2

FOR CONTINUATION SEE SHEET 46981NG-102

FR156586/2

EXISTING WATER TANKS

FOR KERB RETURN DETAILS SEE DRG 46891NG-202

SW3/2

LOT 1

MATCH INTO EXISTING EDGE OF BITUMEN OF POSSUM ROAD CUL DE SAC

EXISTING POSSUM ROAD CUL DE SAC

SW3/1

EXISTING STORMWATER HEADWALL EX IL 58.343

TRANSITION INVERT LEVEL TO EXISTING OPEN DRAIN INVERT LEVEL WITH KERB FLAREOUT DETAIL SEE SHEET 46981NG-003

CONSTRUCT ACCESS FOR LOT 1 AS PER BUILDING PERMIT

LEGEND:

- NEW STORMWATER
- EX. FENCE
- TOP/TOE BANK
- ASPHALT
- CONCRETE DRIVEWAY

NOTES:

1. FOR PROJECT NOTES SEE DRG 46891NG-002
2. FOR SW LONG SECTION SEE DRG 46891NG-400-403

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: ROAD & STORMWATER DETAIL PLAN SHEET 1 OF 2

PDA SURVEYORS, ENGINEERS & PLANNERS

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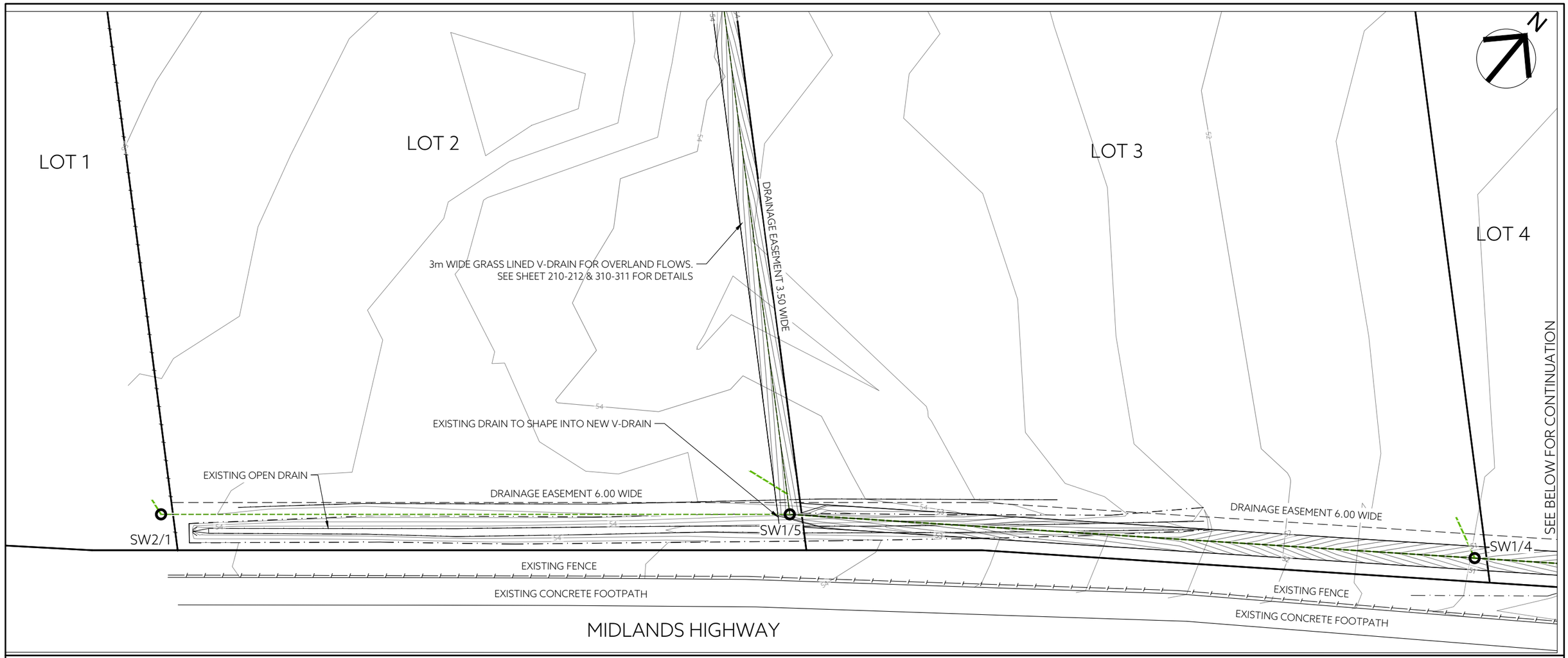
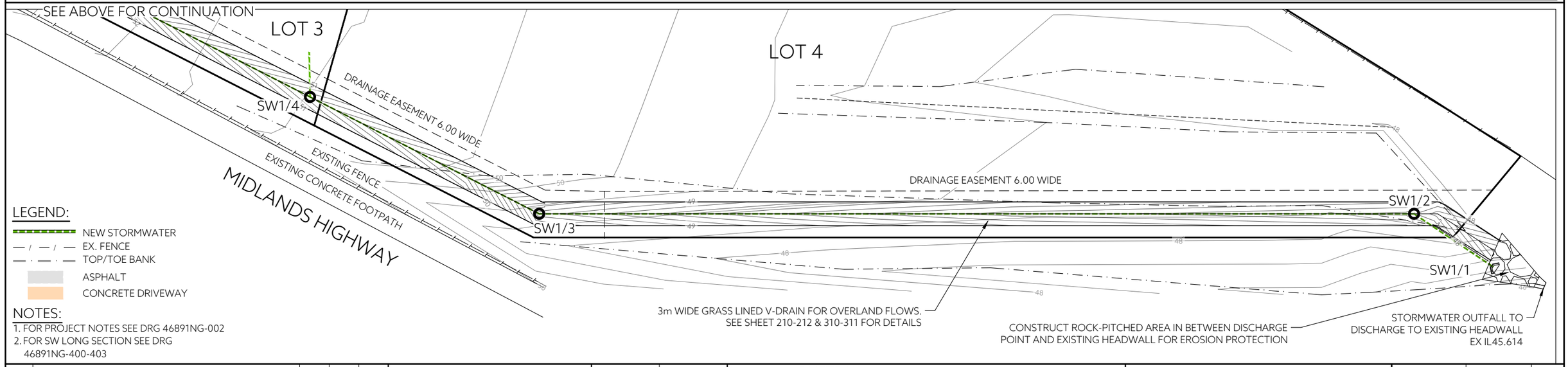
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	101	A

DATE/TIME: Friday, 8 August 2025 2:48:47 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG





LEGEND:

- NEW STORMWATER
- EX. FENCE
- TOP/TOE BANK
- ASPHALT
- CONCRETE DRIVEWAY

NOTES:

1. FOR PROJECT NOTES SEE DRG 46891NG-002
2. FOR SW LONG SECTION SEE DRG 46891NG-400-403

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

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COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	RD	REVIEWED:	MW
DRAWN:	RD	REVIEWED:	MW
JOB MANAGER:	CRAIG TERRY		
ISSUED DATE:	25/07/2024		

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: ROAD & STORMWATER DETAIL PLAN
SHEET 2 OF 2

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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	102	A

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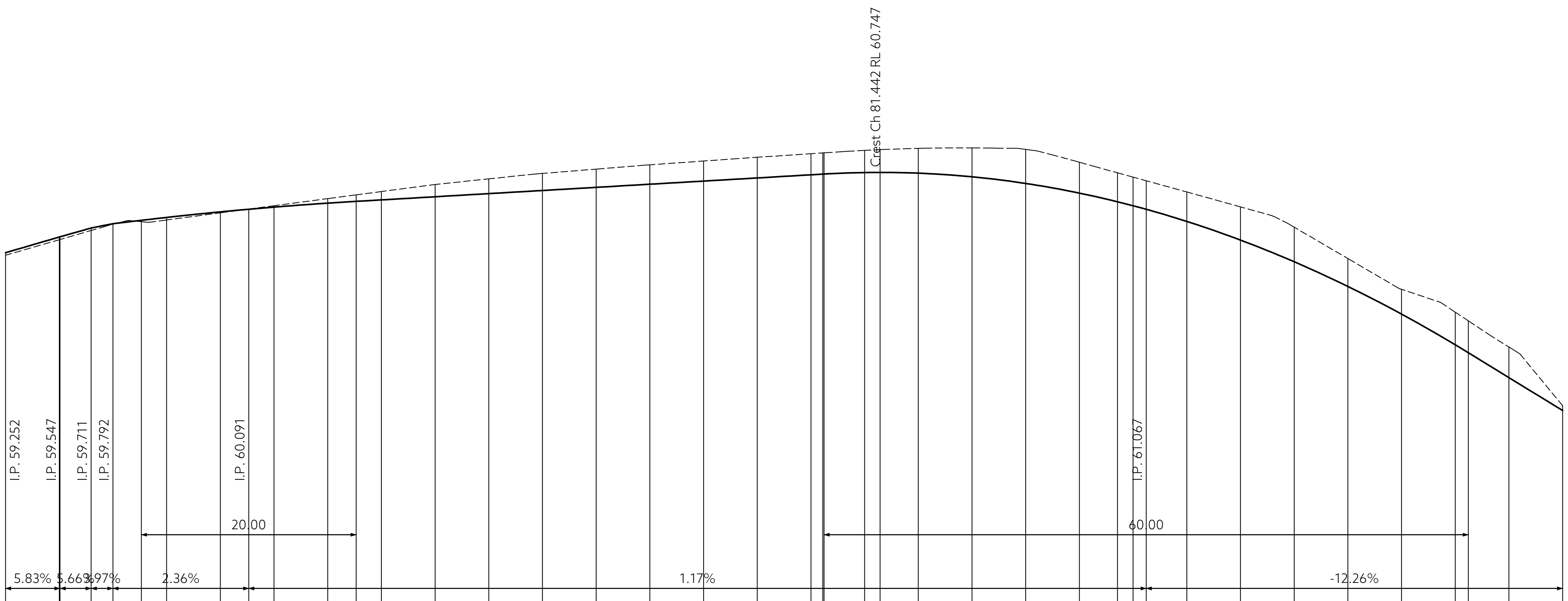

CHAINAGE	EXISTING SURFACE	DESIGN ROAD CENTRELINE	CUT / FILL
0.000	59.205	59.252	+0.047
5.000	59.494	59.543	+0.049
5.072	59.498	59.547	+0.049
7.971	59.665	59.711	+0.046
10.000	59.786	59.792	+0.006
12.649	59.829	59.854	+0.025
15.000	59.861	59.908	+0.047
20.000	59.994	60.012	+0.018
22.649	60.064	60.061	-0.003
25.000	60.127	60.101	-0.026
30.000	60.259	60.175	-0.084
32.649	60.329	60.208	-0.121
35.000	60.391	60.235	-0.156
40.000	60.522	60.293	-0.229
45.000	60.627	60.352	-0.275
50.000	60.728	60.410	-0.318
55.000	60.808	60.469	-0.339
60.000	60.888	60.527	-0.361
65.000	60.959	60.585	-0.374
70.000	61.029	60.644	-0.385
75.000	61.096	60.702	-0.394
76.107	61.111	60.715	-0.396
76.224	61.112	60.716	-0.396
80.000	61.158	60.745	-0.413
81.442	61.171	60.747	-0.424
85.000	61.195	60.733	-0.462
90.000	61.204	60.665	-0.539
95.000	61.176	60.541	-0.635
100.000	60.936	60.362	-0.574
103.548	60.739	60.200	-0.539
105.000	60.659	60.126	-0.533
106.224	60.591	60.060	-0.531
110.000	60.383	59.834	-0.549
115.000	60.105	59.487	-0.618
120.000	59.728	59.083	-0.645
125.000	59.143	58.624	-0.519
130.000	58.570	58.109	-0.461
135.000	58.145	57.537	-0.608
136.224	57.983	57.389	-0.594
140.000	57.497	56.926	-0.571
145.000	56.402	56.313	-0.089

HORIZONTAL GEOMETRY: L=76.107 B=34°34'17" · L=27.441 R=50.000 · L=41.452 B=66°00'59"

LONG SECTION -ROAD 1
SCALES: (H) 1:500 (V) 1:100 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
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DESIGNED: RD REVIEWED: MW
DRAWN: RD REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: ROAD 1 LONG SECTION
-

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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE	PAPER
------	AS SHOWN	(A3)

JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	200	A

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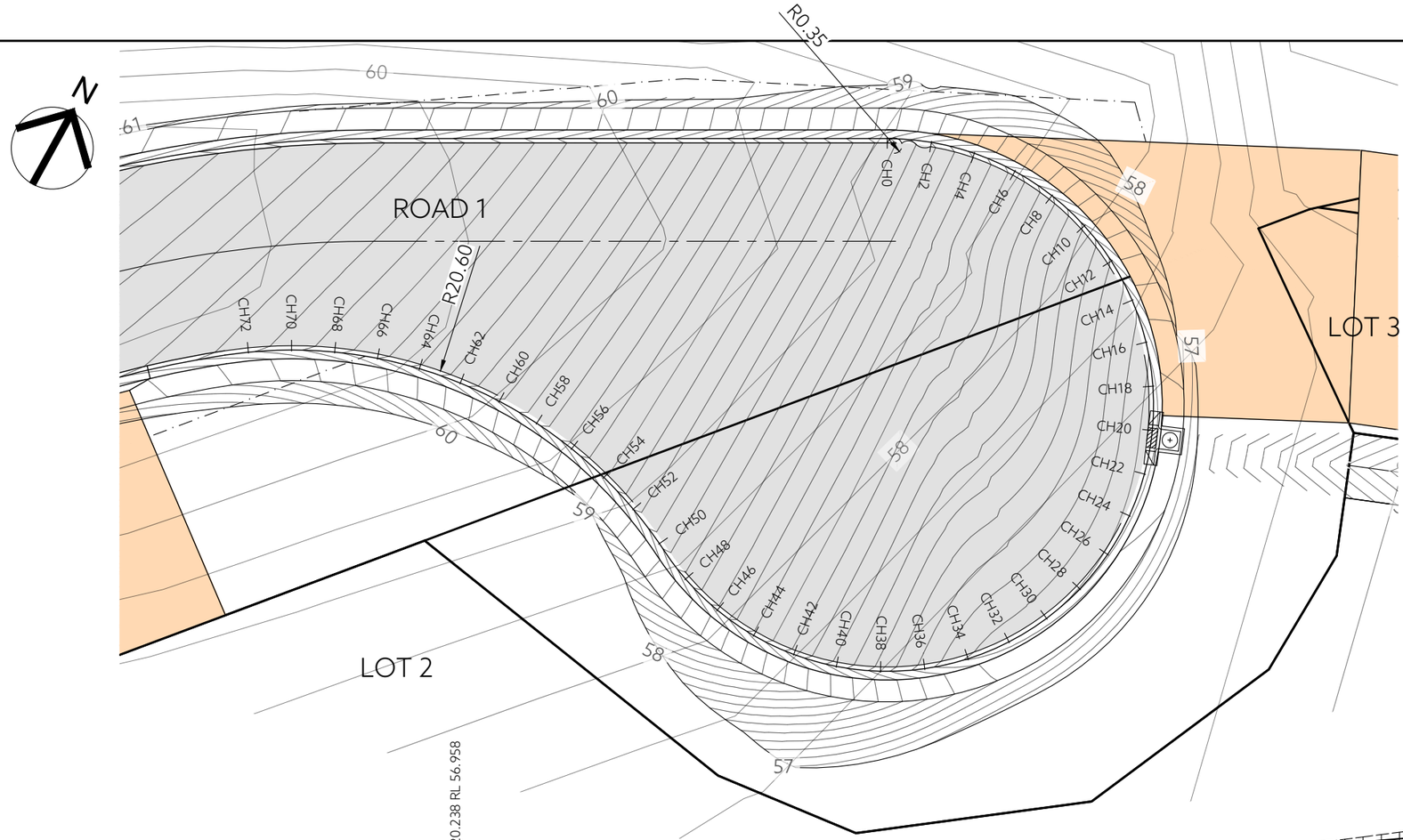
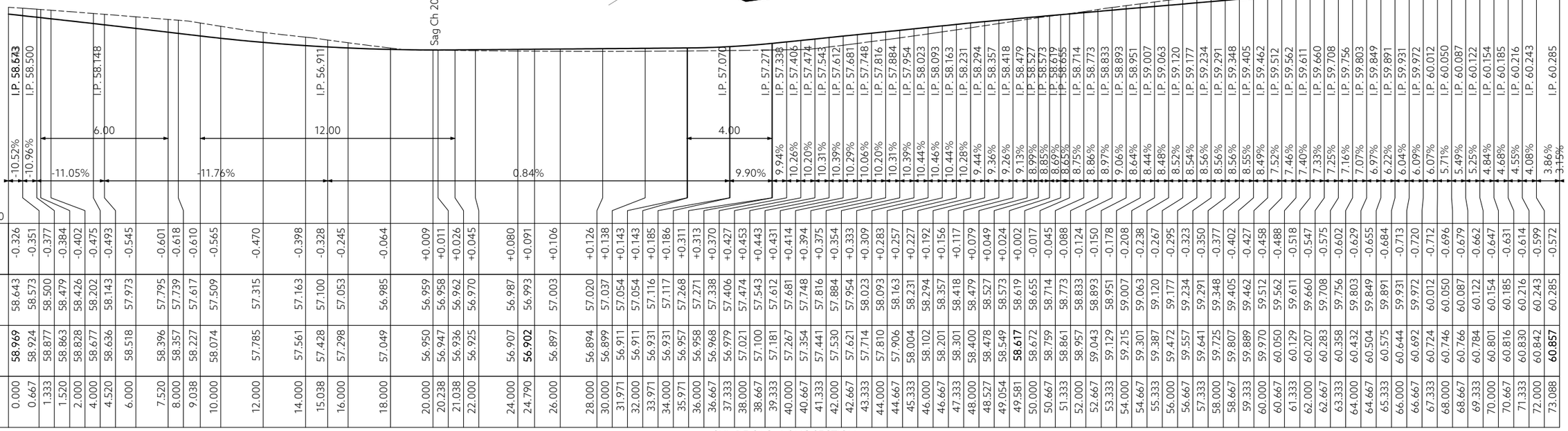


ROAD 1 CUL DE SAC DETAIL PLAN:
SCALE 1:250



R.L. 48.800

CHAINAGE	CUT / FILL	DESIGN / E.O.B.	EXISTING / SURFACE
0.000	-0.326	58.643	**58.969**
0.667	-0.351	58.573	58.924
1.333	-0.377	58.500	58.877
1.520	-0.384	58.479	58.863
2.000	-0.402	58.426	58.828
4.000	-0.475	58.202	58.677
4.520	-0.493	58.143	58.636
6.000	-0.545	57.973	58.518
7.520	-0.601	57.795	58.396
8.000	-0.618	57.739	58.357
9.038	-0.610	57.617	58.227
10.000	-0.565	57.509	58.074
12.000	-0.470	57.315	57.785
14.000	-0.398	57.163	57.561
15.038	-0.328	57.100	57.428
16.000	-0.245	57.053	57.298
18.000	-0.064	56.985	57.049
20.000	+0.009	56.959	56.950
20.238	+0.011	56.958	56.947
21.038	+0.026	56.962	56.936
22.000	+0.045	56.970	56.925
24.000	+0.080	56.987	56.907
24.790	+0.091	56.993	**56.902**
26.000	+0.106	57.003	56.897
28.000	+0.126	57.020	56.894
30.000	+0.138	57.037	56.899
31.971	+0.143	57.054	56.911
32.000	+0.143	57.054	56.911
33.971	+0.185	57.116	56.931
34.000	+0.186	57.117	56.931
35.971	+0.311	57.268	56.957
36.000	+0.313	57.271	56.958
36.667	+0.370	57.338	56.968
37.333	+0.427	57.406	56.979
38.000	+0.453	57.474	57.021
38.667	+0.443	57.543	57.100
39.333	+0.431	57.612	57.181
40.000	+0.414	57.681	57.267
40.667	+0.394	57.748	57.354
41.333	+0.375	57.816	57.441
42.000	+0.354	57.884	57.530
42.667	+0.333	57.954	57.621
43.333	+0.309	58.023	57.714
44.000	+0.283	58.093	57.810
44.667	+0.257	58.163	57.906
45.333	+0.227	58.231	58.004
46.000	+0.192	58.294	58.102
46.667	+0.156	58.357	58.201
47.333	+0.117	58.418	58.301
48.000	+0.079	58.479	58.400
48.527	+0.049	58.527	58.478
49.054	+0.024	58.573	58.549
49.581	+0.002	58.619	**58.617**
50.000	-0.017	58.655	58.672
50.667	-0.045	58.714	58.759
51.333	-0.088	58.773	58.861
52.000	-0.124	58.833	58.957
52.667	-0.150	58.893	59.043
53.333	-0.178	58.951	59.129
54.000	-0.208	59.007	59.215
54.667	-0.238	59.063	59.301
55.333	-0.267	59.120	59.387
56.000	-0.295	59.177	59.472
56.667	-0.323	59.234	59.557
57.333	-0.350	59.291	59.641
58.000	-0.377	59.348	59.725
58.667	-0.402	59.405	59.807
59.333	-0.427	59.462	59.889
60.000	-0.458	59.512	59.970
60.667	-0.488	59.562	60.050
61.333	-0.518	59.611	60.129
62.000	-0.547	59.660	60.207
62.667	-0.575	59.708	60.283
63.333	-0.602	59.756	60.358
64.000	-0.629	59.803	60.432
64.667	-0.655	59.849	60.504
65.333	-0.684	59.891	60.575
66.000	-0.713	59.931	60.644
66.667	-0.720	59.972	60.692
67.333	-0.712	60.012	60.724
68.000	-0.696	60.050	60.746
68.667	-0.679	60.087	60.766
69.333	-0.662	60.122	60.784
70.000	-0.647	60.154	60.801
70.667	-0.631	60.185	60.816
71.333	-0.614	60.216	60.830
72.000	-0.599	60.243	60.842
73.088	-0.572	60.285	**60.857**

CUL-DE-SAC - LONG SECTION
Scales: (H) 1 in 200 (V) 1 in 200 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
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DESIGNED: RD | REVIEWED: MW
DRAWN: RD | REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: ROAD 1 CUL DE SAC PLAN & LONG SECTION
-

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CONTRACT NO.	SCALE	PAPER
------	AS SHOWN	(A3)
JOB NUMBER	DISCIPLINE / SHEET / REVISION	
46981NG	C 201	A

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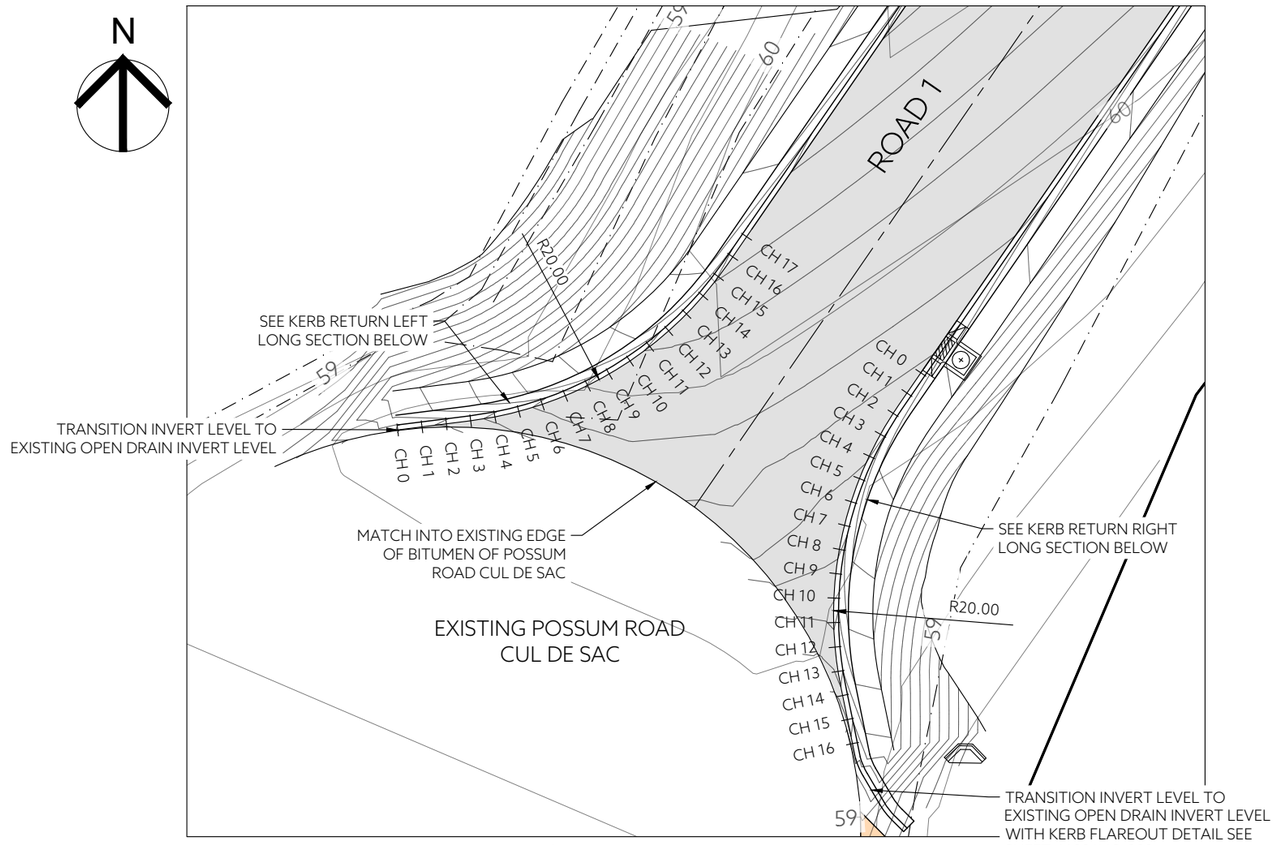
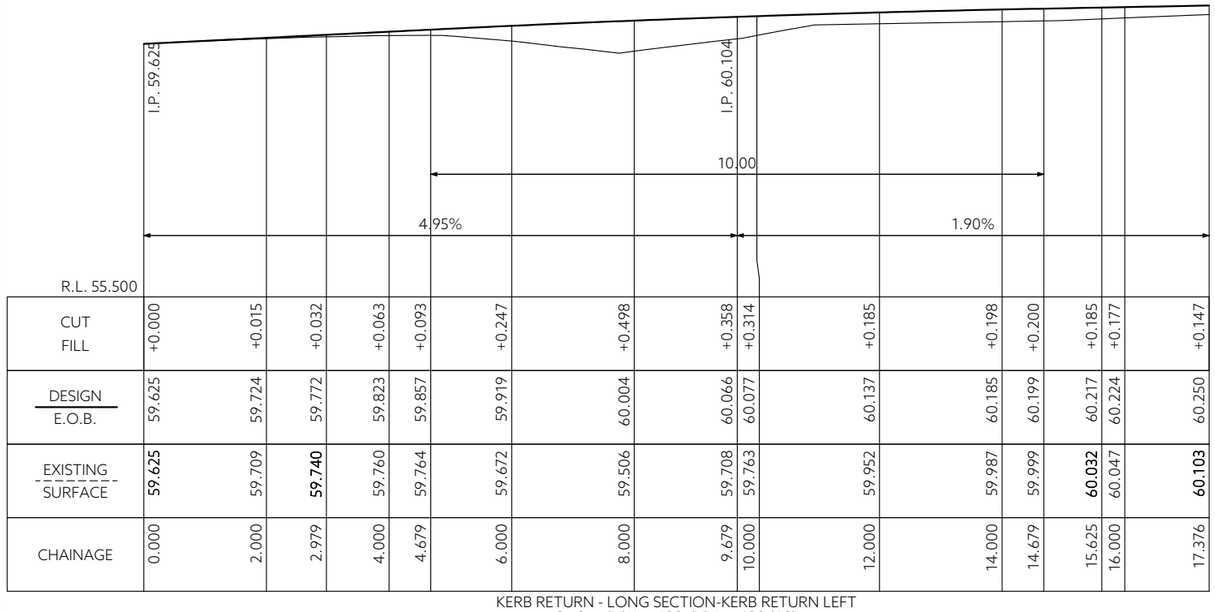


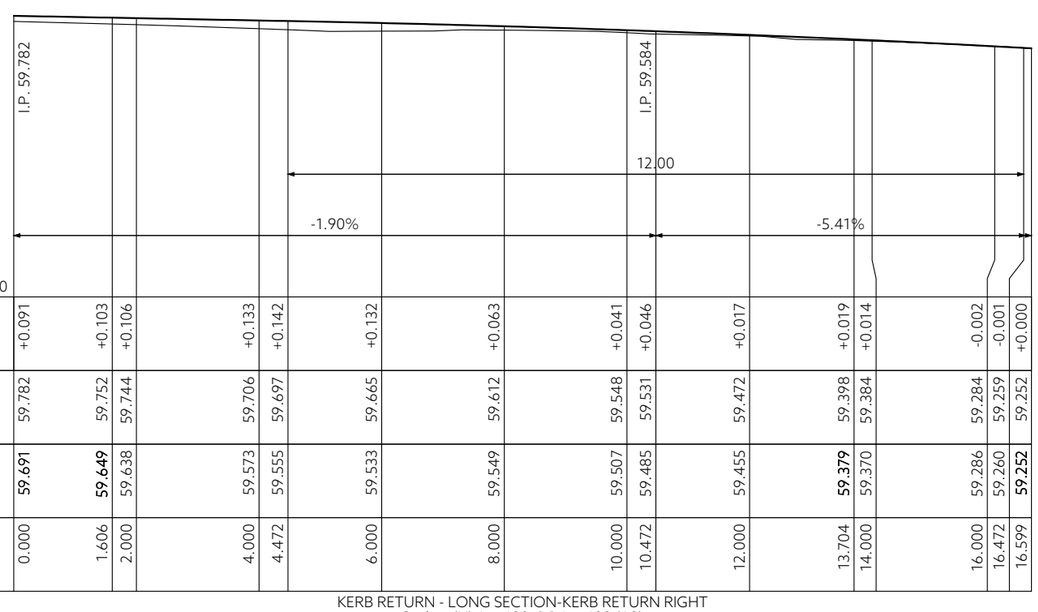
ROAD 1 KERB RETURN DETAIL PLAN:

SCALE 1:250



R.L. 55.500													
CUT / FILL	+0.000	+0.015	+0.032	+0.063	+0.093	+0.247	+0.498	+0.358	+0.314	+0.185	+0.198	+0.200	+0.185	+0.177	+0.147
DESIGN / E.O.B.	59.625	59.724	59.772	59.823	59.857	59.919	60.004	60.066	60.077	60.137	60.185	60.199	60.217	60.224	60.250
EXISTING / SURFACE	**59.625**	59.709	**59.740**	59.760	59.764	59.672	59.506	59.708	59.763	59.952	59.987	59.999	**60.032**	60.047	**60.103**
CHAINAGE	0.000	2.000	2.979	4.000	4.679	6.000	8.000	9.679	10.000	12.000	14.000	14.679	15.625	16.000	17.376

KERB RETURN - LONG SECTION-KERB RETURN LEFT
Scales: (H) 1 in 100 (V) 1 in 100 (A3)



R.L. 55.200													
CUT / FILL	+0.091	+0.103	+0.106	+0.133	+0.142	+0.132	+0.063	+0.041	+0.046	+0.017	+0.019	+0.014	-0.002	-0.001	+0.000
DESIGN / E.O.B.	59.782	59.752	59.744	59.706	59.697	59.665	59.612	59.548	59.531	59.472	59.398	59.384	59.284	59.259	59.252
EXISTING / SURFACE	**59.691**	**59.649**	59.638	59.573	59.555	59.533	59.549	59.507	59.485	59.455	**59.379**	59.370	59.286	59.260	**59.252**
CHAINAGE	0.000	1.606	2.000	4.000	4.472	6.000	8.000	10.000	10.472	12.000	13.704	14.000	16.000	16.472	16.599

KERB RETURN - LONG SECTION-KERB RETURN RIGHT
Scales: (H) 1 in 100 (V) 1 in 100 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

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DRAWN:	RD	REVIEWED:	MW
JOB MANAGER:	CRAIG TERRY		
ISSUED DATE:	25/07/2024		

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: ROAD 1 KERB RETURN PLAN & LONG SECTION
-

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CONTRACT NO.	SCALE		PAPER
------	AS SHOWN (A3)		
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	201	A

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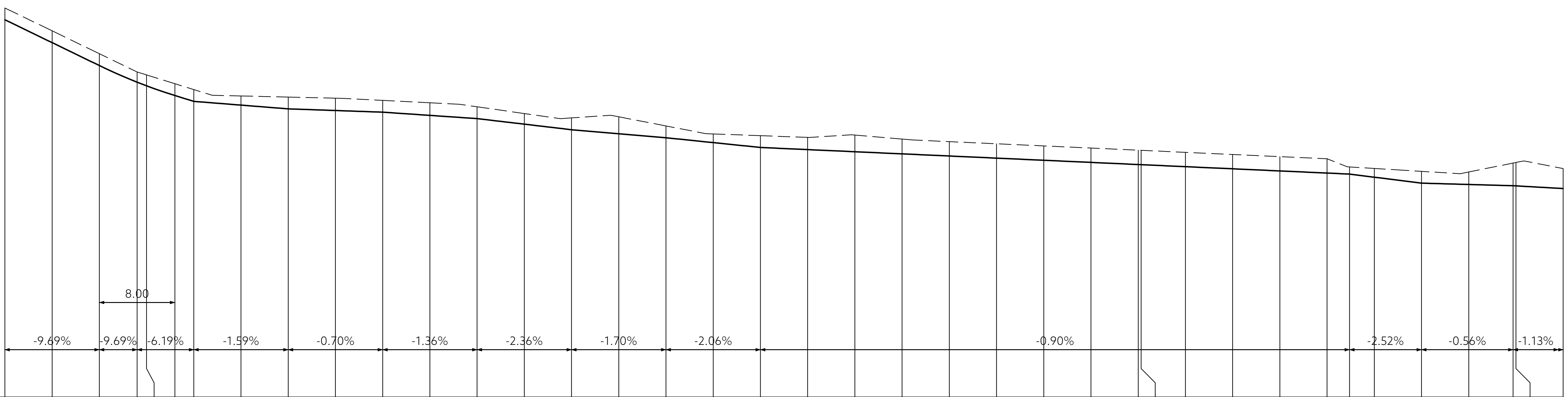

CHAINAGE	CUT / FILL	INVERT OF OPEN DRAIN	EXISTING SURFACE
0.000	-0.250	55.987	56.237
5.000	-0.250	55.502	55.752
10.000	-0.250	55.018	55.268
13.999	-0.215	54.666	54.881
15.000	-0.231	54.588	54.819
17.997	-0.250	54.383	54.633
20.000	-0.250	54.259	54.509
25.000	-0.194	54.180	54.374
30.000	-0.250	54.100	54.350
35.000	-0.262	54.065	54.327
40.000	-0.250	54.030	54.280
45.000	-0.267	53.962	54.229
50.000	-0.250	53.894	54.144
55.000	-0.226	53.776	54.002
60.000	-0.250	53.658	53.908
65.000	-0.358	53.574	53.932
70.000	-0.250	53.489	53.739
75.000	-0.184	53.386	53.570
80.000	-0.250	53.283	53.533
85.000	-0.258	53.238	53.496
90.000	-0.354	53.192	53.546
95.000	-0.314	53.147	53.461
100.000	-0.300	53.102	53.402
105.000	-0.300	53.057	53.357
110.000	-0.300	53.012	53.312
115.000	-0.300	52.967	53.267
120.000	-0.300	52.921	52.221
120.317	-0.300	52.919	53.219
125.000	-0.300	52.876	53.176
130.000	-0.300	52.831	53.131
135.000	-0.300	52.786	53.086
140.000	-0.303	52.741	53.044
142.383	-0.151	52.719	52.870
145.000	-0.185	52.653	52.838
150.000	-0.250	52.527	52.777
155.000	-0.265	52.499	52.764
159.691	-0.479	52.473	52.952
160.000	-0.496	52.469	52.965
165.000	-0.423	52.413	52.836

HORIZONTAL GEOMETRY

- L=13.999 B=73°52'24"
- L=106.318 B=132°56'00"
- L=119.775 B=54°00'57"

LONG SECTION - V-DRAIN
SCALES: (H) 1:500 (V) 1:100 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED: RD REVIEWED: MW
DRAWN: RD REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

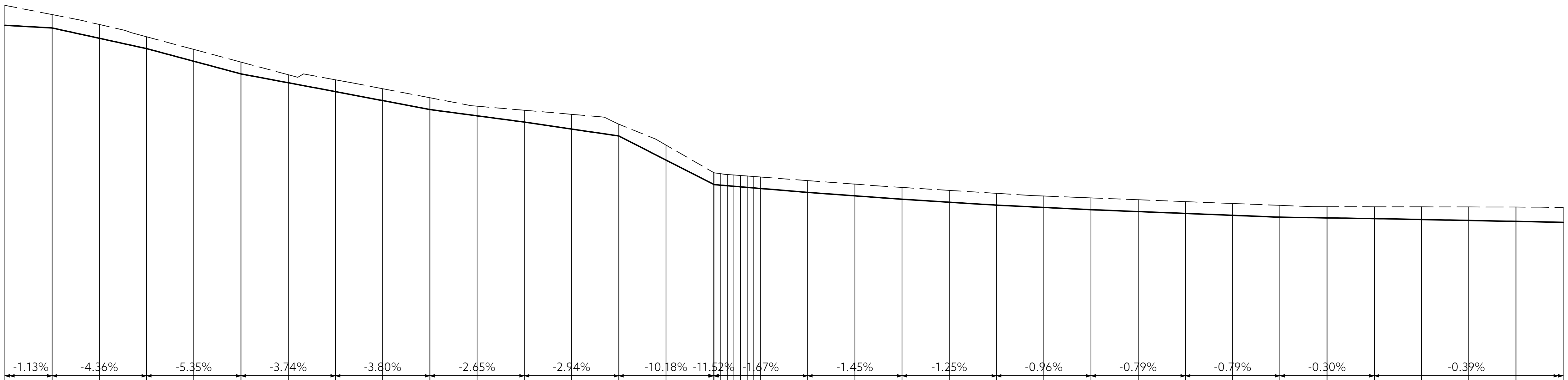
CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: V-DRAIN LONG SECTION SHEET 1 OF 3

PDA SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE	PAPER
------	AS SHOWN	(A3)

JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	210	A

DATE/TIME: Friday, 8 August 2025 2:48:50 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



R.L. 44.000

CHAINAGE	CUT / FILL	INVERT OF OPEN DRAIN	EXISTING SURFACE
165.000	-0.423	52.413	52.836
170.000	-0.283	52.357	52.640
175.000	-0.292	52.138	52.430
180.000	-0.250	51.920	52.170
185.000	-0.250	51.653	51.903
190.000	-0.250	51.385	51.635
195.000	-0.169	51.198	51.367
200.000	-0.250	51.011	51.261
205.000	-0.251	50.821	51.072
210.000	-0.250	50.631	50.881
215.000	-0.203	50.499	50.702
220.000	-0.250	50.366	50.616
225.000	-0.311	50.219	50.530
230.000	-0.250	50.072	50.322
235.000	-0.318	49.563	49.881
240.000	-0.250	49.054	49.304
240.092	-0.250	49.044	49.294
240.793	-0.242	49.032	49.274
241.494	-0.237	49.020	49.257
242.196	-0.237	49.009	49.246
242.897	-0.239	48.997	49.236
243.598	-0.240	48.985	49.225
244.299	-0.241	48.974	49.215
245.000	-0.242	48.962	49.204
250.000	-0.250	48.879	49.129
255.000	-0.247	48.806	49.053
260.000	-0.250	48.734	48.984
265.000	-0.250	48.672	48.922
270.000	-0.250	48.609	48.859
275.000	-0.242	48.561	48.803
280.000	-0.250	48.513	48.763
285.000	-0.250	48.473	48.723
290.000	-0.250	48.434	48.684
295.000	-0.250	48.394	48.644
300.000	-0.250	48.355	48.605
305.000	-0.237	48.340	48.577
310.000	-0.250	48.325	48.575
315.000	-0.268	48.305	48.573
320.000	-0.285	48.286	48.571
325.000	-0.303	48.266	48.569
330.000	-0.311	48.247	48.558

HORIZONTAL GEOMETRY: L=119.775 B=54°00'57" (165.000–240.000); L=114.059 B=27°01'17" (240.000–330.000)

LONG SECTION - V-DRAIN
SCALES: (H) 1:500 (V) 1:100 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

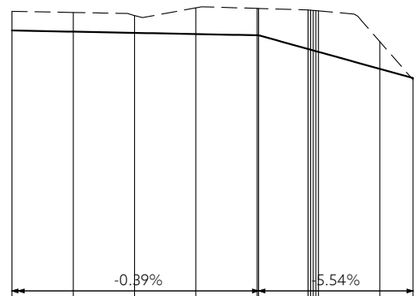
DESIGNED: RD REVIEWED: MW
DRAWN: RD REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: V-DRAIN LONG SECTION
SHEET 2 OF 3

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE	PAPER
------	AS SHOWN	(A3)

JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	211	A



R.L. 43.000													
CUT / FILL	-0.311	-0.313	-0.282	-0.428	-0.438	-0.437	-0.637	-0.645	-0.653	-0.661	-0.668	-0.444	+0.028
INVERT OF OPEN DRAIN	48.247	48.227	48.207	48.188	48.168	48.168	47.945	47.933	47.921	47.909	47.898	47.621	47.471
EXISTING SURFACE	48.558	48.540	48.489	48.616	48.606	48.605	**48.582**	48.578	48.574	48.570	48.566	48.065	**47.443**
CHAINAGE	330.000	335.000	340.000	345.000	350.000	350.121	354.151	354.364	354.576	354.788	355.000	360.000	362.711
HORIZONTAL GEOMETRY	L=114.059 B=27°01'17"										L=8.560 B=60°07'11"		

LONG SECTION - V-DRAIN
SCALES: (H) 1:500 (V) 1:100 (A3)

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL
COORDINATE/ DATUM: GDA2020 GRID
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DESIGNED: RD | REVIEWED: MW
DRAWN: RD | REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: V-DRAIN LONG SECTION
SHEET 3 OF 3

PDA
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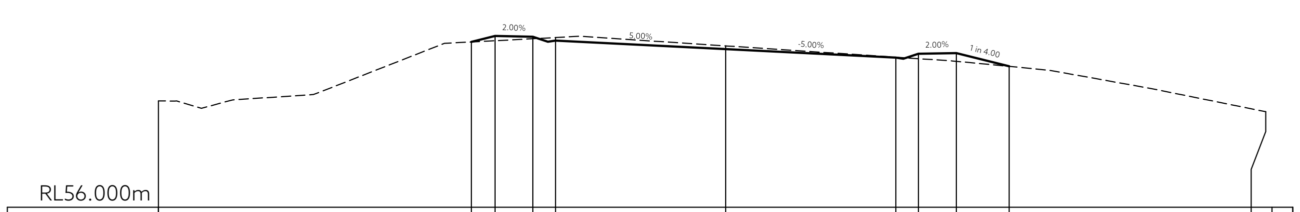
Also at: Kingston, Launceston & Burnie
REGISTRATION NUMBER: ----

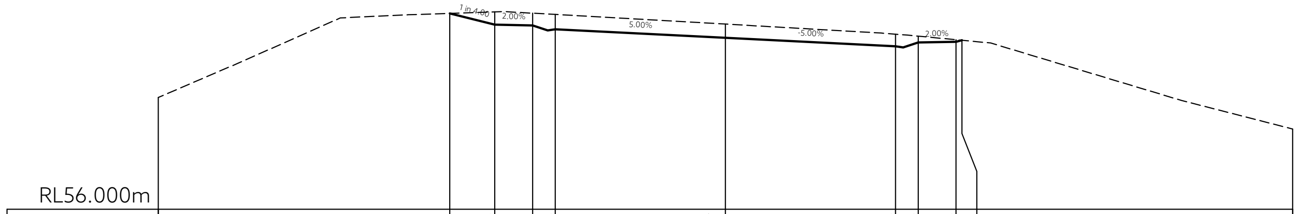
CONTRACT NO.	SCALE		PAPER
------	AS SHOWN (A3)		
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	212	A



RL56.000m

DESIGN		60.363	60.520	60.500	60.400	60.175	59.950	60.050	60.070	59.720			
EXISTING	58.805	60.363	60.395	60.448	60.480	60.259	59.961	59.922	59.852	59.720	58.523		
OFFSET	-15.000	-6.726	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	7.498	14.284	14.474	15.000

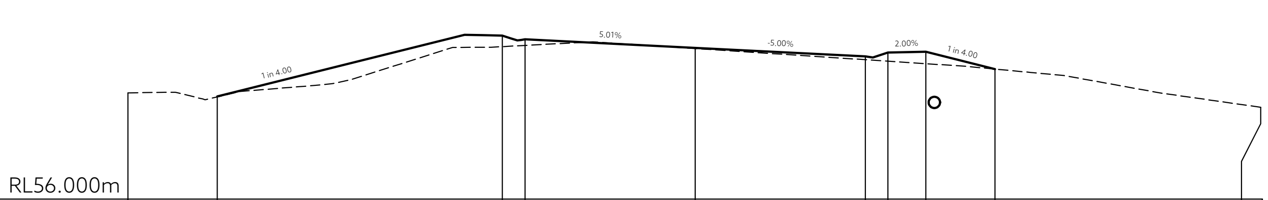
Ch 30.00



RL56.000m

DESIGN		61.170	60.872	60.852	60.752	60.527	60.302	60.402	60.422	60.461	
EXISTING	58.951	61.170	61.210	61.176	61.142	60.888	60.621	60.569	60.475	60.461	58.118
OFFSET	-15.000	-7.293	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	6.255	15.000

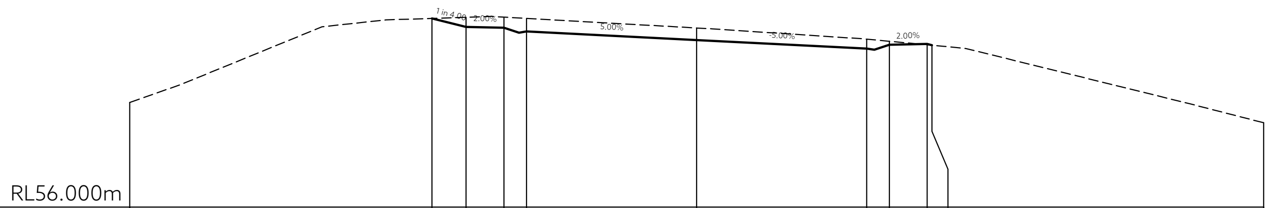
Ch 60.00



RL56.000m

DESIGN		58.722	60.338	60.238	60.012	59.787	59.887	59.907	59.449		
EXISTING	58.818	58.711	60.045	60.076	59.994	59.698	59.659	59.592	59.449	58.435	
OFFSET	-15.000	-12.642	-5.100	-4.500	0.000	4.500	5.100	6.100	7.930	14.960	15.000

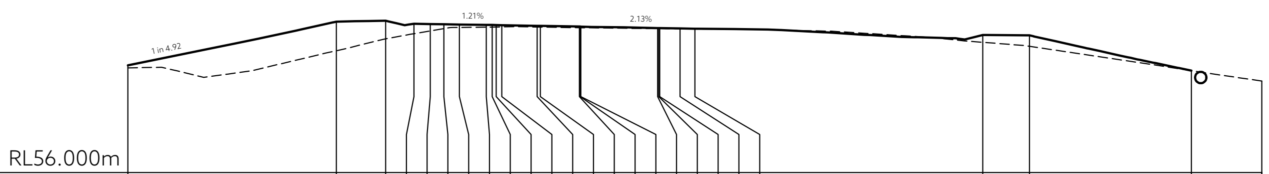
Ch 20.00



RL56.000m

DESIGN		60.981	60.755	60.735	60.635	60.410	60.185	60.285	60.305	60.274	
EXISTING	58.762	60.981	61.011	61.015	60.981	60.728	60.432	60.377	60.285	60.274	58.227
OFFSET	-15.000	-7.003	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	6.227	15.000

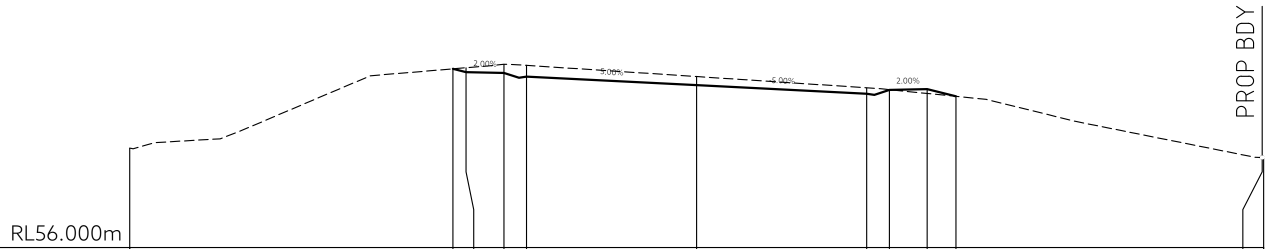
Ch 50.00



RL56.000m

DESIGN	58.826	59.979	60.002	59.923	59.914	59.911	59.903	59.894	59.891	59.889	59.885	59.869	59.867	59.850	59.849	59.849	59.810	59.809	59.809	59.799	59.792	59.626	59.616	58.687	
EXISTING	58.759	59.211	59.525	59.662	59.734	59.794	59.824	59.834	59.837	59.838	59.841	59.837	59.835	59.820	59.819	59.819	59.794	59.794	59.794	59.789	59.786	59.434	59.329	58.687	58.413
OFFSET	-15.000	-9.487	-8.184	-7.431	-7.000	-6.640	-6.231	-5.519	-5.363	-5.256	-5.108	-4.173	-4.088	-3.057	-3.035	-3.018	-0.985	-0.957	-0.931	-0.391	0.000	7.618	8.852	13.137	15.000

Ch 10.00



RL56.000m

DESIGN		60.725	60.638	60.618	60.518	60.293	60.068	60.168	60.188	59.998		
EXISTING	58.629	60.725	60.754	60.845	60.818	60.522	60.224	60.175	60.075	59.998	58.379	58.378
OFFSET	-15.000	-6.447	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	6.861	14.962	15.000

Ch 40.00

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED: RD | REVIEWED: MW
DRAWN: RD | REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: ROAD 1 CROSS SECTIONS SHEET 1 OF 3

PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
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PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
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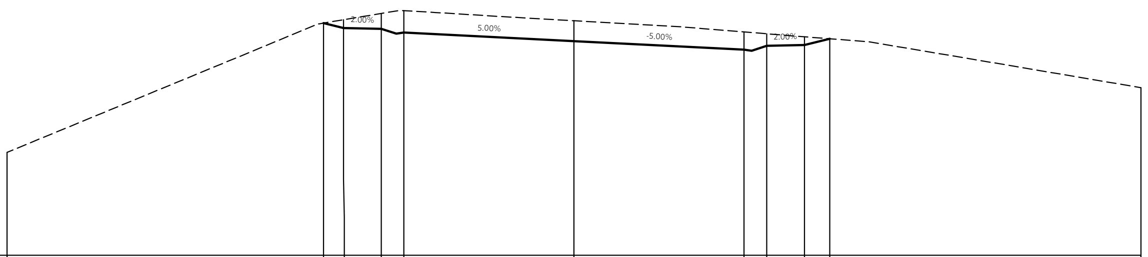
Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 200		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	300	A

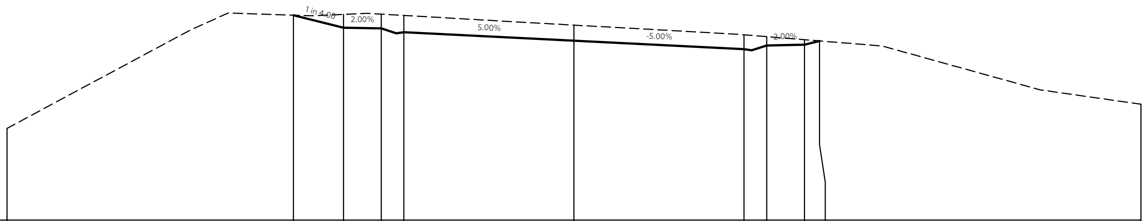
DATE/TIME: Friday, 8 August 2025 2:48:51 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



RL55.000m

DESIGN		61.142	61.010	60.990	60.890	60.665	60.440	60.540	60.560	60.727	
EXISTING	57.721	61.142	61.230	61.396	61.471	61.204	60.907	60.858	60.778	60.727	59.432
OFFSET	-15.000	-6.627	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	6.767	15.000

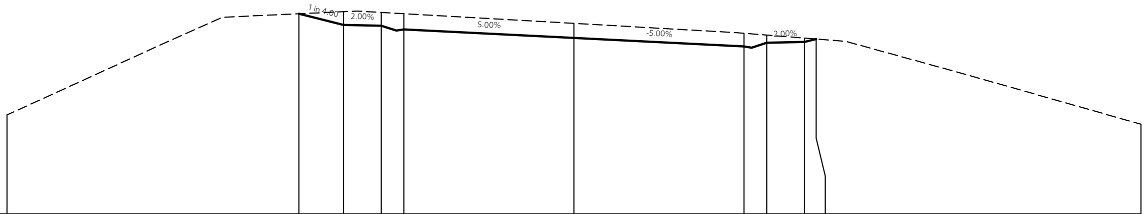
Ch 90.00



RL56.000m

DESIGN		61.421	61.090	61.070	60.970	60.745	60.520	60.620	60.640	60.739	
EXISTING	58.427	61.421	61.442	61.448	61.414	61.158	60.901	60.853	60.771	60.739	59.065
OFFSET	-15.000	-7.424	-6.100	-5.100	-4.500	-0.000	4.500	5.100	6.100	6.498	15.000

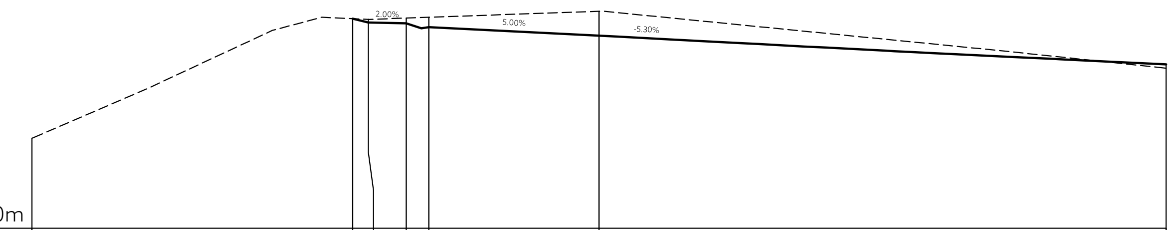
Ch 80.00



RL56.000m

DESIGN		61.283	60.989	60.969	60.869	60.644	60.419	60.519	60.539	60.616	
EXISTING	58.612	61.283	61.337	61.318	61.284	61.029	60.767	60.720	60.641	60.616	58.366
OFFSET	-15.000	-7.278	-6.100	-5.100	-4.500	0.000	4.500	5.100	6.100	6.409	15.000

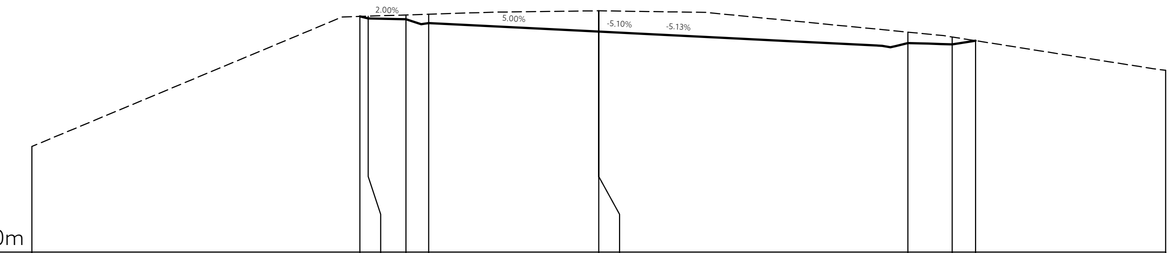
Ch 70.00



RL54.000m

DESIGN		59.532	59.428	59.408	59.308	59.083	58.327
EXISTING	56.373	59.532	59.514	59.547	59.568	59.728	58.224
OFFSET	-15.000	-6.515	-6.100	-5.100	-4.500	0.000	15.000

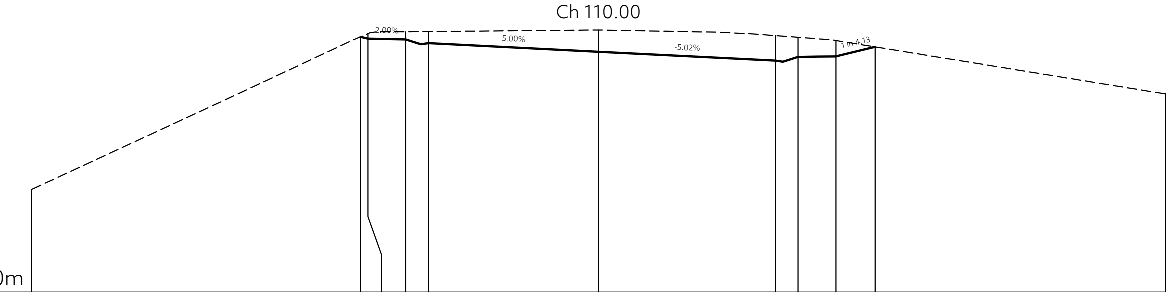
Ch 120.00



RL54.000m

DESIGN		60.234	60.179	60.159	60.059	59.834	59.834	59.529	59.494	59.593	
EXISTING	56.793	60.234	60.241	60.273	60.292	60.383	60.383	59.819	59.690	59.593	58.809
OFFSET	-15.000	-6.320	-6.100	-5.100	-4.500	-0.001	0.000	8.181	9.354	9.971	15.000

Ch 110.00



RL54.000m

DESIGN		60.755	60.707	60.687	60.587	60.362	60.127	60.224	60.238	60.489	
EXISTING	56.717	60.755	60.838	60.890	60.895	60.936	60.787	60.742	60.657	60.489	59.244
OFFSET	-15.000	-6.295	-6.100	-5.100	-4.500	0.000	4.678	5.280	6.285	7.321	15.000

Ch 100.00

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED: RD | REVIEWED: MW
DRAWN: RD | REVIEWED: MW
JOB MANAGER: CRAIG TERRY
ISSUED DATE: 25/07/2024

CLIENT: ONETRAK
PROJECT DESCRIPTION: 4 LOT SUBDIVISION
ADDRESS: 80 POSSUM ROAD
DRAWING TITLE: ROAD 1 CROSS SECTIONS SHEET 2 OF 3

PDA SURVEYORS, ENGINEERS & PLANNERS

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Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
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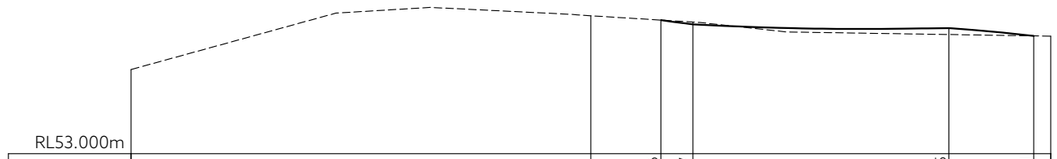
Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE	PAPER
------	1: 200	(A3)

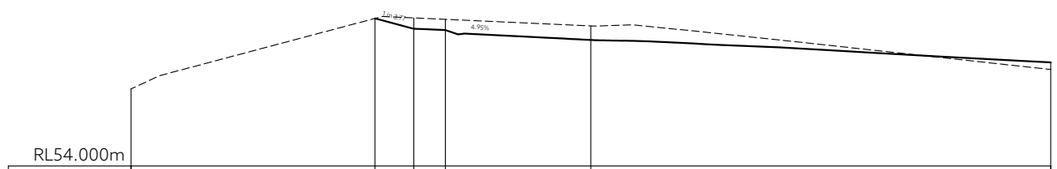
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	301	A

DATE/TIME: Friday, 8 August 2025 2:48:52 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



RL53.000m											
DESIGN						57.356	57.217			57.095	56.841
EXISTING	55.744				57.497	57.356	57.292			56.885	56.841	56.832
OFFSET	-15.000				0.000	2.283	3.329			11.681	14.449	15.000

Ch 140.00



RL54.000m											
DESIGN			58.813	58.478	58.434			58.111			57.381
EXISTING	56.515		58.813	58.830	58.784			58.570			57.149
OFFSET	-15.000		-7.043	-5.777	-4.749			0.000			15.000

Ch 130.00

REV	AMENDMENTS	DRAWN	DATE	APPR.
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-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	RD	REVIEWED:	MW
DRAWN:	RD	REVIEWED:	MW
JOB MANAGER:	CRAIG TERRY		
ISSUED DATE:	25/07/2024		

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: ROAD 1 CROSS SECTIONS SHEET 3 OF3

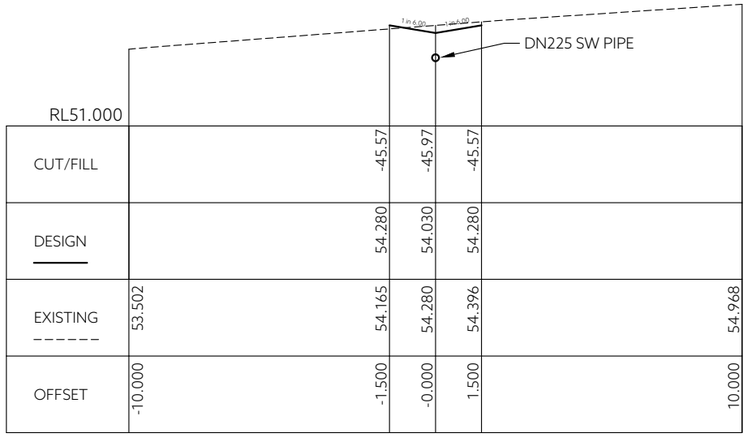
PDA
SURVEYORS, ENGINEERS & PLANNERS

127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

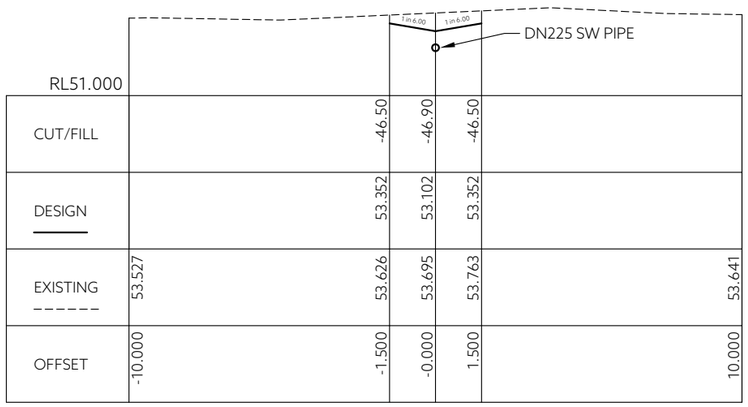
CONTRACT NO.	SCALE		PAPER
------	1: 200		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	302	A

DATE/TIME: Friday, 8 August 2025 2:48:52 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



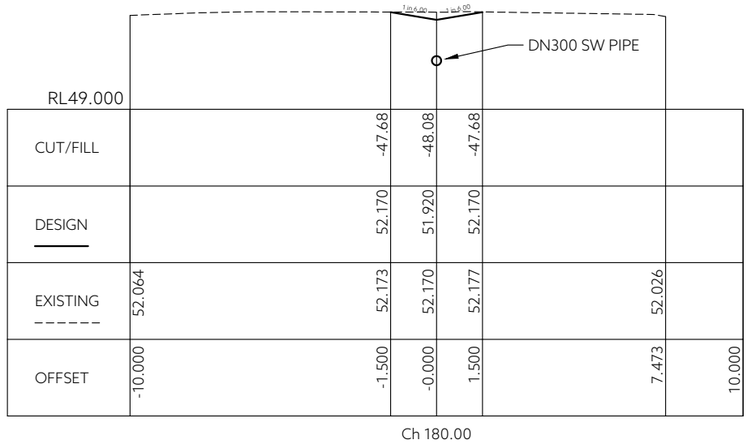
RL51.000					
CUT/FILL		-45.57	-45.97	-45.57	
DESIGN		54.280	54.030	54.280	
EXISTING	53.502	54.165	54.280	54.396	54.968
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 40.00



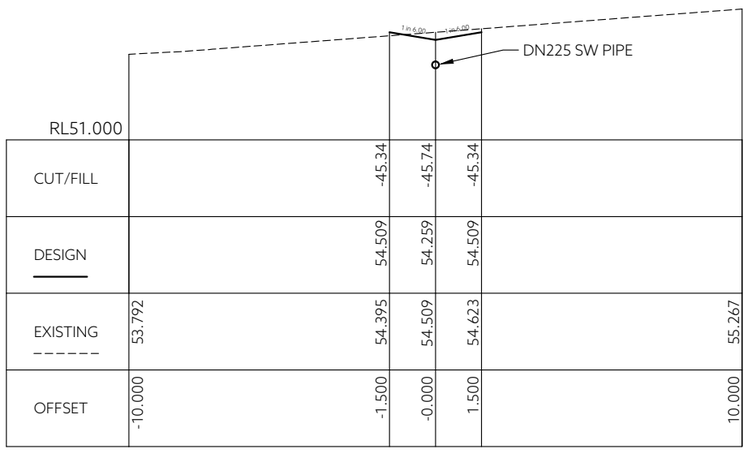
RL51.000					
CUT/FILL		-46.50	-46.90	-46.50	
DESIGN		53.352	53.102	53.352	
EXISTING	53.527	53.626	53.695	53.763	53.641
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 100.00



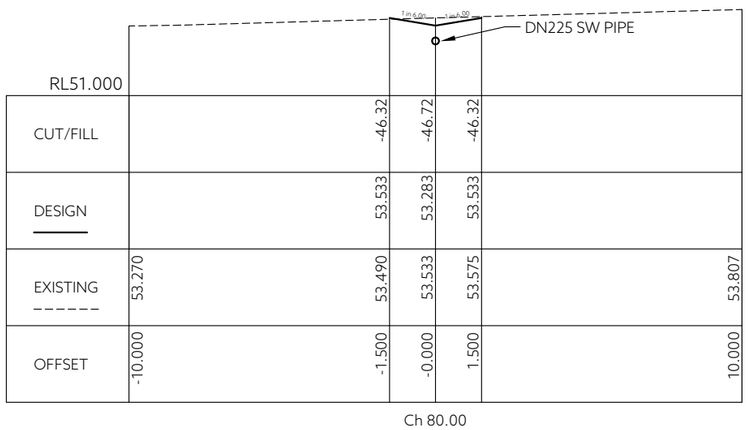
RL49.000						
CUT/FILL		-47.68	-48.08	-47.68		
DESIGN		52.170	51.920	52.170		
EXISTING	52.064	52.173	52.170	52.177	52.026	
OFFSET	-10.000	-1.500	-0.000	1.500	7.473	10.000

Ch 180.00



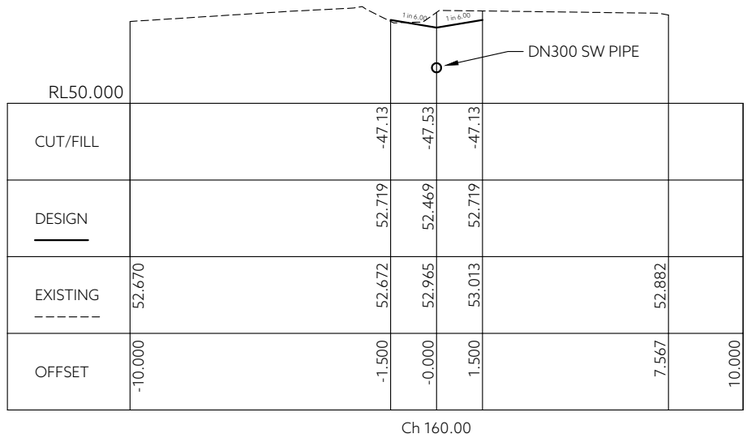
RL51.000					
CUT/FILL		-45.34	-45.74	-45.34	
DESIGN		54.509	54.259	54.509	
EXISTING	53.792	54.395	54.509	54.623	55.267
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 20.00



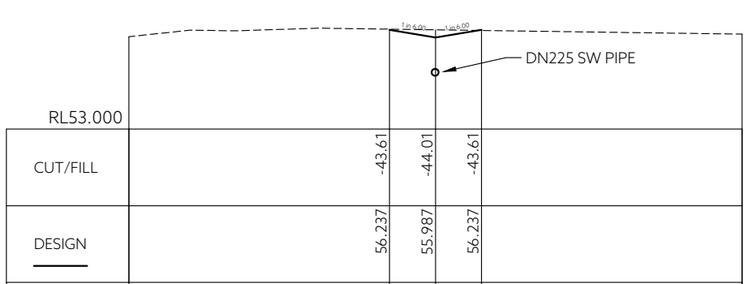
RL51.000					
CUT/FILL		-46.32	-46.72	-46.32	
DESIGN		53.533	53.283	53.533	
EXISTING	53.270	53.490	53.533	53.575	53.807
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 80.00



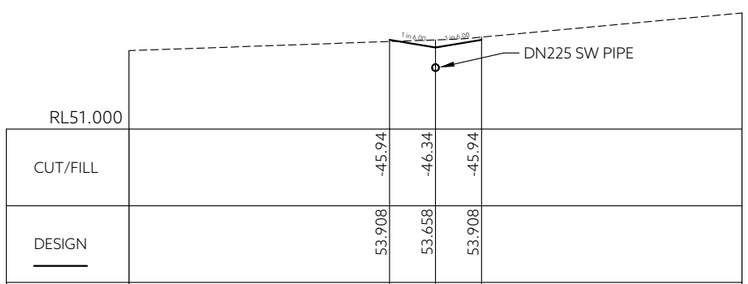
RL50.000						
CUT/FILL		-47.13	-47.53	-47.13		
DESIGN		52.719	52.469	52.719		
EXISTING	52.670	52.672	52.965	53.013	52.882	
OFFSET	-10.000	-1.500	-0.000	1.500	7.567	10.000

Ch 160.00



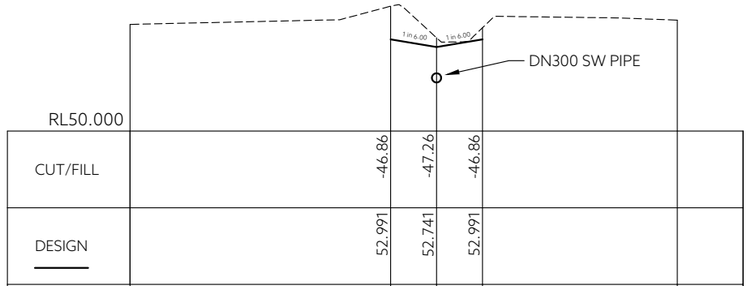
RL53.000					
CUT/FILL		-43.61	-44.01	-43.61	
DESIGN		56.237	55.987	56.237	
EXISTING	56.013	56.258	56.237	56.216	56.096
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 0.00



RL51.000					
CUT/FILL		-45.94	-46.34	-45.94	
DESIGN		53.908	53.658	53.908	
EXISTING	53.558	53.856	53.908	53.998	54.785
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 60.00



RL50.000						
CUT/FILL		-46.86	-47.26	-46.86		
DESIGN		52.991	52.741	52.991		
EXISTING	53.479	54.072	53.044	53.371	53.616	
OFFSET	-10.000	-1.500	-0.000	1.500	7.853	10.000

Ch 140.00

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: V-DRAIN CROSS SECTIONS SHEET 1 OF 2

PDA
SURVEYORS, ENGINEERS & PLANNERS
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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE	PAPER
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JOB NUMBER	DISCIPLINE / SHEET	REVISION
46981NG	C 310	A

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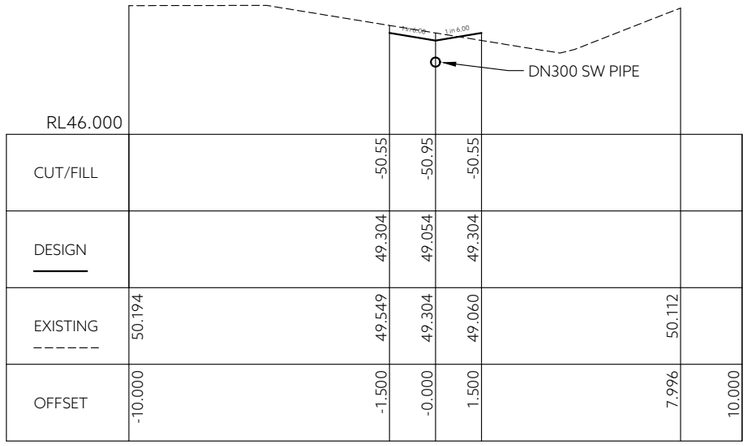
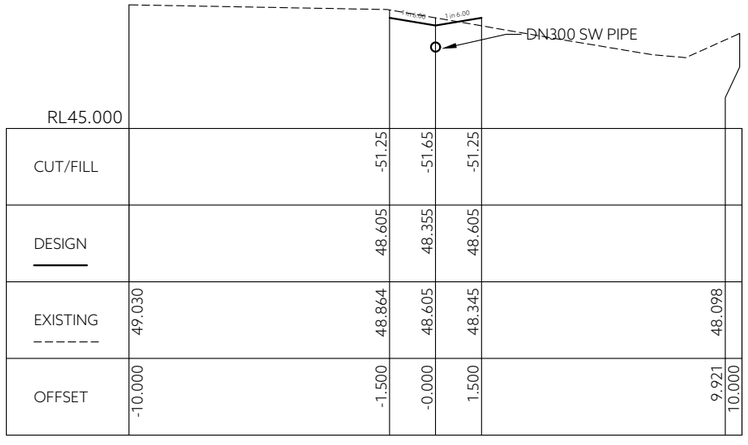


RL46.000

CUT/FILL		-50.55	-50.95	-50.55		
DESIGN		49.304	49.054	49.304		
EXISTING	50.194	49.549	49.304	49.060	50.112	
OFFSET	-10.000	-1.500	-0.000	1.500	7.996	10.000

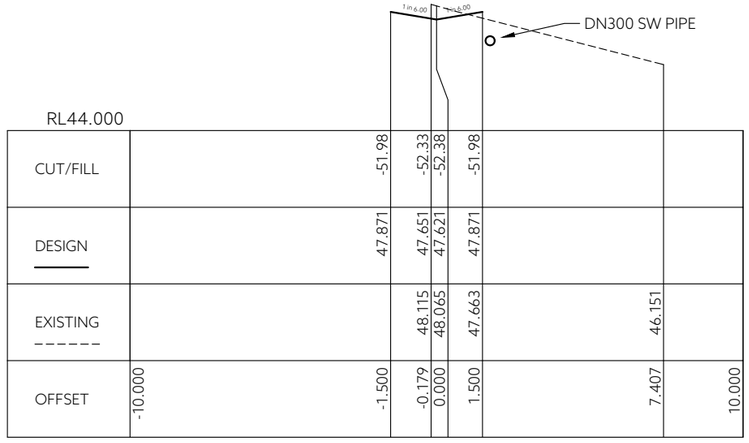
Ch 240.00



RL45.000

CUT/FILL		-51.25	-51.65	-51.25	
DESIGN		48.605	48.355	48.605	
EXISTING	49.030	48.864	48.605	48.345	48.098
OFFSET	-10.000	-1.500	-0.000	1.500	9.921 10.000

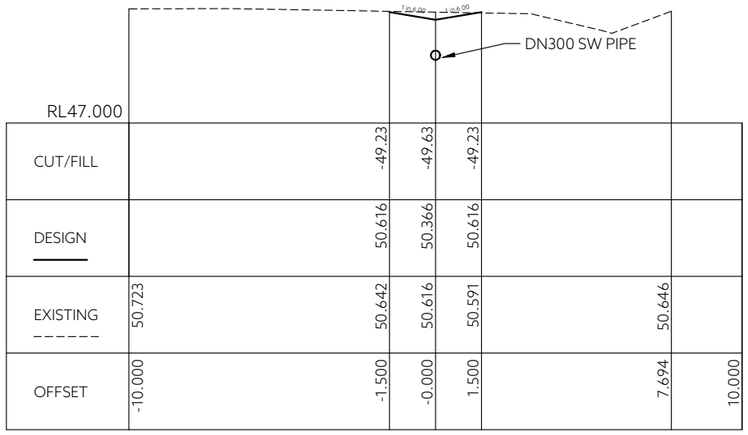
Ch 300.00



RL44.000

CUT/FILL		-51.98	-52.33	-52.38	-51.98	
DESIGN		47.871	47.651	47.621	47.871	
EXISTING			48.115	48.065	47.663	46.151
OFFSET	-10.000	-1.500	-0.179	0.000	1.500	7.407 10.000

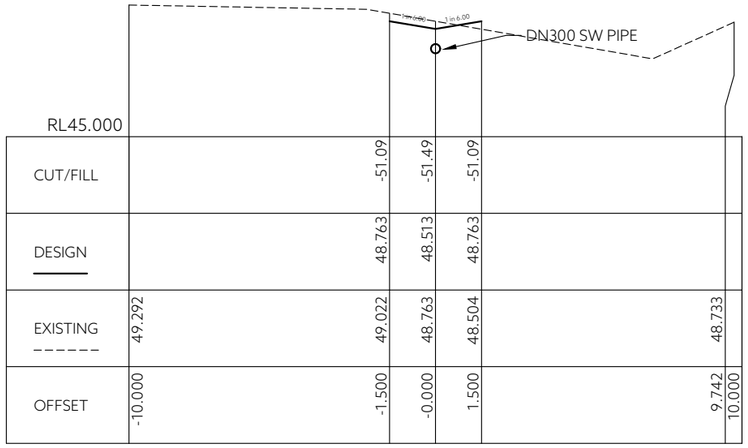
Ch 360.00



RL47.000

CUT/FILL		-49.23	-49.63	-49.23		
DESIGN		50.616	50.366	50.616		
EXISTING	50.723	50.642	50.616	50.591	50.646	
OFFSET	-10.000	-1.500	-0.000	1.500	7.694	10.000

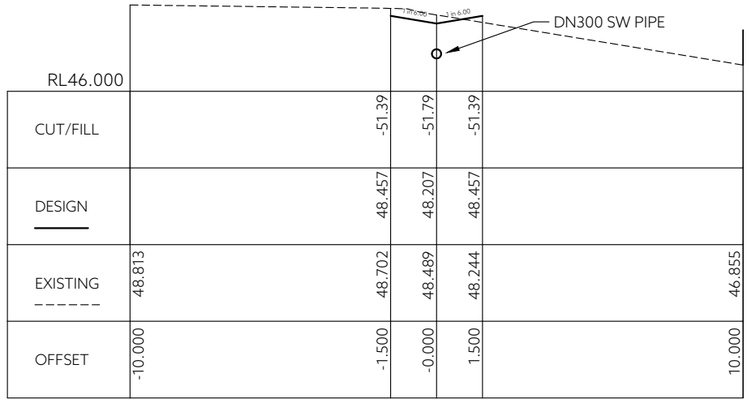
Ch 220.00



RL45.000

CUT/FILL		-51.09	-51.49	-51.09	
DESIGN		48.763	48.513	48.763	
EXISTING	49.292	49.022	48.763	48.504	48.733
OFFSET	-10.000	-1.500	-0.000	1.500	9.742 10.000

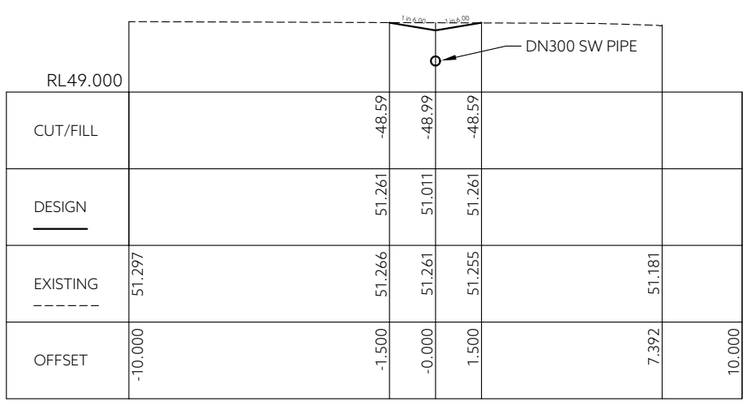
Ch 280.00



RL46.000

CUT/FILL		-51.39	-51.79	-51.39		
DESIGN		48.457	48.207	48.457		
EXISTING	48.813	48.702	48.489	48.244	46.855	
OFFSET	-10.000	-1.500	-0.000	1.500	10.000	

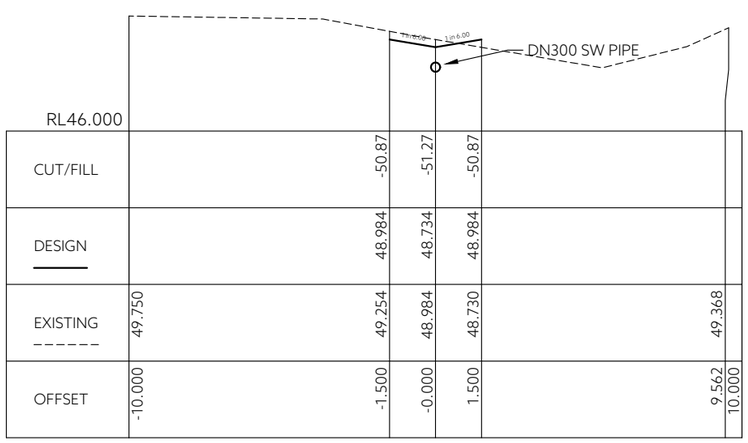
Ch 340.00



RL49.000

CUT/FILL		-48.59	-48.99	-48.59		
DESIGN		51.261	51.011	51.261		
EXISTING	51.297	51.266	51.261	51.255	51.181	
OFFSET	-10.000	-1.500	-0.000	1.500	7.392	10.000

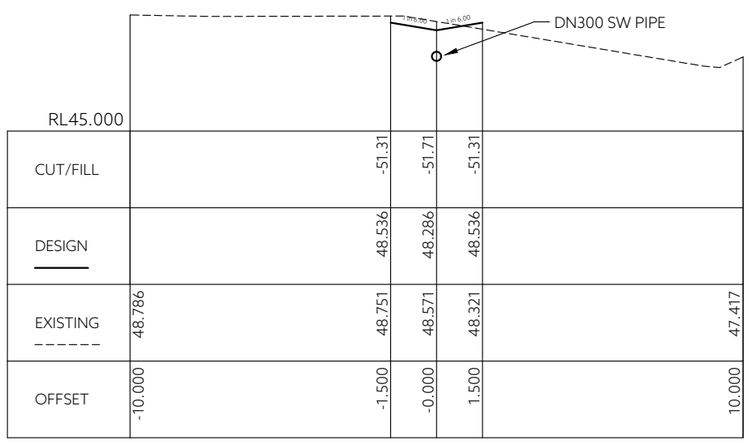
Ch 200.00



RL46.000

CUT/FILL		-50.87	-51.27	-50.87	
DESIGN		48.984	48.734	48.984	
EXISTING	49.750	49.254	48.984	48.730	49.368
OFFSET	-10.000	-1.500	-0.000	1.500	9.562 10.000

Ch 260.00



RL45.000

CUT/FILL		-51.31	-51.71	-51.31	
DESIGN		48.536	48.286	48.536	
EXISTING	48.786	48.751	48.571	48.321	47.417
OFFSET	-10.000	-1.500	-0.000	1.500	10.000

Ch 320.00

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED: RD | REVIEWED: MW

DRAWN: RD | REVIEWED: MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: V-DRAIN CROSS SECTIONS SHEET 2 OF 2

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127 Bathurst Street
Hobart, Tasmania, 7000
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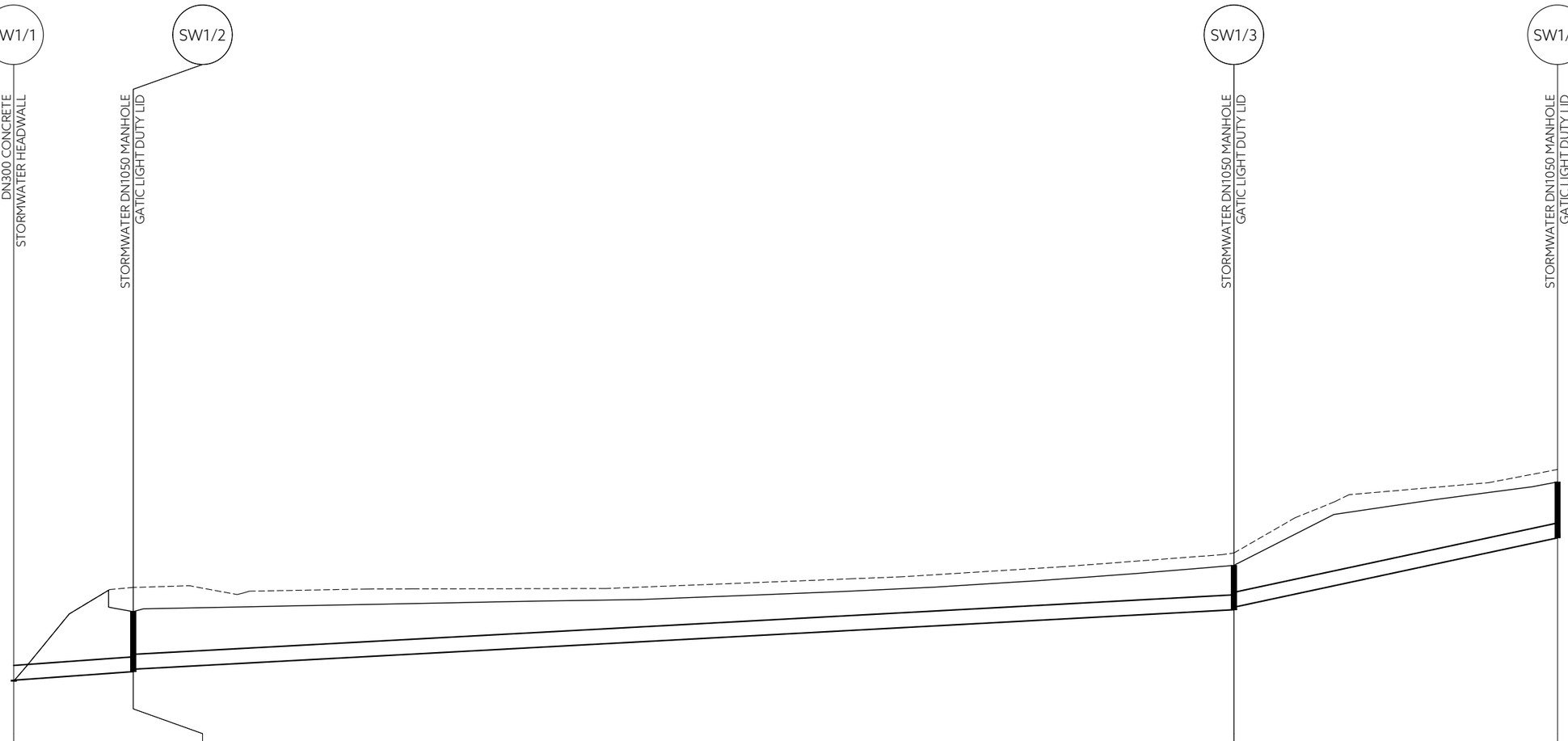
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 200		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	311	A

DATE/TIME: Friday, 8 August 2025 2:48:53 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



SW-LS LEGEND:

FCR BACKFILL

PIPE DETAILS	DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)		DN300 PP SN8 (RRJ)	
GRADE	1.44%		1.08%		4.29%	
DATUM RL 44						
COVER	-0.318	0.911	0.861	0.594	0.544	0.828
DEPTH TO INVERT	-0.018	1.211	1.161	0.894	0.844	1.128
INVERT LEVEL	46.726	46.900	46.950	48.150	48.200	49.600
FINISHED SURFACE	46.708	48.111		49.044		50.728
EXISTING SURFACE	46.708	48.599		49.294		50.979
CHAINAGE	0.000	12.066		123.125		155.777
	12.066		111.059		32.651	

SW LS - LINE SW1

SCALE: HORIZ 1:500 VERT 1:100

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: STORMWATER LONG SECTION SHEET 1 OF 4

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	AS SHOWN (A3)		
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	400	A

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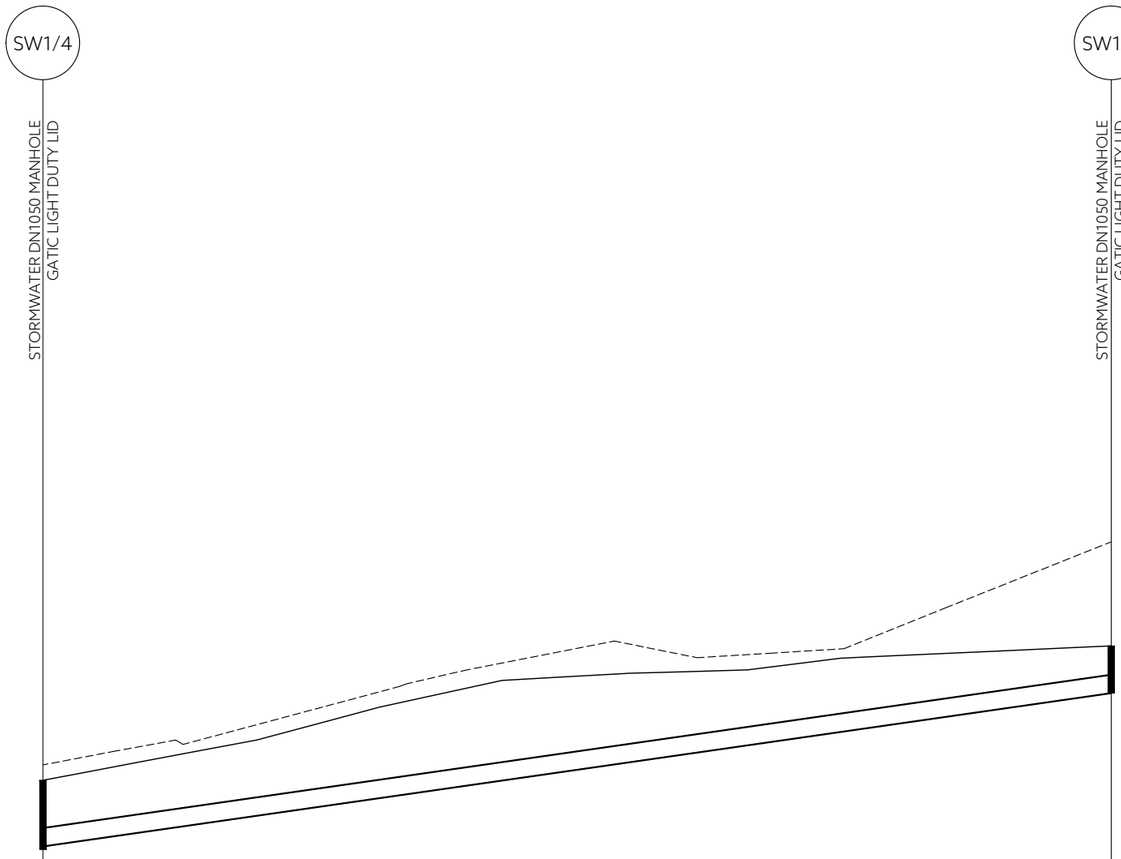

SW-LS LEGEND:

FCR BACKFILL

PIPE DETAILS	DN300 PP SN8 (RRJ)	
GRADE	2.87%	
DATUM RL 47		
COVER	0.778	0.469
DEPTH TO INVERT	1.078	0.769
INVERT LEVEL	49.650	52.150
FINISHED SURFACE	50.728	52.919
EXISTING SURFACE	50.979	54.616
CHAINAGE	155.777	242.901
	87.124	

SW LS - LINE SW1

SCALE: HORIZ 1:500 VERT 1:100

REV	AMENDMENTS	DRAWN	DATE	APPR.
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

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CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: STORMWATER LONG SECTION SHEET 2 OF 4

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CONTRACT NO.	SCALE		PAPER
------	AS SHOWN		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	401	A

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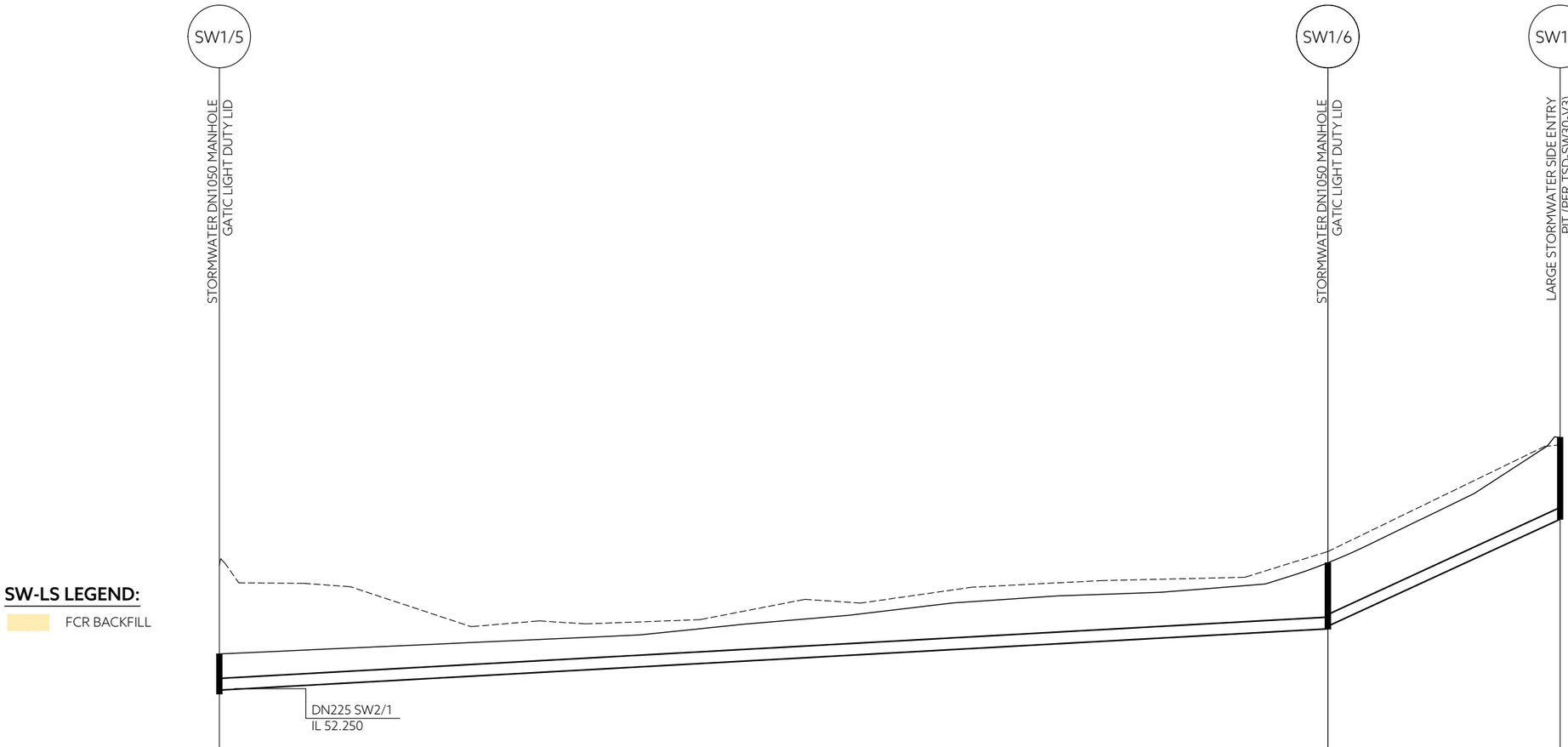

SW-LS LEGEND:

FCR BACKFILL

PIPE DETAILS	DN225 PP SN8 (RRJ)		DN225 PP SN8 (RRJ)
GRADE	1.11%		9.20%
DATUM RL 49			
COVER	0.469	1.041	0.991 1.343
DEPTH TO INVERT	0.694	1.266	1.216 1.568
INVERT LEVEL	52.225	53.400	53.450 55.500
FINISHED SURFACE	52.919	54.666	57.068
EXISTING SURFACE	54.616	54.881	56.925
CHAINAGE	242.901	349.218	371.504
	106.318		22.286

SW LS - LINE SW1

SCALE: HORIZ 1:500 VERT 1:100

REV	AMENDMENTS	DRAWN	DATE	APPR.
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-	----	--	--/--/----	--
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DRAWN:	RD	REVIEWED:	MW
JOB MANAGER:	CRAIG TERRY		
ISSUED DATE:	25/07/2024		

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: STORMWATER LONG SECTION SHEET 3 OF 4

PDA
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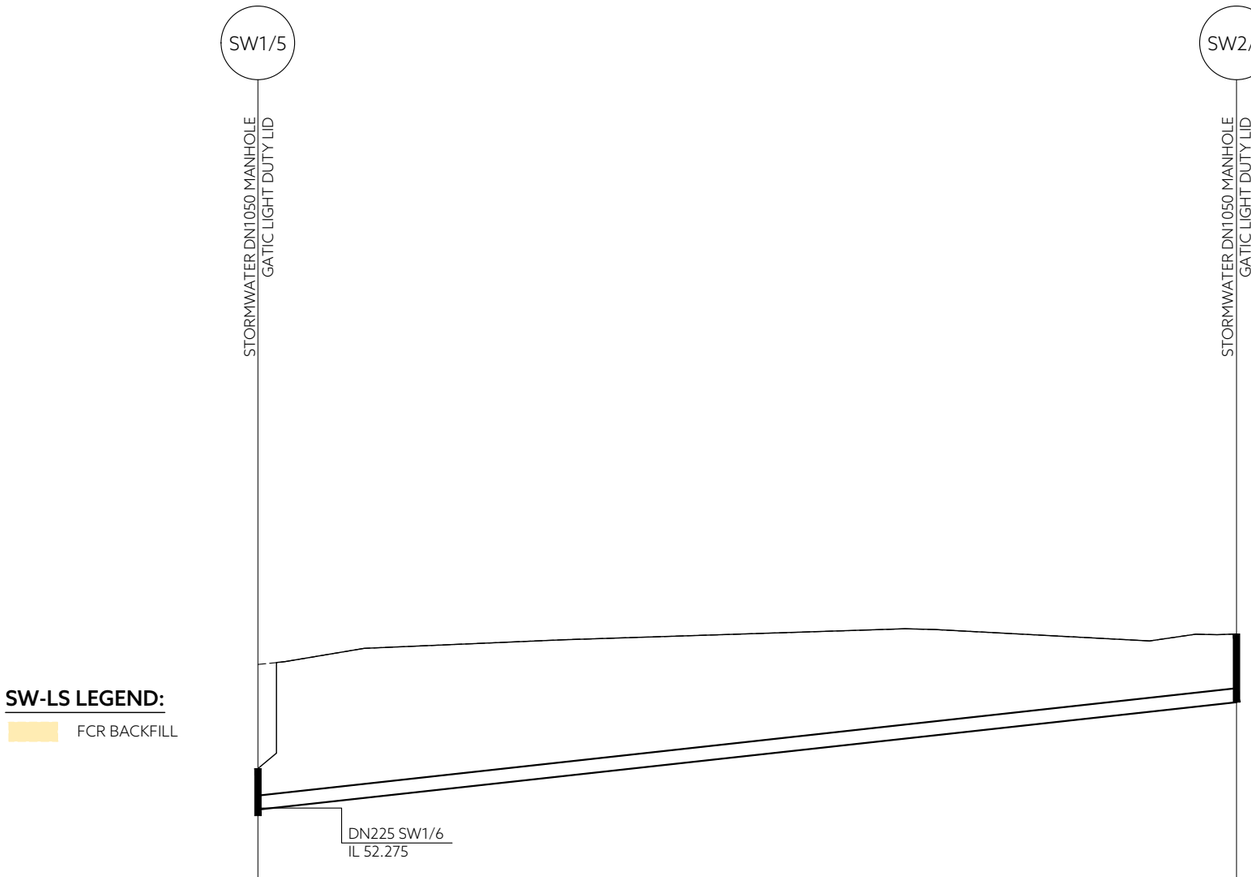
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	AS SHOWN		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	402	A

DATE/TIME: Friday, 8 August 2025 2:48:54 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



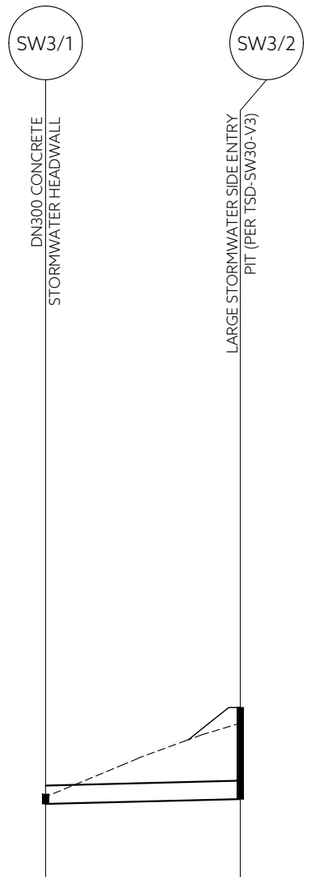
SW-LS LEGEND:

FCR BACKFILL

PIPE DETAILS		DN225 Class 4 RRJ	
GRADE		2.19%	
DATUM RL 49			
COVER	0.440		0.886
DEPTH TO INVERT	0.669		1.115
INVERT LEVEL	52.250		54.000
FINISHED SURFACE	52.919		55.115
EXISTING SURFACE	54.616		55.115
CHAINAGE	0.000	79.811	79.811

SW LS - LINE SW2

SCALE: HORIZ 1:500 VERT 1:100



PIPE DETAILS		DN300 Class 4 RRJ	
GRADE		0.50%	
DATUM RL 55			
COVER	-0.141		1.194
DEPTH TO INVERT	0.159		1.494
INVERT LEVEL	58.343		58.422
FINISHED SURFACE	58.502		59.917
EXISTING SURFACE	58.460		59.658
CHAINAGE	0.000	15.874	15.874

SW LS - LINE SW3

SCALE: HORIZ 1:500 VERT 1:100

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	RD	REVIEWED:	MW
DRAWN:	RD	REVIEWED:	MW
JOB MANAGER:	CRAIG TERRY		
ISSUED DATE:	25/07/2024		

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: STORMWATER LONG SECTION SHEET 4 OF 4

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie
REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	AS SHOWN (A3)		
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	403	A

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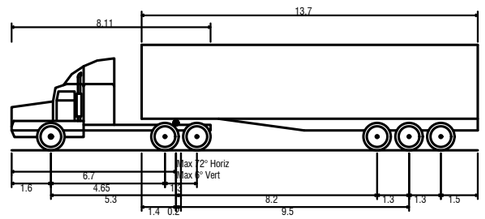
N

ROAD 1

LOT 2

LOT 1



Prime mover and semi-trailer (19 m)

Overall Length	19.000m
Overall Width	2.500m
Overall Body Height	4.300m
Min Body Ground Clearance	0.540m
Track Width	2.500m
Lock-to-lock time	6.00s
Curb to Curb Turning Radius	12.500m

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
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A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: TRUCK TURNING PATHS FOR LOT 2

PDA
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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	700	A

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N

NEW STREET LIGHT

SOUTH RAIL LINE

RIGHT O F WAY 10.00 WIDE

RIGHT O F WAY 10.00 WIDE

12.50

LOT 3

LOT 2

LOT 1

12.50

9.00

ROAD 1

FR156586/2

EXISTING POSSUM ROAD
CUL DE SAC

LEGEND:

- NEW STREET LIGHTING
- NEW STORMWATER
- EX. STORMWATER
- EX. FENCE
- TOP/TOE BANK
- ASPHALT
- CONCRETE

NOTES:

1. FOR GENERAL NOTES SEE DWG 46981NG-002
2. 0.1m FINAL DESIGN MODEL CONTOURS SHOWN
3. STREETLIGHT COLUMNS TO BE 7.5m + 2.0m O/R. 18W LED LANTERN TBC DURING CONSTRUCTION.
4. NEW UNDERGROUND POWER TO BE CONFIRMED DURING CONSTRUCTION

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
A	COUNCIL RAI RESPONSE (DATED 30.06.2025)	FM	8/08/2025	MW

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

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DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: STREET LIGHTING DETAIL PLAN

PDA
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REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	900	A

DATE/TIME: Friday, 8 August 2025 2:48:56 PM PLOTTED: FINN MURPHY FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG

STORMWATER MANAGEMENT REPORT

PROPOSED SUBDIVISION OF 80 POSSUM ROAD BRIDGEWATER

R.G. HAZELL PTY LTD
& DAVID HAZELL PTY LTD

April 2025

TABLE OF CONTENTS

Issuing Office: 117 Harrington Street, Hobart 7000 **JMG Project No. J230659CS**								
Document Issue Status								
Ver.	**Issue Date**	**Description**	**Originator**		**Checked**		**Approved**	
0	15/12/2023	Draft for Client Review	GLA		JMB		CJM	
1	31/07/2024	Revised for new layout	GLA		JMB		CJM	
2	10/09/2024	Revised for State Growth Culvert	GLA		JMB		CJM	
3	15/04/2025	Revised for 1% AEP Detention	GLA		JMB		CJM	

1. Introduction

Johnstone McGee & Gandy Pty Ltd (JMG) have been commissioned to provide a stormwater management report to support the application for subdivision of 80 Possum Road in Bridgewater into four lots together with Road. The proposed subdivision is detailed in the Plan of Subdivision as per PDA drawing 46981NG-1f in Appendix A. The proposal plan includes Right of Ways and Drainage Easements.

2. Scope of Report

This report analyses the existing and ultimate development stormwater behaviour of the parent lot. Given that it is zoned General Industrial, the stormwater events to be designed for are the 2% AEP plus Climate Change and the 1% AEP plus Climate Change storms. As the site drains to the existing Department of State Growth stormwater system installed as part of the Brighton Bypass, detention requirements are based on the 1% AEP event at their request.

As this is a subdivision proposal only, a conservative approach has been adopted for the future development of the lots. It is assumed that the entire area of the lots will be impervious with the exception of an allowance of 400m^2 for potential effluent disposal area per lot inclusive of a 100% reserve area.

3. Stormwater Requirements

Stormwater for the proposed subdivision needs to cater for the design rain events and comply with Water Sensitive Urban Drainage (WSUD) principles. The two key components are on site detention and water quality treatment.

Onsite detention is designed so that the runoff flow rate for the designed event is no greater than the predeveloped situation. This is achieved by providing the required amount of onsite storage and limiting the outflow rate.

Stormwater quality treatment can be achieved by numerous methods, with “natural” treatment options favoured over those that require replacement of filter elements, etc.

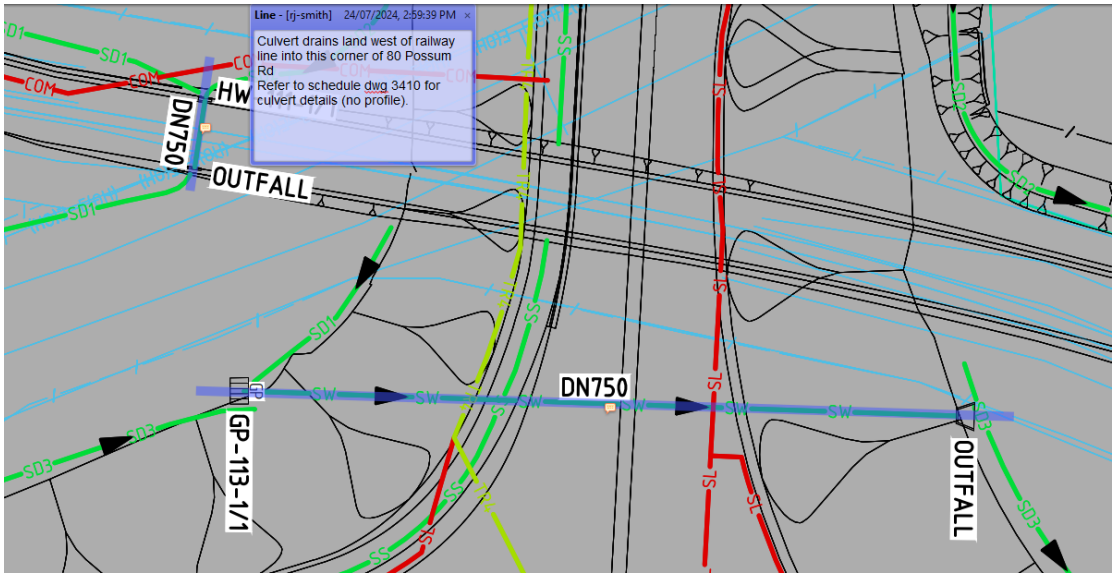
As Brighton Council have advised that they do not wish to have a combined detention facility and also that they do not support open swales passing through multiple lots, the management for detention and stormwater quality is proposed to be undertaken on each lot as part of the individual development applications. While the required detention volume is calculated for the subdivision, the actual detention storage will be provided for each new lot when development occurs, not during the construction of the subdivision.

Drainage from the lots is to an existing culvert which is a Department of State Growth asset installed as part of the Brighton Bypass project. This culvert is a 750mm outlet pipe, eventually flowing via a detention pond and into a tributary of the Jordan River. The detention on each of the new development lots will need to comply with the design assumptions for this outlet pipe and will need Department of State Growth approval for any discharge or connection to their system. The piped network for the subdivision will cater for the 2% AEP event as required for the General Industrial zoning, while the required detention is based on the 1% AEP event.

4. Existing Situation

It is noted that the proposed Lot 1 has been extensively developed under a previous Planning Permit. The entire lot drains under the Midland Highway offramp through the 750mm culvert which is a Department of State Growth asset. This culvert also picks up discharge under the railway formation and upstream properties as per the below snip from the as-constructed drawings provided below. This culvert discharges via a series of open drains, culverts, and a detention basin, eventually discharging via an unnamed watercourse into the Jordan River. Apart from the previous development on the proposed Lot 1, the remainder of the site is well vegetated with mostly grass cover. Note that although the Department of State Growth as-constructed drawings show the inlet as a grated pit GPP-113-1/1 a site visit confirms that this is in fact a culvert as per the below photo.

Photo of Department of State Growth Culvert Inlet Headwall and 750mm Pipe



Snip of State Growth As-Constructed Stormwater System showing Grated Pit GP-113-1/1 & 750 outlet

5. Proposed Ultimate Development

The subdivision design needs to allow for the ultimate development even though details of the developments on each lot will be the subject of future planning applications. For the purposes of analysis and to be conservative, it is assumed that each lot will be developed in a manner that results in the entire lot areas being impervious with the exception of an area of 400m^2 per lot to allow for onsite wastewater treatment and disposal which includes for a 100% backup area (as TasWater sewer disposal does not exist in this area).

The extension to Possum Road (formerly the Midland Highway) will drain partly to the southeast, with Brighton Council requiring kerb and channel although there is no kerb and channel on the existing road. The stormwater will be collected in a pit then piped to the existing open drain on the southeastern side of Possum Road that runs across Lot 1. The northern section of the Possum Road extension will drain from a pit at the low point of the new cul-de-sac head via a new piped system to discharge upstream of the existing Department of State Growth 750mm culvert noted above.

Lots 2, 3, and 4 will need to provide the required detention volume and stormwater quality improvement systems to meet the future individual development needs at the time of development. Lot 1 is the previously developed lot and is not required to provide detention or a stormwater quality system, with the total required detention being shared between Lots 2, 3, & 4 only.

6. Proposed Stormwater System

The runoff from each lot is proposed to be directed to a piped network within the drainage easements providing connections for future connection as developments occur. The pipe network is proposed to discharge into the existing Department of State Growth 750mm outlet pipe under the embankment.

The proposed piped network is designed to cater for the 2% AEP + Climate Change event, while the 1% AEP plus Climate Change event will discharge along the southwest of the lots via the existing overland flow path to the existing culvert, which has significant head

available in such an event due to the embankment constructed for the bypass and the natural slope of the proposed Lot 4. Refer Appendix D for a profile of the existing culvert showing the State Growth road above. The 1% AEP + Climate Change event will require overland flow across lot boundaries. The overland flow from the cul-de-sac will need to be directed down the easement on Lot 2 clear of any future building development. The overland flow from the south western extension of Possum Road will run into the existing open drain on Possum Road.

Given the lack of TasWater water mains, it is assumed that each new lot will be collecting roof runoff for onsite use, which would provide some improvement in water quality. Each new lot development will need to separately address the requirements for stormwater quality and onsite detention as part of their individual stormwater systems.

The extension of Possum Road is proposed to be provided with kerb and channel, with an inlet pit on the southern end discharging to the existing open drain across Lot 1, and an inlet pit from the cul-de-sac low point discharging via a new easement across Lot 2 to join the main pipe network.

The proposed concept is shown in Appendix C – Concept Stormwater Plan.

7. Analysis

The Department of State Growth have provided drawings and design reports for the existing stormwater system in this area. The design report notes that industrial land has been assumed to be fully developed, with industrial land allowed on the basis of 70% impervious and adopted C value of 0.9 with 30% pervious area with adopted C value of 0.2.

As the actual % impervious areas of the fully developed lots are not known at this stage, a conservative assumption is that all areas except for 400m^2 per lot is impervious. This results in a higher runoff than has been assumed in the Department of State Growth design. The calculations determine how much additional flow this results in for the 1% AEP event. Note that increasing the C value for pervious areas to 0.4 rather than Department of State Growth value of 0.2 results in an increased additional resultant flow, so has been adopted to be conservative.

The development of the lots will need to provide onsite detention to restrict the flow in the design event for the outlet pipe.

The full calculations of the proposed stormwater system are provided in Appendix B, with the important aspects summarised below.

- Time of Concentration = 25 minutes (Bransby-Williams)
- 2% AEP 25-minute intensity = 46.6mm/hr (BoM)
- 1% AEP 25-minute intensity = 53.8mm/hr (BoM)
- Total area = 43,080m^2
- Pervious Area = 1,600m^2
- Runoff coefficients C=0.9 impervious, C=0.2 pervious (as per State Growth design basis)
- 1% AEP post development flow = 445.7 l/s (excludes CC allowance)
- Climate Change factor 18.3 % (ARR)
- Detention Volume required = 292m^3 (Boyd's- based on DSG allowance)

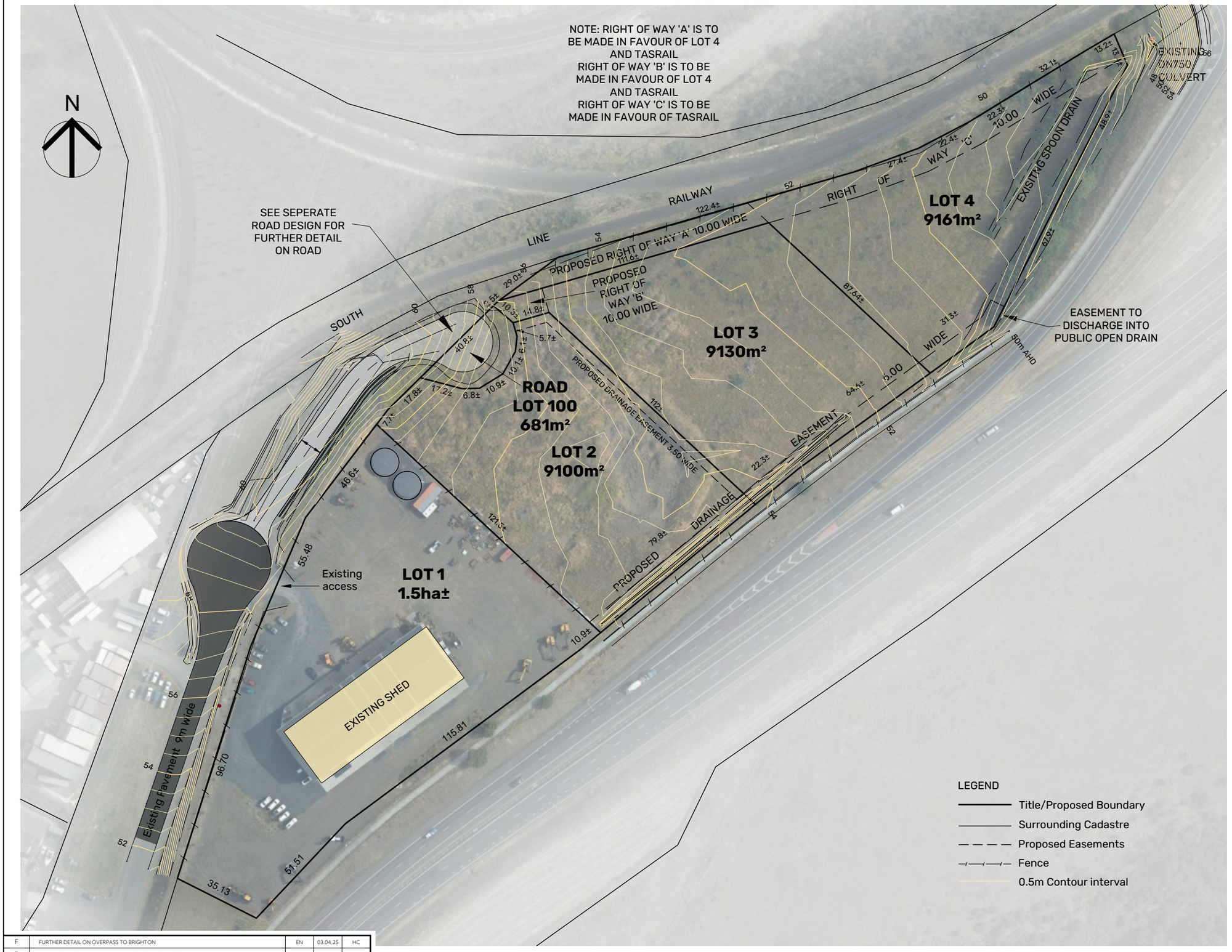
The Department of State Growth data shows the 750mm culvert has a slope of 0.5%. As shown on the calculations, this results in a Manning's capacity of 917 l/s. Given the socket end and headwall upstream end of the culvert, to achieve this maximum flow requires a headwater to diameter ratio of 1.6.

8. Conclusion

The analysis demonstrates that the required stormwater detention of 98kl per lot for lots 2,3, & 4 can be managed on the proposed lots (note that a reduction in impervious area compared to the calculations may justify less detention volume), with the quality treatment to meet or exceed the State Stormwater Strategy target reductions also being addressed for the individual developments on each lot. The extension of Possum Road will partly drain to the existing open drain on the southeastern side of the existing road, with the northern end of the extension draining via a pit and piped system via easements including overland flow path for larger events. The proposed stormwater management therefore complies with the applicable requirements for the proposed subdivision. Department of State Growth approval will be required for discharge of the proposed stormwater network into their system.

APPENDIX A

Plan of Subdivision (PDA Dwg 46981NG-1f)



PLAN OF SUBDIVISION

Owners
R.G. Hazell Pty. Ltd.
David Hazell Pty Ltd

Title References
FR 159938/4

Address
80 Possum Road Bridgewater

Council
Brighton Council

Tasmanian Planning scheme
Brighton Local Provisions Schedule

Zone
19.0 General Industrial

Code Overlay
9 Attenuation Code/Bridgewater Quarry
13 Bushfire-prone Area
BRI-S4.0, BRI-S10.0,

PID
3511316

Point of interest GDA2020 MGA55
59752E, 5270805N

Schedule of Easements
Proposed Rights of Way as shown.
Proposed Drainage Easement 6.00 wide

NOTES

This plan has been prepared only for the purpose of obtaining preliminary subdivision approval from the Council and the information shown hereon should be used for no other purpose.
All measurements and areas are subject to final survey.

All lots shown on plan cannot be serviced by water and sewer. On site treatment for sewer is required.

Water tanks are required for firefighting purposes.

The Site is covered in its entirety by the Codes listed above and have not been shown for clarity.

REV	AMENDMENTS	DRAWN	DATE	APPR.
F	FURTHER DETAIL ON OVERPASS TO BRIGHTON	EN	03.04.25	HC
E	CHANGED SW OUTLET DIAMETER AND REMOVED VARIOUS NOTES	EN	07.03.25	MK
D	EXTENDED DETAIL	EN	13.08.24	HC
C	ROAD LOT EXTENDED TO MATCH ROAD DESIGN	EN	01.08.24	HC
B	ROAD WIDTH INCREASED TO 11m	EN	18.07.24	CT
A	TURNING CIRCLE ADDED	EN	30.11.23	CT
0	PLAN OF SUBDIVISION	EN/MK	05.09.23	HC

NOTES:

SURVEYOR	GEOCIVIL
EN	EN
DRAWN	CHECKED
EN/MK	HC
DATE	
03/04/2025	

PLAN OF SUBDIVISION
80 POSSUM ROAD, BRIDGEWATER
for DAVID HAZELL

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
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SCALE	PAPER
1:1500	(A3)
JOB NUMBER	DRAWING
46981NG-1f	

APPENDIX B

Stormwater Calculations

80 Possum Road Subdivision										
Stormwater Management										
JMG Ref:	**J230659CS**									
Lot No.	**Area m2**		**Pervious area**	**Impervious area**	**Drainage easement**	**Piped Flow per lot for 2% l/s**	**Cumulative flow l/s**	**Nominal Slope 1 in**	**Nominal pipe size**	**Slope %**
1	15000		400	14600		60.06	60.06			
2	9631		400	9231		38.56	98.63	82	225	1.22%
3	9195		400	8795		36.82	135.45	27	300	3.70%
4	9161		400	8761		36.68	172.13	35	300	2.86%
101	234		0	234		0.94				
Total	43221		1600	41621	1595					
Title shows		4.304	Ha							
Whole site % pervious			3.70%							
Whole site % impervious			96.30%							
Zoning requires 2% AEP piped drainage and overland flow for 1% AEP storm events										
DSG require detention for 1% AEP event.										
Time of concentration										
Flow in overland swales & pipes										
Length of flow path < 1,000m										
CPAA Fig 2.10 with flow length 500m with 6m fall = 1.2% n= 0.045 yields						30 mins				
Bransby Williams										
L	0.5	km								
S	12	m/km								
A	4.304	Ha								
F	92.5									
Tc	24.32	Minutes								
Adopt 25 minutes										
2% AEP	46.6	mm/hr	Ex BOM Data Tab							
1% AEP	53.8	mm/hr	Ex BOM Data Tab							

Runoff coefficients										
Roofed and sealed areas			0.9							
Existing grass/effluent disposal/swales			0.4							
Post development flow rates										
Event	Coefficient	Intensity	Area ha	Flow l/s	Effective area					
2% AEP	0.4	46.6	0.3195	6.0	3.71385	Ha				
2% AEP	0.9	46.6	3.9845	167.1						
2% AEP	Totals		4.304	173.1						
1% AEP	0.4	53.8	0.3595	7.7						
1% AEP	0.9	53.8	3.9845	192.9						
1% AEP	Totals		4.344	200.7	total 1% AEP + CC	237.4	for overland flow			
Overland flow path is defined by the existing terrain which slopes from the Possum Road side to the embankment of the highway off ramp, which has capacity for more than the 1% AEP + CC flow (922 l/s as per below)										
Department of State Growth Design Basis for Brighton Bypass						**TRAPEZOIDAL DRAIN CAPACITY**				
Industrial land % impervious allowed				70%		Width (m) =	1			
Industrial land % pervious allowed				30%		Slope X:1v =	2			
DSG adopted C value impervious				0.9		Depth (m) =	0.3			
DSG adopted C value pervious				0.2		Slope (%) =	1.22			
						n Value =	0.03	Grass, some weeds		
Increased % impervious from subdivision development, so additional flow results						Area A	0.48			
						H Radius R	0.204984472			
DSG Allowance in culvert design						Flow l/s Q	**614**	Exceeds 237 l/s overland flow		
Area		43221	m2			Velocity m/s	1.28	and assumes zero pipe flow		
Effective C		0.69				Velocity Head	0.083505088			
25 minute Intensity 1% AEP		53.8	mm/hr	1.49444E-05	m/s					
DSG flow rate				0.446	m3/s					
				445.681	**l/s**	**Detention outflow rate**				

Developed subdivision									
Area	43221	m2							
Effective C	0.874				Effective Area =		37778.9	m2	
25 minute Intensity 1% AEP	53.8	mm/hr	1.49444E-05				3.77789	Ha	
Revised Flow Rate			0.565	m3/s					
			564.585	l/s					
Increased flow =			118.904	l/s					
Existing Pipe is 750 diam. At 0.5% grade.									
Mannings Capacity with n=0.012			917	l/s					
Change % of capacity			12.967						
The 4 new lots can detain runoff on site for the design storm to restrict the flow by 119 l/s min for the 25 minute 1% AEP event									
Total detention volume as per Detention tab =			**292**	**m3 over new lots**					
		or	97.33	m3 per lot for 3 lots (assuming Lot 1 stays as is)					
Could be for example a 100kl tank per lot subject to final design for each lot 2 to 4									

80 Possum Road Subdivision					
Stormwater Management					
JMG Ref:	**J230659CS**				
Detention Calculation					
Effective area =	3.77789	Ha			
Storm Duration	**1% AEP**	**Qi**	**Qo**	**V1**	**Smax**
(mins)	**Intensity (mm/hr)**	**(m3/s)**	**(m3/s)**	**(m3)**	**(m3)**
5	114	1.196	0.446	359	225
10	88.8	0.932	0.446	559	292
15	72.7	0.763	0.446	687	286
20	61.7	0.647	0.446	777	242
25	53.8	0.565	0.446	847	178
30	47.9	0.503	0.446	905	103
45	36.5	0.383	0.446	1034	-169
60	30	0.315	0.446	1133	-471
90	22.8	0.239	0.446	1292	-1115
120	18.9	0.198	0.446	1428	-1781
180	14.7	0.154	0.446	1666	-3147
270	11.6	0.122	0.446	1972	-5248
360	9.92	0.104	0.446	2249	-7378
540	7.99	0.084	0.446	2717	-11723
720	6.86	0.072	0.446	3110	-16143
1080	5.5	0.058	0.446	3740	-25140
1440	4.65	0.049	0.446	4216	-34291
Total volume required =		**292**	**m3 over new subdivision**		

80 Possum Road Bridgewater

IFD Design Rainfall Intensity (mm/h)

Issued:	29-Jul-24			
Location Label:	80 Possum Road			
Requested coordinate:	Latitude	-42.7125	Longitude	147.241
Nearest grid cell:	Latitude	42.7125 (S)	Longitude	147.2375 (E)

		Annual Exceedance Probability (AEP)						
Duration	**Duration in min**	**63.20%**	**50%**	**20%**	**10%**	**5%**	**2%**	**1%**
1 min	1	59.4	67.4	94.4	114	135	165	190
2 min	2	51.2	57.8	79	93.9	109	127	141
3 min	3	45.2	51.1	70.2	83.8	97.6	115	129
4 min	4	40.7	46.1	63.7	76.5	89.5	107	121
5 min	5	37.2	42.2	58.6	70.6	83	100	114
10 min	10	27	30.7	43.1	52.5	62.3	76.7	88.8
15 min	15	21.9	24.9	35	42.7	50.7	62.7	72.7
20 min	20	18.8	21.3	29.9	36.4	43.3	53.3	61.7
25 min	25	16.6	18.8	26.4	32.1	38	46.6	53.8
30 min	30	15	17	23.8	28.8	34.1	41.6	47.9
45 min	45	12	13.5	18.8	22.6	26.6	32.1	36.5
1 hour	60	10.2	11.5	15.9	19	22.2	26.6	30
1.5 hour	90	8.1	9.17	12.6	14.9	17.3	20.4	22.8
2 hour	120	6.9	7.8	10.7	12.6	14.5	17	18.9
3 hour	180	5.5	6.22	8.47	9.97	11.4	13.3	14.7
4.5 hour	270	4.37	4.96	6.74	7.92	9.05	10.5	11.6
6 hour	360	3.7	4.21	5.74	6.74	7.69	8.96	9.92
9 hour	540	2.91	3.32	4.54	5.35	6.12	7.18	7.99
12 hour	720	2.44	2.78	3.83	4.53	5.2	6.14	6.86
18 hour	1080	1.88	2.14	2.98	3.54	4.09	4.88	5.5
24 hour	1440	1.54	1.76	2.46	2.95	3.42	4.11	4.65
30 hour	1800	1.32	1.51	2.11	2.53	2.96	3.56	4.04
36 hour	2160	1.15	1.32	1.85	2.23	2.61	3.15	3.58
48 hour	2880	0.925	1.06	1.49	1.8	2.12	2.56	2.91
72 hour	4320	0.669	0.763	1.07	1.3	1.53	1.85	2.1
96 hour	5760	0.528	0.6	0.839	1.01	1.19	1.43	1.63
120 hour	7200	0.438	0.497	0.691	0.831	0.975	1.16	1.32
144 hour	8640	0.376	0.426	0.588	0.704	0.82	0.976	1.1
168 hour	10080	0.331	0.375	0.514	0.611	0.707	0.837	0.947

APPENDIX C

Concept Stormwater Plan

N

SOUTH RAIL LINE

RIGHT O F WAY 10.00 WIDE

RIGHT O F WAY 10.00 WIDE

12.50

SW1/6

LOT 3

SW1/7

R12.00

R20.60

12.50

FOR CUL DE SAC DETAILS
SEE DRG 46891NG-201

ROAD 1

9.00

FR156586/2

FOR KERB RETURN DETAILS
SEE DRG 46891NG-202

SW3/2

MATCH INTO EXISTING EDGE
OF BITUMEN OF POSSUM
ROAD CUL DE SAC

EXISTING POSSUM ROAD
CUL DE SAC

SW3/1

EXISTING STORMWATER HEADWALL
EX IL 58.343

TRANSITION INVERT LEVEL TO
EXISTING OPEN DRAIN INVERT LEVEL
WITH KERB FLAREOUT DETAIL SEE
SHEET 46981NG-003

EXISTING WATER
TANKS

LOT 1

LOT 2

FOR CONTINUATION SEE SHEET 46981NG-102

LEGEND:

- NEW STORMWATER
- EX. FENCE
- TOP/TOE BANK
- ASPHALT
- CONCRETE DRIVEWAY

NOTES:

1. FOR PROJECT NOTES SEE DRG 46891NG-002
2. FOR SW LONG SECTION SEE DRG 46891NG-400-403

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED: RD | REVIEWED: MW

DRAWN: RD | REVIEWED: MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

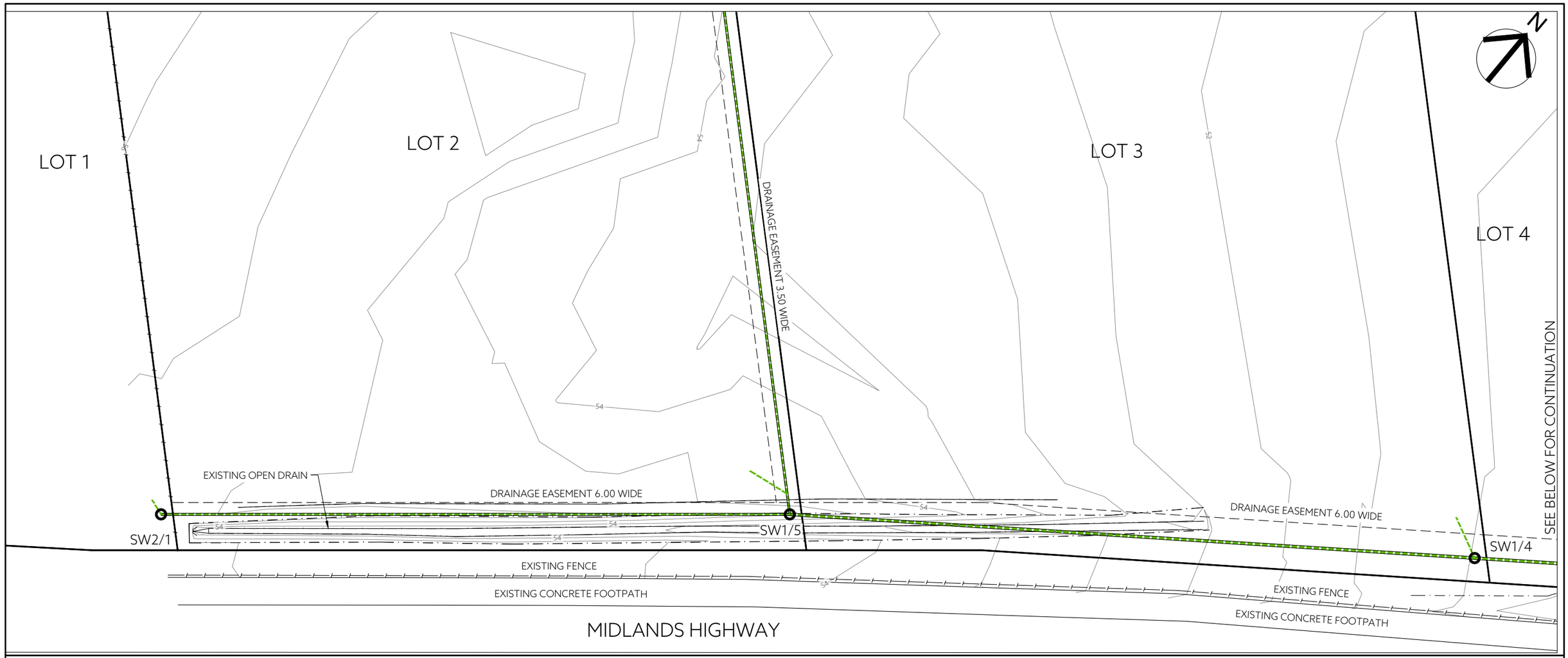
DRAWING TITLE: ROAD & STORMWATER DETAIL PLAN
SHEET 1 OF 2

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston,
Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	101	P

DATE/TIME: Wednesday, 14 August 2024 3:25:09 PM PLOTTED: ROWAN DEMMER FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG



LEGEND:

- NEW STORMWATER
- EX. FENCE
- TOP/TOE BANK
- ASPHALT
- CONCRETE DRIVEWAY

NOTES:

1. FOR PROJECT NOTES SEE DRG 46891NG-002
2. FOR SW LONG SECTION SEE DRG 46891NG-400-403

REV	AMENDMENTS	DRAWN	DATE	APPR.
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--
-	----	--	--/--/----	--

DRAWING STATUS: FOR APPROVAL

COORDINATE/ DATUM: GDA2020 GRID

THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED

DESIGNED:	REVIEWED:
RD	MW
DRAWN:	REVIEWED:
RD	MW

JOB MANAGER: CRAIG TERRY

ISSUED DATE: 25/07/2024

CLIENT: ONETRAK

PROJECT DESCRIPTION: 4 LOT SUBDIVISION

ADDRESS: 80 POSSUM ROAD

DRAWING TITLE: ROAD & STORMWATER DETAIL PLAN
SHEET 2 OF 2

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
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www.pda.com.au
Also at: Kingston, Launceston & Burnie

REGISTRATION NUMBER: ----

CONTRACT NO.	SCALE		PAPER
------	1: 500		(A3)
JOB NUMBER	DISCIPLINE	SHEET	REVISION
46981NG	C	102	P

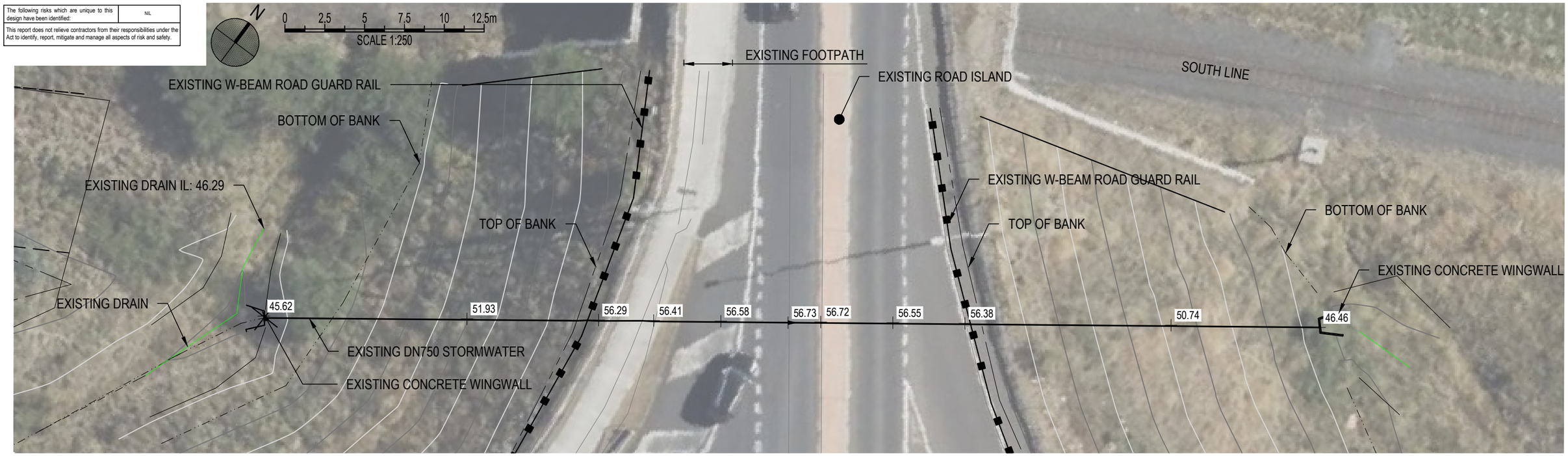
DATE/TIME: Wednesday, 14 August 2024 3:25:09 PM PLOTTED: ROWAN DEMMER FILE LOCATION: S:\46981NG - ONETRAK - 80 POSSUM ROAD, BRIDGEWATER\AUTOCAD DRAWINGS\ENG\46981NG-ENG.DWG

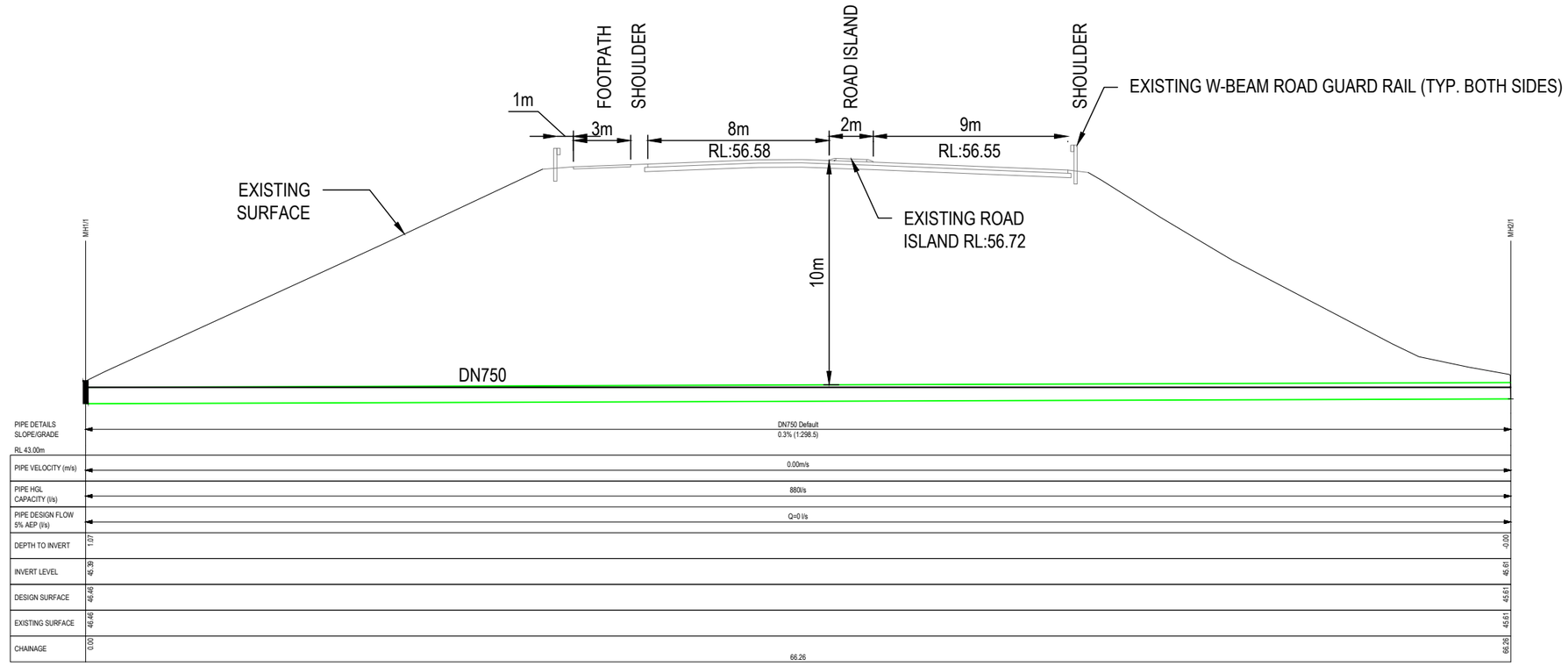
APPENDIX D

Existing Department of State Growth DN750 Culvert Profile

SAFETY IN DESIGN REPORT PER WHS REGULATIONS

The following risks which are unique to this design have been identified:	NIL
This report does not relieve contractors from their responsibilities under the Act to identify, report, mitigate and manage all aspects of risk and safety.	





EXISTING DN750 STORMWATER PROFILE

SCALE: 1:250

REV	DATE	REMARK
P1	07.04.2025	PRELIMINARY SKETCH

Accepted CJM (Discipline Head)	Date	
Accepted GLA (Team Leader)	Date	
Approved CJM (Principal)	Date	

This document must be signed "Approved" by JMG to authorise it for use. JMG accept no liability whatsoever for unauthorised or unlicensed use.

SCALES @ A3	DESIGNED BY	DRAWN BY
	CEF	CEF
	PLOT DATE	15/04/2025

DO NOT SCALE. Use only figured dimensions. Locations of structure, fittings, services etc on this drawing are indicative only. CONTRACTOR to check Architects & other project drawings for co-ordination between structure, fabric, fixtures, fittings, services etc. CONTRACTOR to site check all dimensions and exact locations of all items. JMG accepts no responsibility for dimensional information scaled or digitally derived from this document.

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PROJECT
80 POSSUM ROAD
BRIDGEWATER
SUBDIVISION

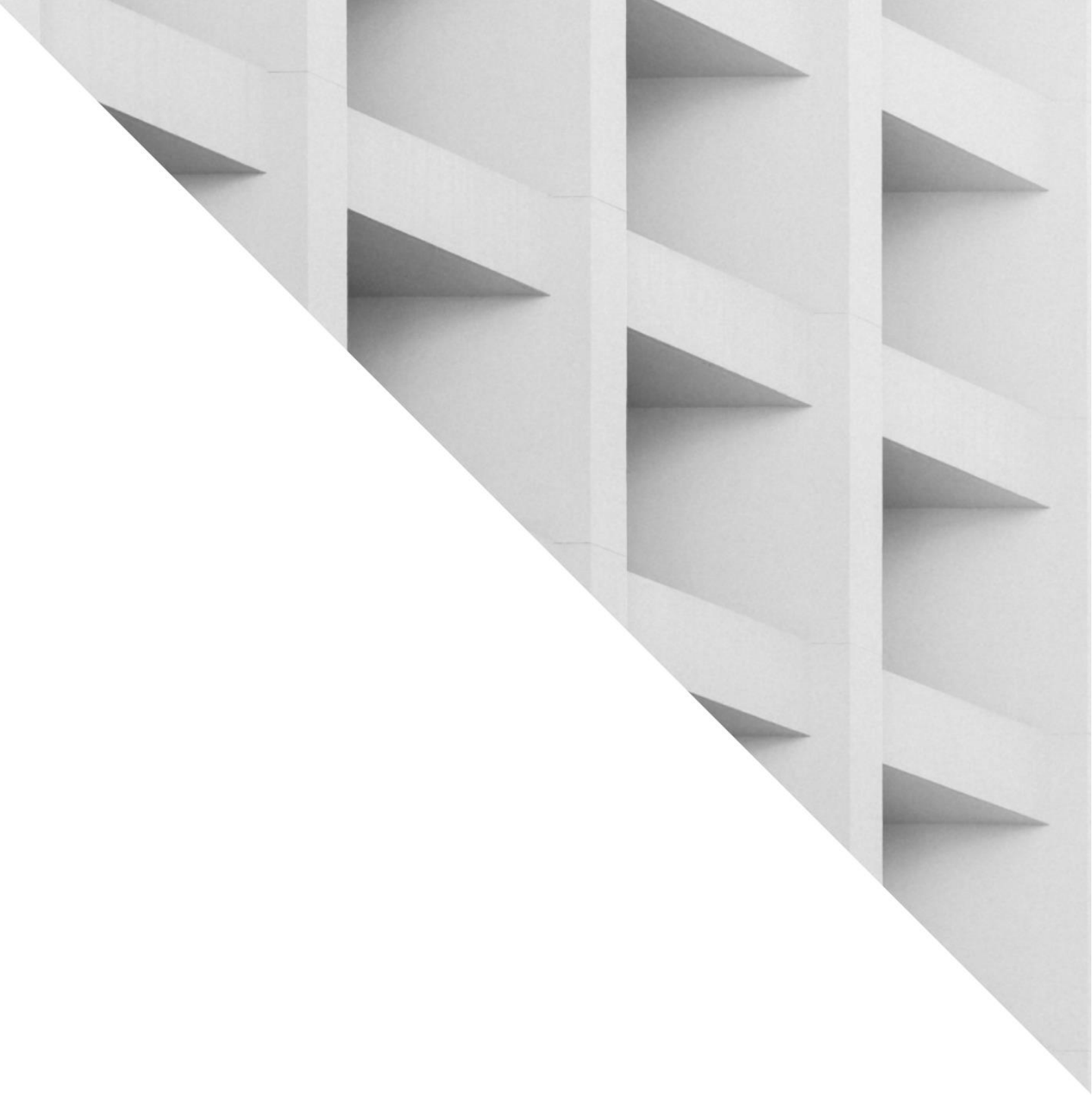
TITLE
EXISTING DN750
STORMWATER
PLAN & PROFILE

PROJECT NO. 230659CS

DWG NO. CS01

REVISION P1

PLOT DETAILS 230659CS - 3D BASE.DWG



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Hobart, Tas. 7000

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E info@hed-consulting.com.au

BUSHFIRE HAZARD REPORT &

BUSHFIRE HAZARD MANAGEMENT PLAN

SUBDIVSION – ONE LOT INTO SIX LOTS

80 POSSUM ROAD
BRIDGEWATER 7030

HAZELL BROS

12 AUGUST 2025 – VERSION 2.0

EXECUTIVE SUMMARY

The subject land is located at 80 Possum Road, Bridgewater (C.T. 159938/4). The development proposal includes a subdivision (one lot into five lots). Lot 1 has an existing building, Lots 2 – 4 vacant, Lot 100 (road). The proposed subdivision is assessed and deemed to comply with the requirements of C13.0 Bushfire-Prone Areas Code of the Tasmania Planning Scheme.

LIMITATIONS

This report is based on findings concluded from a desktop and field investigation of the subject property. Classification of vegetation has been based on the site inspection does not account for any further modification to the existing vegetation (planting, clearing etc.)

The assessment is based on information provided at the time of the report and location shown on the Bushfire Hazard Management Plan (BHMP). If the location of the proposed development (indicative building area) differs from the location shown on the BHMP a new assessment will be required.

The BAL assessment is based on the Fire Danger Index (FDI) of 50. The FDI will exceed 50 when the Australian Fire Danger Ratings System (AFDRS) level is Extreme or Catastrophic.

The forward of AS3959 – 2018, *Construction of buildings in bushfire prone areas* states that "It should be borne in mind that the measures contained in this standard cannot guarantee that a building will survive a bushfire event on every occasion. This is substantially due to the degree of vegetation management, the unpredictable nature and behaviour of fire, and extreme weather conditions."

Due to the unpredictable nature and behaviour of fire, compliance with AS359-2018 does not guarantee a dwelling will survive a bushfire event.

CONTENTS PAGE

1.0 INTRODUCTION

1.1 SCOPE

To assess the proposed subdivision against the requirements of C13.0 Bushfire-Prone Areas Code of the Tasmanian Planning Scheme.

1.2 PROPOSAL

Subdivision of one lot into five lots. The subdivision will create four lots that can support building works. This includes Lot 1 - 1.5ha±, Lot 2 - 9100m^2, Lot 3 - 9130m^2, Lot 4 - 9161m^2. Lot 100 - 681m^2 will be a part road (turning area).

1.3 GENERAL INFORMATION

SITE ADDRESS

80 Possum Road, Bridgewater

OWNER

Hazell Bros

TITLE REFERENCE

C.T. 159938/4

PROPERTY ID NUMBER

3511316

EXISITING PROPERTY SIZE

43080m^2

CURRENT USE

Industrial

MUNICPALITY

Brighton Council

2.0 SITE DESCRIPTION

2.1 LOCALITY AND PROPOSED SUBDIVISION

The subject land is located at 80 Possum Road, Bridgewater. The site is located at the end of Possum Road. The existing lot is surrounded by large mostly undeveloped lots with grassland and bushland. Developed industrial development exists further towards the south and south – west. A railway line is adjacent the north – west property boundary. The subdivision shall create five lots. Lot 1 will have road frontage to Possum Road (existing property access). Possum Road shall be extended by length of 129m. Two proposed Right of Ways shall be installed at the end of Possum Road to provide access for Lot 2, Lot 3, Lot 4. The proposed plan of subdivision is provided in the appendix of this report. Onetrak currently has a machinery workshop and offices located at the southern end of the existing lot and will be located within Lot 1.

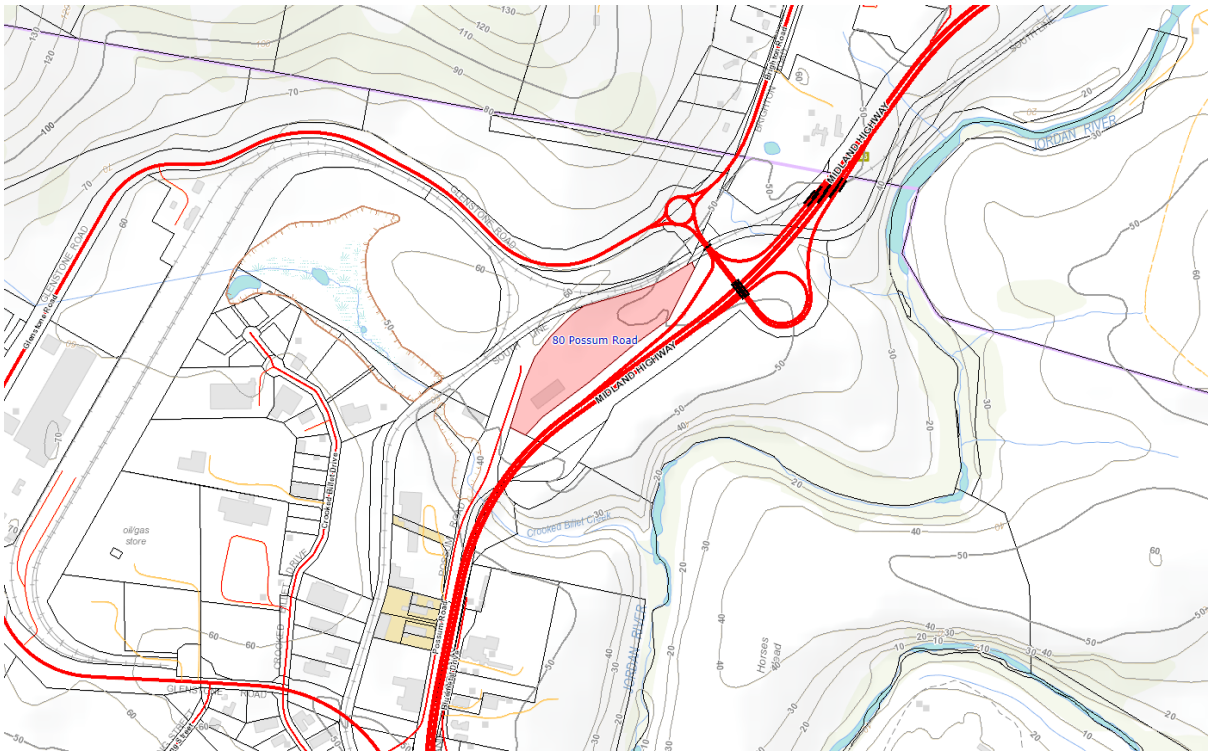


Figure 1: Locality map of the area with proposed subdivision shown (red highlight). Source: Land Information System Tasmania, http://www.thelist.tas.gov.au

2.1.2 FIRE HISTORY

Recent bushfire and / or planned burns were identified within 1km of the property boundaries. Data collected from LIST Map 'Fire History Layer'[1].

Ignition date	Fire / Planned burn name	Type	Size	Distance to site
21/1/2003	Broadmarsh-Bluff Rd (TFS)	Bushfire	14345 Ha.	80m
7/2/1967	1967 Fire	Bushfire	198781 Ha.	425m

[1] LIST Map Data is incomplete, and majority of fire history is not shown on the LIST.

2.1.2 PLANNING – ZONING & TENURE

The lot is zoned as General Industrial and is privately owned. Zoning and tenure of surrounding lots is shown below (within 100m from the existing property boundaries).

Direction	Zoning	Tenure
North	Utilities	Authority Crown & Public Reserve
East	Utilities & Rural	Public Reserve
South	Utilities	Public Reserve
West	Utilities, Industrial & Environmental Management	Authority Crown & Public Reserve

2.1.3 PLANNING – OVERLAYS

Overlay	Code	Development Response
Bushfire-prone areas	Bushfire-prone Areas	The Bushfire Hazard Report and Bushfire Hazard Management Plan (BHMP) satisfy the requirements of this code.
Attenuation area	Attenuation	The provisions of the BHMP do not conflict with the requirements of this code.
Waterway and coastal protection area	Natural Assets	The provisions of the BHMP do not conflict with the requirements of this code.

2.1.4 PLANNING – THREATENED FLORA AND FAUNA

A threatened flora and fauna search[2] revealed threatened flora and fauna as below.

Common name	Species name	Development response
Eastern barred bandicoot	Perameles gunnii	The fauna point is located on the north-west property boundary, 25m north of the existing property access to proposed Lot 1. This area is not within a proposed Hazard Management Area and no vegetation removal is required. Some vegetation clearing will be required construct the road and turning area near this fauna point.
Variable raspwort	Haloragis heterophylla	Located in proposed lot 2. The HMA requires maintaining a minimal fuel condition which includes mowing the grass to a nominal height of 100mm. If required, further advice should be sought from a suitably qualified ecologist / botanist regarding the impact of the proposed development will on this specie.

[2] Threatened species search using Land Information Systems Tasmania. This is not a complete search and other information may be available from other agencies.

Milky beautyheads	Calocephalus lacteus	As above
Doublejointed speargrass	Austrostipa bigeniculata	As above

2.2 TOPOGRAPHY AND VEGETATION

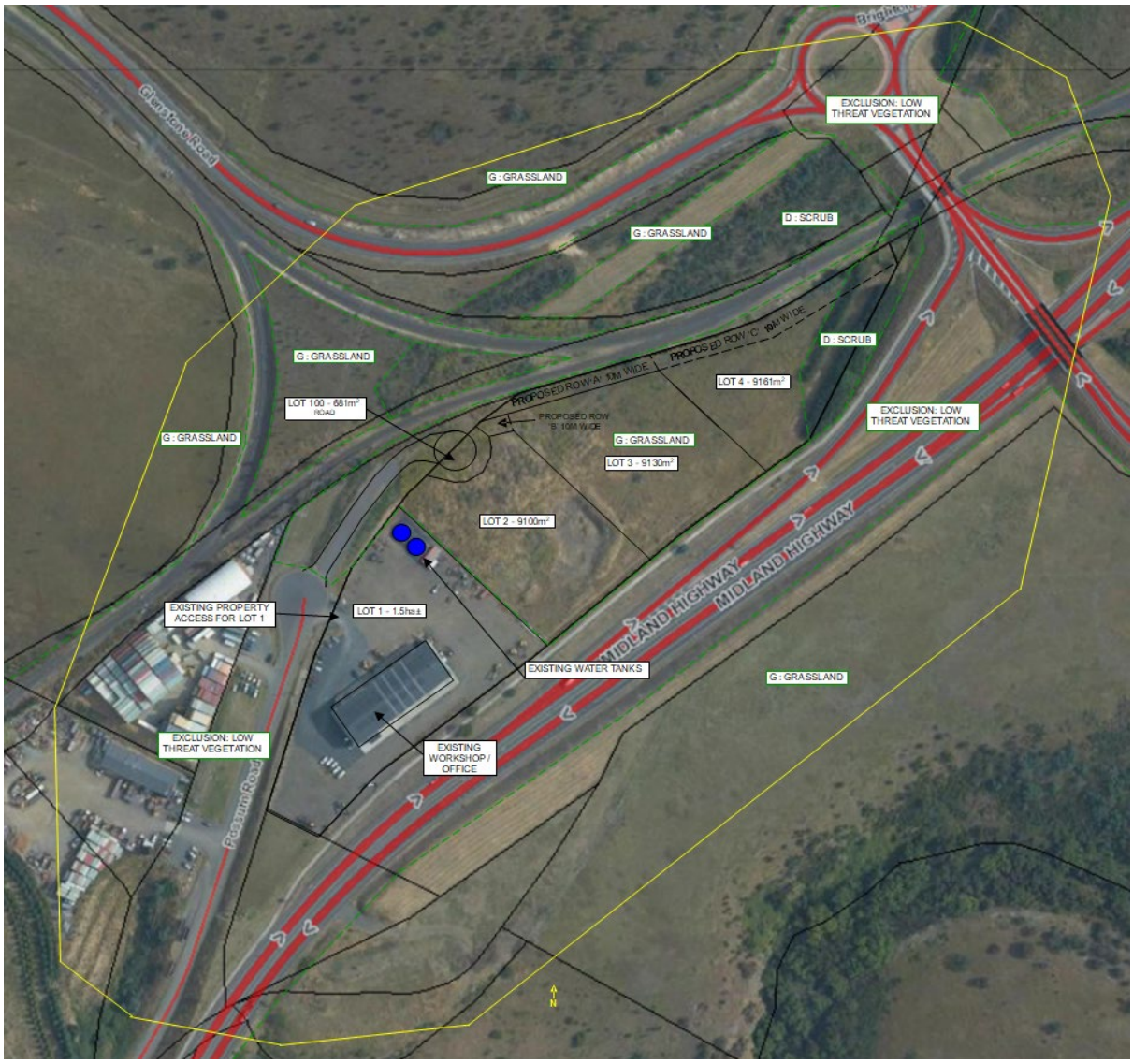


Figure 2: Aerial photo of the area showing the proposed subdivision. The yellow line is approximately 140m from the edge of the existing lot. Green dashed line shows borders between classified vegetation and exclusions shown. Source: Land Information System Tasmania, http://www.thelist.tas.gov.au.

TASVEG 4.0 community FAG – Agricultural Land cover the entire existing property and all proposed lots.

TASVEG 4.0 communities within 140m from existing property boundaries is shown below.

Direction	**TAS Veg 4.0 Description**
North	FAG – Agricultural land, FPE – Permanent easements (modified land) &

	GTL – Lowland Themeda triandra grassland
East	FAG – Agricultural land & FPE - Permanent easements (modified land)
South	FAG – Agricultural land, FPE - Permanent easements (modified land) & NBA – Bursaria acacia woodland
West	FAG – Agricultural land & FUR – Urban areas

Figure 3: Aerial photo of the area showing TAS Veg 4.0 communities surrounding the existing property. Source: Land Information System Tasmania, http://www.thelist.tas.gov.au.

Direction, vegetation types, exclusion and effective slope are shown below for proposed lots 1 to 4. Lot 101 (public open space) and Lot 100 (road) have not been assessed as the use of these lots are not for building works.

Lot 1 (measured from existing building):

Direction	Existing Vegetation Description	Effective slope
North - east	0-48m: Gravel area with no vegetation. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (e) of AS3959:2018.** 48-140m: Dense pasture with isolated shrubs. Over storey foliage cover is less than 10%. **Classified vegetation: G: Grassland**	0 – 5^{o} downslope
South - east	0-20m: Gravel area and hardstand area. Garden area exists near boundary to highway. Vegetation is assessed as low threat. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (e) & (f) of AS3959:2018.** 20-40m: Concreate pathway, road cutting and narrow strip of grass vegetation. Vegetation is too narrow to increase the severity of a bushfire attack. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (d) & (e) of AS3959:2018.** 40-80m: Midland highway and road cutting. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) of AS3959:2018.** 80-105m: Dense pasture, periodically slashed. **Classified vegetation: G: Grassland** 105-140m+: Dense pasture, grazing paddock. **Classified vegetation: G: Grassland**	0^{o} / Upslope 0^{o} / Upslope
South - west	0-140m+: Gravel and hardstand area on subject lot. Narrow vegetation strip along Possum Road is managed by Brighton Council and kept is minimal fuel condition. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) & (f) of AS3959:2018.**	
North - west	0-120m: Gravel and hardstand area on subject lot. Narrow vegetation strip along Possum Road is managed by Brighton Council and kept is minimal fuel condition.	

	Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) & (f) of AS3959:2018. 120-140m+: Over storey of wattle trees with height less than 6m. Over storey foliage cover is estimated to be <10%. Dominant grassy under storey with isolated shrubs. **Classified vegetation: G: Grassland**	0° / Upslope

Lot 2 (measured from indicative building area):

Direction	Existing Vegetation Description	Effective slope
North - east	0-140m+: Dense pasture with isolated shrubs. Over storey foliage cover is less than 10%. **Classified vegetation: G: Grassland**	0 – 5° downslope
South - east	0-30m: Dense pasture with isolated shrubs. Over storey foliage cover is less than 10%. **Classified vegetation: G: Grassland** 30-50m: Concreate pathway, road cutting and narrow strip of grass vegetation. Vegetation is too narrow to increase the severity of a bushfire attack. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (d) & (e) of AS3959:2018.** 50-90m: Midland highway and road cutting. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) of AS3959:2018.** 90-140m+: Dense pasture, grazing paddock. **Classified vegetation: G: Grassland**	0° / Upslope 0° / Upslope
South - west	0-20m: Dense pasture. **Classified vegetation: G: Grassland** 20-140m+: Gravel and hardstand area on adjacent lot. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) & (f) of AS3959:2018.**	0° / Upslope
North - west	0-40m: Dense pasture. **Classified vegetation: G: Grassland**	0° / Upslope

	40-60m: Gravel and hardstand area. Narrow vegetation strip along Possum Road is managed by Brighton Council and kept is minimal fuel condition. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) & (f) of AS3959:2018.** 60-70m: Railway line. Vegetation periodically maintained along line. Verges have small wattles and grassy understorey. **Classified vegetation: G: Grassland** 70-100m: Wattle trees with height of 6m. Grassy under storey. **Classified vegetation: D: Scrub** 100-140m: Over storey of wattle trees with height less than 6m. Over storey foliage cover is estimated to be <10%. Dominant grassy under storey with isolated shrubs. **Classified vegetation: G: Grassland**	0° / Upslope 0° / Upslope 0° / Upslope

Lot 3 (measured from indicative building area):

Direction	Existing Vegetation Description	Effective slope
North - east	0-75m: Dense pasture with isolated shrubs. Over storey foliage cover is less than 10%. **Classified vegetation: G: Grassland** 75-140m: Trees with 8m height. Grassy under storey. **Classified vegetation: D: Scrub**	0 – 5° downslope 0 – 5° downslope
South - east	0-20m: Dense pasture. **Classified vegetation: G: Grassland** 20-40m: Concreate pathway, road cutting and narrow strip of grass vegetation. Vegetation is too narrow to increase the severity of a bushfire attack. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (d) & (e) of AS3959:2018.** 40-80m: Midland highway and road cutting. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) of AS3959:2018.** 80-140m+: Dense pasture, grazing paddock. **Classified vegetation: G: Grassland**	0° / Upslope 0° / Upslope

South - west	0-140m: Dense pasture. **Classified vegetation: G: Grassland**	 0º / Upslope
North - west	0-40m: Dense pasture. **Classified vegetation: G: Grassland** 40-50m: Wattle trees. Over storey foliage cover less than 10%. Dominant grassy under storey. Includes railway line. **Classified vegetation: G: Grassland** 50-100m: Wattle trees with height less than 6m. Foliage cover is 10-30%. Grassy under storey. **Classified vegetation: D: Scrub** 100-140m: Dense pasture with isolated trees. **Classified vegetation: G: Grassland**	 0º / Upslope 0º / Upslope 0º / Upslope 0º / Upslope

Lot 4 (measured from indicative building area):

Direction	Existing Vegetation Description	Effective slope
North - east	0-22m: Dense pasture with isolated shrubs. Over storey foliage cover is less than 10%. **Classified vegetation: G: Grassland** 22-80m: Trees with 8m height. Grassy under storey. **Classified vegetation: D: Scrub** 80-140m: Roadway and road reserve. Vegetation is managed with grass kept to short height. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 (d) & (e) of AS3959:2018.**	 0 – 5º downslope 0 – 5º downslope
South - east	0-19m: Dense pasture. **Classified vegetation: G: Grassland** 19-31m: Trees with 8m height. Grassy under storey. **Classified vegetation: D: Scrub** 15-35m: Concreate pathway, road cutting and narrow strip of grass vegetation. Vegetation is too narrow to increase the severity of a bushfire attack.	 0º / Upslope 0º / Upslope

	Exclusion: Low threat vegetation as per clauses 2.2.3.2 (d) & (e) of AS3959:2018. 35-75m: Midland highway and road cutting. **Exclusion: Low threat vegetation as per clauses 2.2.3.2 & (e) of AS3959:2018.** 75-140m: Dense pasture. **Classified vegetation: G: Grassland**	0^{o} / Upslope
South - west	0-140m: Dense pasture. **Classified vegetation: G: Grassland**	0^{o} / Upslope
North - west	0-20m: Dense pasture with isolated wattle trees. Over storey foliage cover less than 10%. **Classified vegetation: G: Grassland** 20-35m: Railway line. **Exclusion: Non – vegetated area as per clause 2.2.3.2 (e).** 35-75m: Wattle trees with height less than 6m. Foliage cover is 10-30%. Grassy under storey. **Classified vegetation: D: Scrub** 75-95m: Dense pasture with isolated trees. **Classified vegetation: G: Grassland** 95-110: Wattle trees with height less than 6m. Foliage cover is 10-30%. Grassy under storey. **Classified vegetation: D: Scrub** 110-130m: Roadway and road reserve. **Exclusion: Low threat vegetation and non – vegetated area as per clause 2.2.3.2 (e) & (f).** 130-140m: Wattle trees with height less than 8m. Over storey foliage cover less than 10%. **Classified vegetation: G: Grassland**	0^{o} / Upslope 0^{o} / Upslope 0^{o} / Upslope 0^{o} / Upslope

3.0 BUSHFIRE SITE ASSESSMENT

3.1 EXISTING BUSHFIRE HAZARD ASSESSMENT

3.2.1 CONSTRUCTION Lot 1: Existing workshop and office for Onetrak. Steel cladding and metal sheet roof. Lots 2-4: Vacant.
3.2.2 PROPERTY ACCESS Lot 1: The lot is hardstand with property access directly from Possum Road. Lots 2 – 4: No existing property access.
3.2.3 WATER SUPPLY All lots: No reticulated water supply. Lot 1 has two large tanks (approximately 140,000 litres) used for firefighting.
3.2.4 HAZARD MANAGEMENT AREA Lot 1: Buildings and hardstand area. A small garden exists between the existing building and the south – east property boundary. Lots 2 – 4: No existing hazard management area. Grass is periodically cured.

3.2 BUSHFIRE ATTACK LEVEL ASSESSMENT

Lot 1 (existing building):

	North - east	South - east	South - west	North - west
Vegetation classification as per AS3959:2018	Grassland	Grassland	NA	NA
Exclusions (where applicable from clause 2.2.3.2 of AS3959 - 2018)				
Distance to classified vegetation (m) from edge of existing building	48	80	>100	>100
Classified vegetation	Grassland	Grassland	NA	NA
Effective slope under the classified vegetation	Down slope >0° to 5°	Upslope / 0°	NA	NA
Minimum separation distance to achieve BAL – 19.	**11m**	**0m**	**0m**	**0m**
BAL - 19 HMA can be achieved within property boundaries	**Yes**	**Yes**	**Yes**	**Yes**

Lot 2

	North - east	South - east	South - west	North - west
Vegetation classification as per AS3959:2018	Grassland	Grassland	Grassland	Grassland & Scrub
Exclusions (where applicable from clause 2.2.3.2 of AS3959 - 2018)				
Distance to classified vegetation (m) from indicative building area	0	0	0	Grassland – 0 Scrub – 70
Classified vegetation	Grassland	Grassland	Grassland	Grassland
Effective slope under the classified vegetation	Down slope >0° to 5°	Upslope / 0°	Upslope / 0°	Upslope / 0°
Minimum separation distance to achieve BAL – 19.	**11m**	**10m**	**10m**	**10m**
BAL - 19 HMA can be achieved within property boundaries	**Yes**	**Yes**	**Yes**	**Yes**

Lot 3

	North - east	South - east	South - west	North - west
Vegetation classification as per AS3959:2018	Grassland & Scrub	Grassland	Grassland	Grassland & Scrub
Exclusions (where applicable from clause 2.2.3.2 of AS3959 - 2018)				
Distance to classified vegetation (m) from indicative building area	Grassland – 0 Scrub - 75	0	0	Grassland – 0 Scrub - 50
Classified vegetation	Grassland	Grassland	Grassland	Grassland
Effective slope under the classified vegetation	Down slope >0° to 5°	Upslope / 0°	Upslope / 0°	Upslope / 0°
Minimum separation distance to achieve BAL – 19.	**11m**	**10m**	**10m**	**10m**
BAL - 19 HMA can be achieved within property boundaries	**Yes**	**Yes**	**Yes**	**Yes**

Lot 4

	North - east	South - east	South - west	North - west
Vegetation classification as per AS3959:2018	Grassland & Scrub	Grassland & Scrub	Grassland	Grassland & Scrub
Exclusions (where applicable from clause 2.2.3.2 of AS3959 - 2018)				
Distance to classified vegetation (m) from indicative building area	Grassland – 0 Scrub - 22	Grassland – 0 Scrub - 19	0	Grassland – 0 Scrub - 35
Classified vegetation	Grassland	Scrub	Grassland	Grassland
Effective slope under the classified vegetation	Down slope >0° to 5°	Upslope / 0°	Upslope / 0°	Upslope / 0°
Minimum separation distance to achieve BAL – 19.	**22m**	**19m**	**10m**	**10m**
BAL - 19 HMA can be achieved within property boundaries	**Yes**	**Yes**	**Yes**	**Yes**

If the minimum setback distance between the existing building for lot 1 and indicative building area for lots 2 – 4 and the classified vegetation is maintained, the bushfire attack level is assessed as BAL – 19. The assessment is based on a FDI of 50. The FDI will exceed 50 when the AFDRS is Extreme or Catastrophic.

4.0 PLANNING SCHEME COMPLIANCE

The following bushfire hazard management requirements required to comply with C13.0 Bushfire-Prone Areas Code.

C13.5 Use Standards

C13.5.1 Vulnerable uses

Objective:
That vulnerable uses can only be located on land within a bushfire-prone area where tolerable risks are achieved through mitigation measures that take into account the specific characteristics of both the vulnerable use and the bushfire hazard.
Acceptable Solutions **A1** No acceptable solution
Performance Criteria **P1** A vulnerable use must only be located in a bushfire-prone area it a tolerable risk from bushfire can be achieved and maintained, having regard to: (a) the location, characteristics, nature and scale of the use; (b) whether there is an overriding benefit to the community; (c) whether there is no suitable alternative lower-risk site; (d) the emergency management strategy (vulnerable use) and bushfire hazard management plan; (e) other advice, if any, from the TFS
Development response Lot 1 is not vulnerable use thus P1 is not applicable. Lot 2 – 4 are vacant and proposed use is unknown. Lot 100 is not vulnerable use.
A2 An emergency management strategy (vulnerable use) is endorsed by the TFS or accredited person.
P2 No Performance Criterion.

Development Response Lot 1 is not vulnerable use thus P1 is not applicable. Lot 2 – 4 are vacant and proposed use is unknown. Lot 100 is not vulnerable use.
A3 A bushfire hazard management plan that contains appropriate bushfire protection measures that is certified by the TFS or an accredited person.
P3 No Performance Criterion.
Development Response Lot 1 is not vulnerable use thus P1 is not applicable. Lot 2 – 4 are vacant and proposed use is unknown. Lot 100 is not vulnerable use.

C13.5.2 Hazardous uses

Objective:
That hazardous uses can only be located on land within a bushfire-prone area where tolerable risks are achieved through mitigation measures that take into account the specific characteristics of both the hazardous use and the bushfire hazard.
Acceptable Solutions **A1** No acceptable solution
Performance Criteria **P1** A hazardous use must only be located in a bushfire-prone area it a tolerable risk from bushfire can be achieved and maintained, having regard to: (a) the location, characteristics, nature and scale of the use; (b) whether there is an overriding benefit to the community; (c) whether there is no suitable alternative lower-risk site; (d) the emergency management strategy (vulnerable use) and bushfire hazard management plan; (e) other advice, if any, from the TFS
Development response Lot 1 – Onetrak does not have a hazardous chemical register. If hazardous chemicals are stored on the lot are of manifest quantity (as defined by the Tasmanian Planning Scheme) compliance to P1 will need to be demonstrated. Lot 2 – 4 are vacant and proposed use is unknown. Lot 100 is not hazardous use.
A2 An emergency management strategy (hazardous use) is endorsed by the TFS or accredited person.
P2 No Performance Criterion.
Development Response Lot 1 – Onetrak does not have a hazardous chemical register. If hazardous chemicals are stored on the property that are of manifest quantity (as defined by the Tasmanian Planning Scheme) compliance to A2 will need to be demonstrated. Lot 2 – 4 are vacant and proposed use is unknown. Lot 100 is not hazardous use.
A3 A bushfire hazard management plan that contains appropriate bushfire protection measures that is certified by the TFS or an accredited person.

P3

No Performance Criterion.

Development Response

Lot 1 – Onetrak does not have a hazardous chemical register. If hazardous chemicals are stored on the lot are of manifest quantity (as defined by the Tasmanian Planning Scheme) compliance to A3 will need to be demonstrated.

Lot 2 – 4 are vacant and proposed use is unknown.

Lot 100 is not hazardous use.

C13.6 Development Standards for Subdivision

C13.6.1 Subdivision: Provision of hazard management areas

Objective:

That subdivision provides for hazard management areas that:

(a) facilitate an integrated approach between subdivision and subsequent buildings on a lot;
(b) provide for sufficient separation of building areas from bushfire-prone vegetation to reduce radiant heat levels, direct flame attack and ember attack at the building area; and
(c) provide protection for lots at any stage of a staged subdivision.

Acceptable Solutions

A1

(a) TFS or an accredited person certifies that there is an insufficient increase in risk from bushfire to warrant the provision of hazard management areas as part of a subdivision; or

(b) The proposed plan of subdivision:
  (i) shows all lots that are within or partly within a bushfire-prone area, including those developed at each stage of a stage subdivision.
  (ii) shows the building area for each lot;
  (iii) shows hazard management areas between bushfire-prone vegetation and each building area that have dimensions equal to, or greater than, the separation distances required for BAL 19 in Table 2.6 of *Australian Standard* AS 3959:2018 *Construction of buildings in bushfire-prone areas*; and
  (iv) is accompanied by a bushfire hazard management plan that address all the individual lots that is certified by the TFS or accredited person, showing hazard management areas equal to, or greater than, the separation distances required for BAL 19 in Table 2.6 of *Australian Standard* AS 3959:2018 *Construction of buildings in bushfire-prone areas*; and

(c) If hazard management areas are to be located on land external to the proposed subdivision the application is accompanied by the written consent of the owner of that land to enter into an agreement under section 71 of the Act that will be registered on the title of the neighbouring property providing for the affected land to be managed in accordance with the bushfire hazard management plan.

Performance Criteria

A proposed plan of subdivision shows adequate hazard management areas in relation to the building areas shown on lots within a bushfire-prone area, having regard to:

(a) the dimensions of hazard management areas;

(b) a bushfire risk assessment of each lot at any stage of staged subdivision;

(c) the nature of the bushfire-prone vegetation including type, fuel load, structure and flammability;

(d) the topography, including site slope;

(e) any other potential forms of fuel and ignition source;

(f) separation distances from the bushfire-prone vegetation not unreasonably restricting subsequent development;

(g) an instrument that will facilitate management of fuels located on land external to the subdivision;

(h) any advice from the TFS.

Development response

The Bushfire Hazard Report and BHMP satisfies the requirements of C13.6.1 A1(b) for Lots 1 to 4.

Lot 1 shall continue to be managed as a hazard management area.

Lots 2-4: shall have the Hazard Management Area installed when future building works begin on the subject lot.

C13.6.2 Subdivision: Public and firefighting access

Objective:

That access roads to, and the layout of roads, tracks and trails, in a subdivision:

(a) allow safe access and egress for residents, fire fighters and emergency service personnel;

(b) provide access to the bushfire-prone vegetation that enables both property to defend when under bushfire attack and for hazard management works to be undertaken;

(c) are designed and constructed to allow for fire appliances to be manoeuvred;

(d) provide access to water supplies for fire appliances; and

(e) are designed to allow connectivity, and where needed, offering multiple evacuation points.

Acceptable Solutions

A1

(a) TFS or an accredited person certifies that there is an insufficient increase in risk from bushfire to warrant specific measures for public access in the subdivision for the purposes of fire fighting; or

(b) A proposed plan of subdivision showing the layout of roads, fire trails and the location of property access to building areas is included in a bushfire hazard management plan that:

(i) demonstrates proposed roads will comply with Table C13.1, proposed property accesses will comply with Table C13.2 and proposed fire trails will comply with Table C13.3; and

(ii) is certified by the TFS or an accredited person.

Performance Criteria

P1

A proposed plan of subdivision shows access and egress for residents, fire-fighting vehicles and emergency service personnel to enable protection from bushfires, having regard to:

(a) appropriate design measures, including

(i) two – way traffic;
(ii) all weather construction;
(iii) height and width of any vegetation clearances;
(iv) load capacity
(v) provision of passing bays;
(vi) traffic and control devices;
(vii) geometry, alignment and slope of roads, tracks and trails;
(viii) use of through roads to provide for connectivity;
(ix) limits on the length of cul-de-sacs and dead-end roads;
(x) provision of turning areas;
(xi) provision of parking areas;
(xii) perimeter access; and
(xiii) fire trails;

(b) the provision of access to:

(i) bushfire-prone vegetation to permit the undertaking of hazard management works; and
(ii) fire fighting water supplies; and

(c) any advice from the TFS.

Development response

The Bushfire Hazard Report and BHMP satisfies the requirements of C13.6.2 A1(b) for Lots 1 to 4.

Proposed Possum Road extension and turning circle shall comply with Table C13.1. No fire trails are proposed. The road and turning area shall be constructed prior to sealing of final plan.

Existing property access for Lot 1 complies with Table C13.2.

Lot 2 to 4 property access to be designed and constructed to comply with Table C13.2 when future building works are undertaken.

Crossover for Lot 2 - 4 should be a minimum 4m carriageway width. Crossover shall be constructed prior to sealing of final plan.

Lot 100 will form part of the compliant turning circle to Table C13.1

Table C13.1 Standards for Roads

Element		**Requirement**
A.	Property access length is 30m or greater; or access is required for a fire appliance to a fire fighting water point.	Unless the development standards in the zone require a higher standard, the following apply: (a) two-wheel drive, all – weather construction (b) load capacity of at least 20 tonnes, including for bridges and culverts; (c) minimum carriageway width is 7m for a through road, or 5.5m for a dead-end or cul-de-sac road; (d) minimum vertical clearance of 4m; (e) minimum horizontal clearance of 2m from the edge of the carriageway; (f) cross falls of less than 3 degrees (1:20 or 5%); (g) maximum gradient of 15 degrees (1:3.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads; (h) curves with a minimum inner radius of 10m; (i) dead-end or cul-de-sac roads are not more than 200m in length unless the carriageway is 7m in width; (j) dead-end or cul-de-sac roads have a turning circle with a minimum 12m outer radius; and (k) carriageways less than 7m width have 'No Parking' zones on one side, indicated by a road sign that complies with *Australian Standard AS1743:2018 Road signs-Specifications*

Development response

Property road extension for Possum Road and turning circle shall comply with Table C13.1 or unless the development standards for general industrial require a higher standard (LGAT Standard Drawings TSD-R06 Table 2). The road shall be constructed prior to sealing of final plan.

Table C13.2 Standards for Property Access

Element		Requirement
A.	Property access length is less than 30m; or access is not required for a fire appliance to access a firefighting water point	There are no specified design and construction requirements.
B.	Property access length is 30m or greater; or access is required for a fire appliance to a fire fighting water point.	The following design and construction requirements apply to property access: (l) all – weather construction (m) load capacity of at least 20t, including bridges and culverts; (n) minimum carriageway width of 4m; (o) minimum vertical clearance of 4m; (p) minimum horizontal clearance of 0.5m from the edge of the carriageway; (q) cross falls of less than 3 degrees (1:20 or 5%); (r) dips less than 7 degrees (1:8 or 12.5%) entry and exit angle; (s) curves with a minimum inner radius of 10m; (t) maximum gradient of 15 degrees (13.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads; and (u) terminate with a turning area for fire appliances provided by one of the following: (j) a turning circle with a minimum outer radius of 10m; or (ii) a property access encircling the building; or (iii) a hammerhead 'T' or 'Y' turning head 4m wide and 8m long.
C.	Property access length is 200m or greater.	The following design and construction requirements apply to property access: (a) the requirements for B above; and (b) passing bays of 2m additional carriageway width and 20m length provided every 200m.
D.	Property access length is greater than 30m, and access is provided to 3 or more properties	The following design and construction requirements apply to property access: (a) the requirements for B above; and (b) passing bays of 2m additional carriageway width and 20m length provided every 100m.
Development response Existing property access for lot 1 complies with the requirements of Table C13.2.		

Property access for lots 2 to 4 shall comply with the requirements of Table C13.2. Property access and design shall be constructed when future building works are undertaken.

C13.6.3 Subdivision: Provision of water supply for fire fighting purposes

Objective:

That an adequate, accessible and reliable water supply for the purposes of fire fighting can be demonstrated at the subdivision stage and allow for protection of life and property associated with the subsequent use and development of bushfire-prone areas.

Acceptable Solutions	**Performance Criteria**
A1 In areas serviced with reticulated water by the water corporation: (a) TFS or an accredited person certifies that there is an insufficient increase in risk from bushfire to warrant the provision of a water supply for fire fighting purposes; (b) A proposed plan of subdivision showing the layout of fire hydrants, and building areas, is included in a bushfire hazard management plan approved by TFS or accredited person as being compliant with Table E4; or (c) A bushfire hazard management plan certified by the TFS or an accredited person demonstrates that the provision of water supply for fire fighting purposes is sufficient to manage the risks to property and lives in the event of a bushfire	**P1** No Performance Criterion.
A2 In areas that are not serviced by reticulated water by the water corporation: (a) The TFS or an accredited person certifies that there is insufficient increase in risk from bushfire to warrant provision of a water supply for fire fighting purposes; (b) The TFS or an accredited person certifies that a proposed plan of subdivision demonstrates that a static water supply, dedicated to fire fighting, will be provided and located compliant with Table E5; or (c) A bushfire hazard management plan certified by the TFS or an accredited person demonstrates that the provision of water supply for fire fighting purposes is sufficient to manage the risks to property and lives in the event of a bushfire.	**P2** No Performance Criterion.

Development response

The area is not serviced by reticulated water by the water corporation.

Lot 1: Existing static water supply does not comply with C13.6.3 A2 (b). Lot 1 requires the installation of the static water supply compliant to Table C13.5 this will demonstrate compliance to C13.6.3 A2 (b). Static water supply shall be installed prior to sealing of final plan.

Lots 2 to 4 require the installation of the static water supply compliant to Table C13.5 this will demonstrate compliance to C13.6.3 A2 (b). Static water supply shall be installed when future building works are undertaken.

Table C13.5 Static water supply for fire fighting

Element		Requirement
A.	Distance between building area to be protected and water supply.	The following requirements apply: (a) the building area to be protected must be located within 90m of the fire fighting water point of a static water supply; and (b) the distance must be measured as a hose lay, between the fire fighting water point and the furthest part of the building area.
B.	Static Water Supplies	A static water supply: (a) may have a remotely located offtake connected to the static water supply; (b) may be supplied for combined use (fire fighting and other uses) but the specified minimum quantity of fire fighting water must be available at all times; (c) must be a minimum 10,000L per building area to be protected. This volume of water must not be used for any other purpose including fire fighting sprinkler or spray systems; (d) must be metal, concrete or lagged by non-combustible material if above ground; and (e) if a tank can be located so it is shielded in all directions in compliance with section 3.5 of Australian Standard AS 3959:2018 Construction of buildings in bushfire-prone areas, the tank may be constructed of any material provided that the lowest 400mm of the tank exterior is protected by: (i) metal; (ii) non-combustible material; or (iii) fibre-cement a minimum of 6mm thickness.
C.	Fittings, pipework and accessories (including stands and tank supports)	Fittings and pipework associated with a fire fighting water point for a static water supply must: (a) have a minimum nominal internal diameter of 50mm; (b) be fitted with a valve with a minimum nominal internal diameter of 50mm; (c) be metal or lagged by non-combustible materials if above ground (d) if buried, have a minimum depth of 300mm; (e) provide a DIN or NEN standard forged Storz 65mm coupling fitted with a suction washer for connection to fire fighting equipment; (f) ensure the coupling is accessible and available for connection at all times;

		(g) ensure the coupling is fitted with a blank cap and securing chain (minimum 220mm length); (h) ensure underground tanks have either an opening at the top of no less than 250mm diameter or a coupling compliant with this Table; and (i) if a remote offtake is installed, ensure the offtake is in a position that is: (i) visible; (ii) accessible to allow connection by fire fighting equipment; (iii) at a working height of 450 – 600mm above ground level; and (iv) protected from possible damage, including damage by vehicles
D.	Signage for static water connections.	The fire fighting water point for a static water supply must be identified by a sign permanently fixed to the exterior of the assembly in a visible location. The sign must: (a) comply with water tank signage requirements with Australian Standard AS 2304-2019 Water Storage tanks for fire protection systems; or (b) comply with the Tasmanian Fire Service Water Supply Guideline published by the Tasmania Fire Service.
E.	Hardstand	A hardstand area for a fire appliance must be: (a) no more than 3m from the fire fighting water point, measured as a hose lay (including the minimum water level in dams, swimming pools and the like); (b) no closer than 6m from the building area to be protected; (c) a minimum width of 3m constructed to the same standard as the carriageway; and (d) connected to the property access by a carriageway equivalent to the standard of the property access.

Development response

Lots 1 to 4 shall have a static water supply installed that complies with Table C13.5.

Lot 1 static water supply shall be installed prior to sealing of final plan.

Lot 2-4 static water supply shall be installed when future building works are undertaken.

Lots 100 does not require a static water supply (proposed use is not building works).

5.0 CONCLUSION

A Bushfire Hazard Report has been completed for the proposed subdivision of one lot into five lots. This will create four lots that will allow buildings to be constructed. Lot 1 has an existing office/workshop. Lots 2 to 4 are vacant. Lot 100 will be part of the proposed extension of Possum Road. The use of Lot 100 does not allow for buildings to be constructed.

All proposed lots are within the Bushfire Prone Areas overlay. The Bushfire Hazard Report and certified BHMP shows compliance to C13.0 Bushfire-Prone Areas Code Tasmanian Planning Scheme.

This Bushfire Hazard Report and BHMP are valid for any building constructed within the '20m x 40m indicative building area' as shown on the BHMP. Any buildings or part of building located outside this area will require a Bushfire Hazard Report and BHMP to comply with the Director's Determination – Bushfire Hazard Areas V1.2 or any subsequent Determination valid at the time of building.

This Bushfire Hazard Report and BHMP does not endorse the removal of any vegetation without the approval from the local government authority.

It is the owners' responsibility to ensure that the requirements of the Bushfire Hazard Report and BHMP are implemented and maintained for the life of the development.

The BHMP is valid for a period of six years.

6.0 REFERENCES

AS3959 – 2018 - Construction of Buildings in Bushfire Prone Areas

Bushfire Information Publications - Tasmania Fire Service.

The LIST - Department of Primary Industries Parks Water & Environment

C13.0 Bushfire-Prone Areas Code, Tasmanian Planning Scheme

7.0 APPENDIX

7.1 FIELD PHOTOS

Photo 1: Lot 2 – NE direction from the indicative building area.

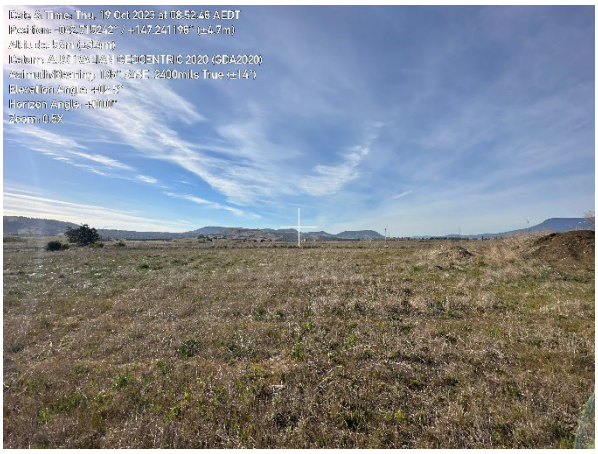


Photo 2: Lot 2 – SE direction from the indicative building area.

Photo 3: Lot 2 – SW direction from the indicative building area.

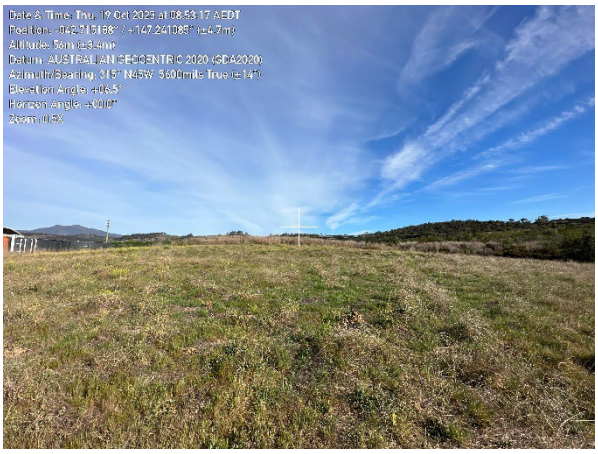


Photo 4: Lot 2 – NW direction from the indicative building area.

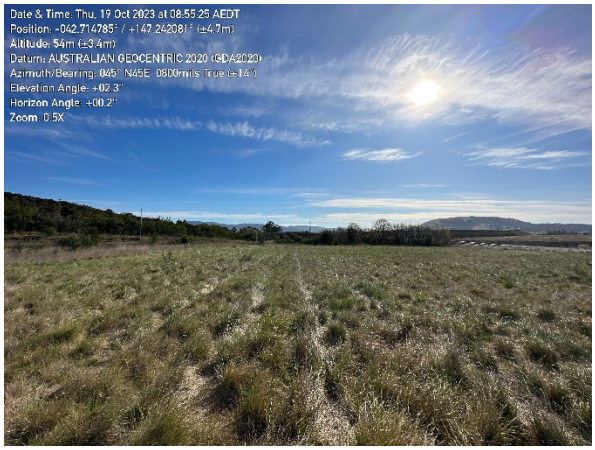


Photo 5: Lot 3 – NE direction from the indicative building area.

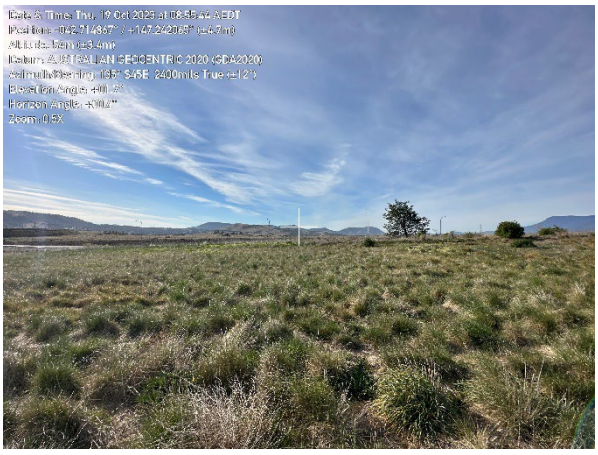


Photo 6: Lot 3 – SE direction from the indicative building area.

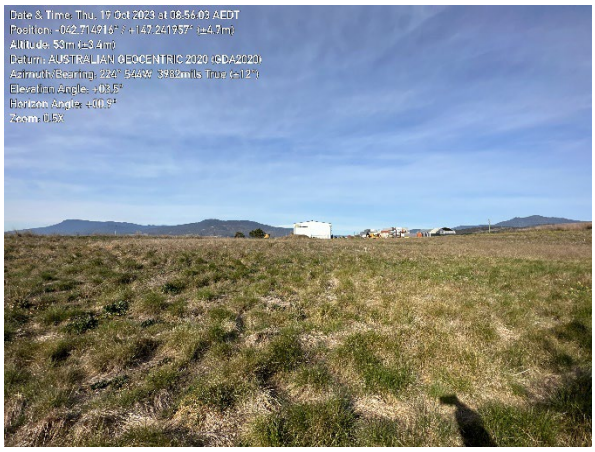


Photo 7: Lot 3 – SW direction from the indicative building area.

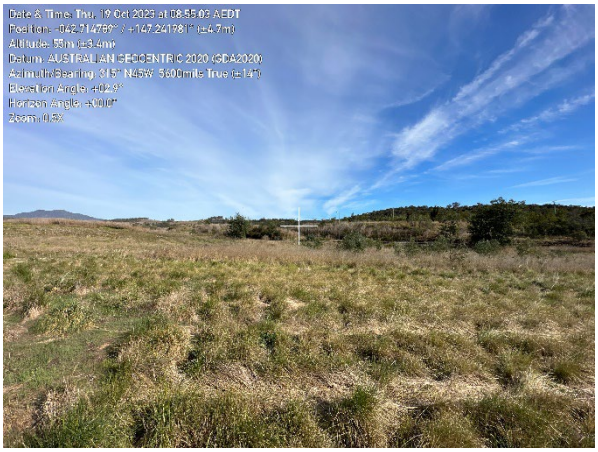


Photo 8: Lot 3 – NW direction from the indicative building area.

Photo 9: Lot 4 – NE direction from the indicative building area.

Photo 10: Lot 4 – SE direction from the indicative building area.

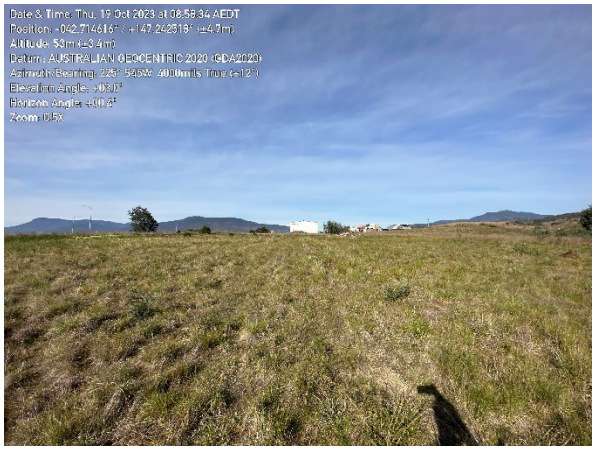


Photo 11: Lot 4 – SW direction from the indicative building area.

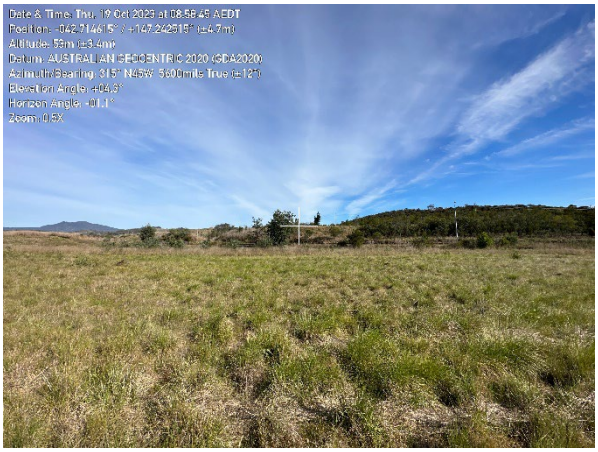


Photo 12: Lot 4 – NW direction from the indicative building area.

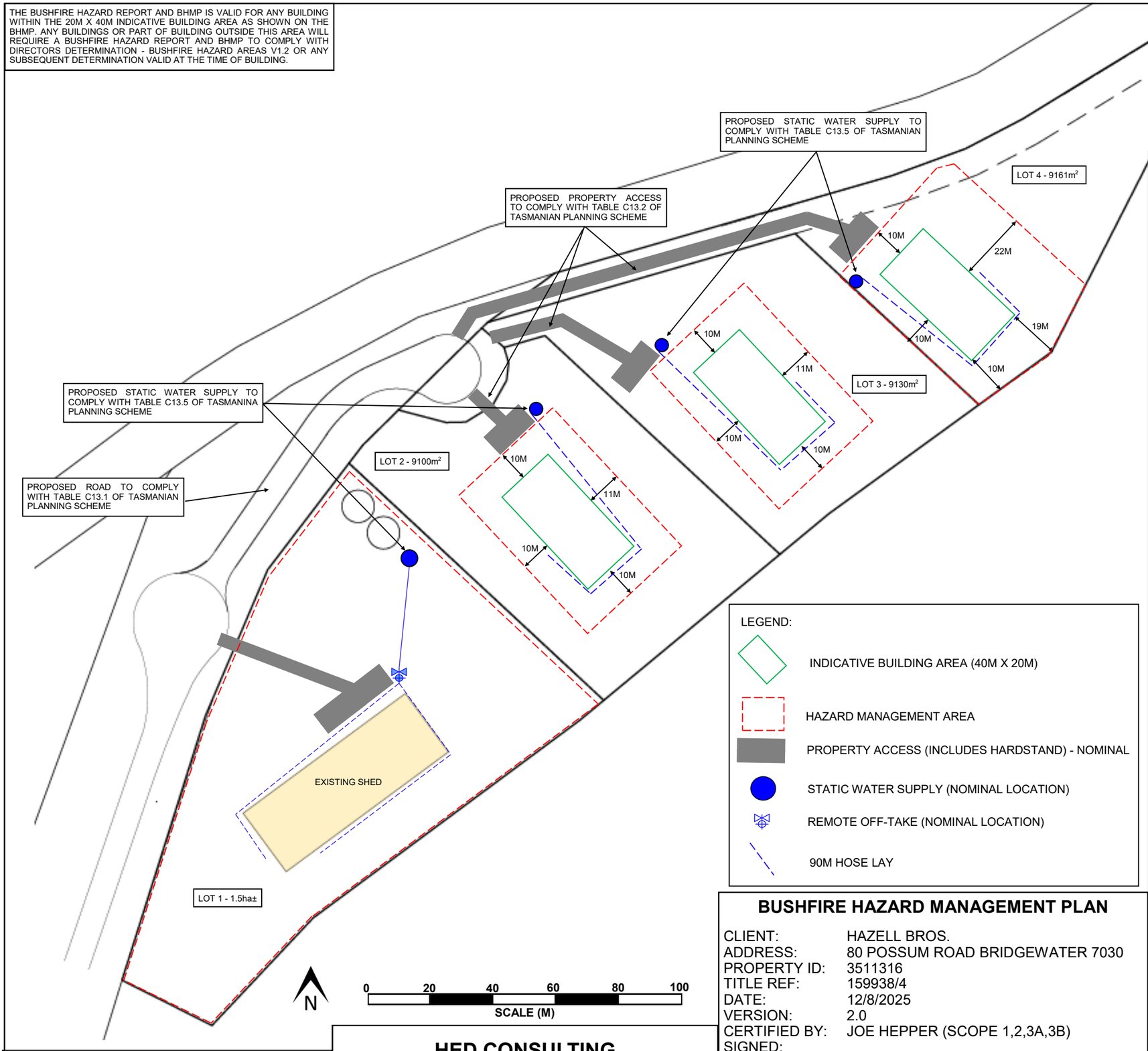
Photo 13: Lot 1 – Existing use as a Onetrak machinery workshop and administration space. Note existing static water supply use for firefighting purposes.

Photo 14: Lot 4 – Example of Classified vegetation D: Scrub (trees) and G: Grassland.

Photo 15: Example of Classified vegetation: D: Scrub on the north – west of the proposed subdivision (north of the railway).

Photo 16: Midland Highway Road reserve and roadway. Vegetation is considered low threat vegetation as per clause 2.2.3.2 (f) of AS3959:2018.

THE BUSHFIRE HAZARD REPORT AND BHMP IS VALID FOR ANY BUILDING WITHIN THE 20M X 40M INDICATIVE BUILDING AREA AS SHOWN ON THE BHMP. ANY BUILDINGS OR PART OF BUILDING OUTSIDE THIS AREA WILL REQUIRE A BUSHFIRE HAZARD REPORT AND BHMP TO COMPLY WITH DIRECTORS DETERMINATION - BUSHFIRE HAZARD AREAS V1.2 OR ANY SUBSEQUENT DETERMINATION VALID AT THE TIME OF BUILDING.



LEGEND:

- INDICATIVE BUILDING AREA (40M X 20M)
- HAZARD MANAGEMENT AREA
- PROPERTY ACCESS (INCLUDES HARDSTAND) - NOMINAL
- STATIC WATER SUPPLY (NOMINAL LOCATION)
- REMOTE OFF-TAKE (NOMINAL LOCATION)
- 90M HOSE LAY

BUSHFIRE HAZARD MANAGEMENT PLAN

CLIENT:	HAZELL BROS.
ADDRESS:	80 POSSUM ROAD BRIDGEWATER 7030
PROPERTY ID:	3511316
TITLE REF:	159938/4
DATE:	12/8/2025
VERSION:	2.0
CERTIFIED BY:	JOE HEPPER (SCOPE 1,2,3A,3B)
SIGNED:	

BUSHFIRE HAZARD MANAGEMENT REQUIREMENTS

1. Provisions of hazard management areas

Lot 1: Complies with C13.6.1: A1 (b). HMA to continue to be managed as per section 4 of this BHMP.

Lots 2-4: Shall comply with C13.6.1: A1 (b) of the Tasmanian Planning Scheme. The Hazard Management Area (HMA) shall comply with section 4 of this BHMP and be implemented when future building works are undertaken.

2. Public and fire fighting access

Lot 100 and acquired road: Extension of road and turning circle shall comply with C13.6.2: A1 (b) of the Tasmanian Planning Scheme. The road shall be constructed prior to sealing of final plan.

Lot 1: Complies with C13.6.2: A1 (b) of the Tasmanian Planning Scheme.

Lots 2-4: Property access shall comply with C13.6.2: A1 (b) pf the Tasmanian Planning Scheme. Crossover shall be minimum 4m carriageway width and installed prior to sealing of final plan.

3. Provision of water supply for fire fighting purposes

Lot 1: Shall comply with C13.6.3: A2 (b) of Tasmanian Planning Scheme. Static water supply for fire fighting purposes shall be installed prior to sealing of final plan.

Lots 2-4: Shall comply with C13.6.3: A2 (b) of Tasmanian Planning Scheme. Static water supply for fire fighting purposes shall be installed when future building works are undertaken.

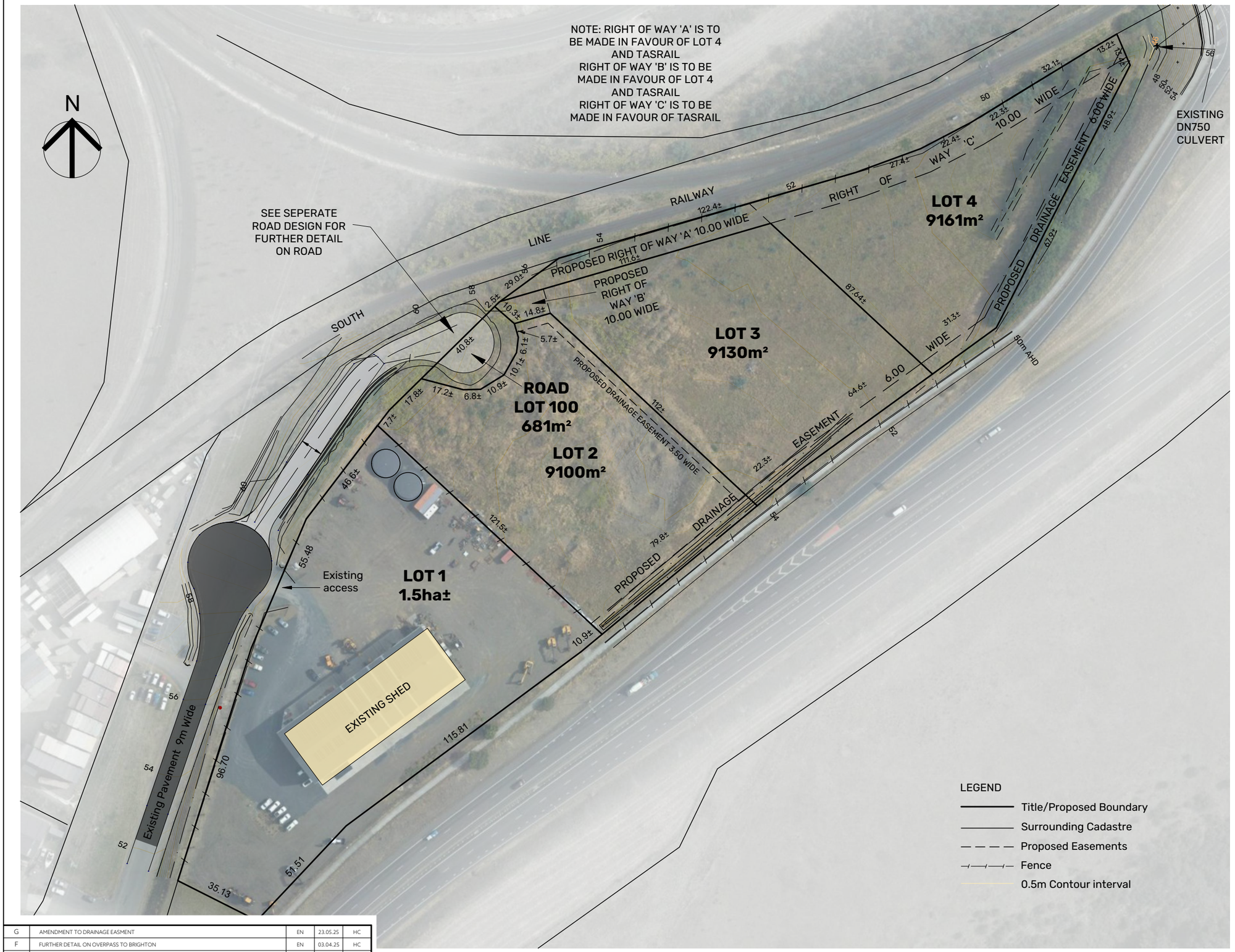
4. Hazard Management Area

This area to be maintained and managed as defendable space from a attack. Area to be managed as per below.

- Establish non-flammable areas around the building area. This includes paths, driveways, and maintained lawns (less than 100mm height).
- Non - combustible ground cover should be used in garden beds (small rock and pebbles instead of pine bark)
- Remove any ground fuels (eg. leaf litter, bark and branches).
- Flammable materials such as woodpiles, fuels and rubbish shall be stored away from the protected building.
- Non-flammable separated shrubs, hedges and small trees shall be used for landscaping around the protected building.
- Tree canopies must not distribute leaf litter into gutters.
- There must be a horizontal separation between the tree crowns and vertical separation between the ground fuels and trees branches.
- No mass plantings of trees greater than 2m.
- Slash surrounding pasture periodically to prevent pasture from becoming woody weed infested or regenerating to bushland.

THE BUSHFIRE HAZARD MANAGEMENT PLAN SHOULD BE READ IN CONJUNCTION WITH THE BUSHFIRE HAZARD REPORT V2.0

HED CONSULTING
UNIT 2, 1 LIVERPOOL STREET HOBART TAS 7000
P 03 6146 0334 / E info@hed-consulting.com.au



PLAN OF SUBDIVISION

Owners
R.G. Hazell Pty. Ltd.
David Hazell Pty Ltd

Title References
FR 159938/4

Address
80 Possum Road Bridgewater

Council
Brighton Council

Tasmanian Planning scheme
Brighton Local Provisions Schedule

Zone
19.0 General Industrial

Code Overlay
9 Attenuation Code/Bridgewater Quarry
13 Bushfire-prone Area
BRI-S4.0, BRI-S10.0,

PID
3511316

Point of interest GDA2020 MGA55
59752E, 5270805N

Schedule of Easements
Proposed Rights of Way as shown.
Proposed Drainage Easement 6.00 wide

NOTES

This plan has been prepared only for the purpose of obtaining preliminary subdivision approval from the Council and the information shown hereon should be used for no other purpose.
All measurements and areas are subject to final survey.

All lots shown on plan cannot be serviced by water and sewer. On site treatment for sewer is required.

Water tanks are required for firefighting purposes.

The Site is covered in its entirety by the Codes listed above and have not been shown for clarity.

REV	AMENDMENTS	DRAWN	DATE	APPR.
G	AMENDMENT TO DRAINAGE EASMENT	EN	23.05.25	HC
F	FURTHER DETAIL ON OVERPASS TO BRIGHTON	EN	03.04.25	HC
E	CHANGED SW OUTLET DIAMETER AND REMOVED VARIOUS NOTES	EN	07.03.25	MK
D	EXTENDED DETAIL	EN	13.08.24	HC
C	ROAD LOT EXTENDED TO MATCH ROAD DESIGN	EN	01.08.24	HC
B	ROAD WIDTH INCREASED TO 11m	EN	18.07.24	CT
A	TURNING CIRCLE ADDED	EN	30.11.23	CT
0	PLAN OF SUBDIVISION	EN/MK	05.09.23	HC

NOTES:

SURVEYOR	GEOCIVIL
EN	EN
DRAWN	CHECKED
EN/MK	HC
DATE	
23/05/2025	

PLAN OF SUBDIVISION
80 POSSUM ROAD, BRIDGEWATER
for DAVID HAZELL

PDA
SURVEYORS, ENGINEERS & PLANNERS
127 Bathurst Street
Hobart, Tasmania, 7000
PHONE: +61 03 6234 3217
FAX: +61 03 6234 5085
EMAIL: pda.hbt@pda.com.au
www.pda.com.au
Also at: Kingston, Launceston & Burnie

SCALE	PAPER
1:1500	(A3)
JOB NUMBER	DRAWING
46981NG-1g	

BUSHFIRE-PRONE AREAS CODE

CERTIFICATE[1] UNDER S51(2)(d) *LAND USE PLANNING AND APPROVALS ACT 1993*

1. Land to which certificate applies

The subject site includes property that is proposed for use and development and includes all properties upon which works are proposed for bushfire protection purposes.

Street address:	80 Possum Road Bridgewater 7030
Certificate of Title / PID:	CT 159938/4, PID 3511316

2. Proposed Use or Development

Description of proposed Use and Development:	Five – lot subdivision (one lot into five lots)
Applicable Planning Scheme:	Tasmanian Planning Scheme

3. Documents relied upon

This certificate relates to the following documents:

Title	Author	Date	Version
Bushfire Hazard Report	HED Consulting	12/8/2025	2.0
Bushfire Hazard Management Plan	HED Consulting	12/8/2025	2.0
Plan of Subdivision 80 Possum Road, Bridgewater for David Hazell	PDA Surveyors, Engineers & Planners	23/5/2025	46981NG-1g

[1] This document is the approved form of certification for this purpose and must not be altered from its original form.

4. Nature of Certificate

The following requirements are applicable to the proposed use and development:

☐	**E1.4 / C13.4 – Use or development exempt from this Code**	
	Compliance test	**Compliance Requirement**
☐	E1.4(a) / C13.4.1(a)	Insufficient increase in risk

☐	**E1.5.1 / C13.5.1 – Vulnerable Uses**	
	Acceptable Solution	**Compliance Requirement**
☐	E1.5.1 P1 / C13.5.1 P1	***Planning authority discretion required. A proposal cannot be certified as compliant with P1.***
☐	E1.5.1 A2 / C13.5.1 A2	Emergency management strategy
☐	E1.5.1 A3 / C13.5.1 A2	Bushfire hazard management plan

☐	**E1.5.2 / C13.5.2 – Hazardous Uses**	
	Acceptable Solution	**Compliance Requirement**
☐	E1.5.2 P1 / C13.5.2 P1	***Planning authority discretion required. A proposal cannot be certified as compliant with P1.***
☐	E1.5.2 A2 / C13.5.2 A2	Emergency management strategy
☐	E1.5.2 A3 / C13.5.2 A3	Bushfire hazard management plan

☒	**E1.6.1 / C13.6.1 Subdivision: Provision of hazard management areas**	
	Acceptable Solution	**Compliance Requirement**
☐	E1.6.1 P1 / C13.6.1 P1	***Planning authority discretion required. A proposal cannot be certified as compliant with P1.***
☐	E1.6.1 A1 (a) / C13.6.1 A1(a)	Insufficient increase in risk
☒	E1.6.1 A1 (b) / C13.6.1 A1(b)	Provides BAL-19 for all lots (including any lot designated as 'balance')
☐	E1.6.1 A1(c) / C13.6.1 A1(c)	Consent for Part 5 Agreement

☐	**E1.6.2 / C13.6.2 Subdivision: Public and fire fighting access**	
	Acceptable Solution	**Compliance Requirement**
☐	E1.6.2 P1 / C13.6.2 P1	***Planning authority discretion required. A proposal cannot be certified as compliant with P1.***
☐	E1.6.2 A1 (a) / C13.6.2 A1 (a)	Insufficient increase in risk
☒	E1.6.2 A1 (b) / C13.6.2 A1 (b)	Access complies with relevant Tables

☒	**E1.6.3 / C13.1.6.3 Subdivision: Provision of water supply for fire fighting purposes**	
	Acceptable Solution	**Compliance Requirement**
☐	E1.6.3 A1 (a) / C13.6.3 A1 (a)	Insufficient increase in risk
☐	E1.6.3 A1 (b) / C13.6.3 A1 (b)	Reticulated water supply complies with relevant Table
☐	E1.6.3 A1 (c) / C13.6.3 A1 (c)	Water supply consistent with the objective
☐	E1.6.3 A2 (a) / C13.6.3 A2 (a)	Insufficient increase in risk
☒	E1.6.3 A2 (b) / C13.6.3 A2 (b)	Static water supply complies with relevant Table
☐	E1.6.3 A2 (c) / C13.6.3 A2 (c)	Static water supply consistent with the objective

5. Bushfire Hazard Practitioner

Name:	Joe Hepper	**Phone No:**	03 6146 0334
Postal Address:	1 Liverpool Street, Hobart 7000	**Email Address:**	info@hed-consulting.com.au
Accreditation No:	BFP – 148	**Scope:**	1,2,3A,3B

6. Certification

I certify that in accordance with the authority given under Part 4A of the *Fire Service Act 1979* that the proposed use and development:

☐ Is exempt from the requirement Bushfire-Prone Areas Code because, having regard to the objective of all applicable standards in the Code, there is considered to be an insufficient increase in risk to the use or development from bushfire to warrant any specific bushfire protection measures, or

☒ The Bushfire Hazard Management Plan/s identified in Section 3 of this certificate is/are in accordance with the Chief Officer's requirements and compliant with the relevant **Acceptable Solutions** identified in Section 4 of this Certificate.

Signed:
certifier

Name: JOE HEPPER **Date:** 12/8/2025

Certificate Number: H2736

(for Practitioner Use only)

TRAFFIC & CIVIL SERVICES

4 LOT SUBDVISION
80 POSSUM ROAD, BRIDGEWATER

TRAFFIC IMPACT ASSESSMENT
OCTOBER 2024

4 Lot Subdivision
80 Possum Road, Bridgewater

TRAFFIC IMPACT ASSESSMENT

- Final #2
- October 2024

Traffic & Civil Services
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Contents

Document history and status

Revision	Date issued	Reviewed by	Approved by	Date approved	Revision type
1	7th Oct 2024	R Burk	R Burk	7th Oct 2024	Draft
2	11th Oct 2024	R Burk	R Burk	11th Oct 2024	Draft 2
3	23rd Oct 2024	R Burk	R Burk	23rd Oct 2024	Final
4	28th Oct 2024	R Burk	R Burk	28th Oct 2024	Final #2

Distribution of copies

Revision	Copy no	Quantity	Issued to
Draft	1	1	Tim Heaysman (Hazell Bros.)
Draft 2	1	1	Tim Heaysman (Hazell Bros.)
Final	1	1	Tim Heaysman (Hazell Bros.)
Final #2	1	1	Tim Heaysman (Hazell Bros.)

Printed:	28 October 2024
Last saved:	28 October 2024 10:24 AM
File name:	80 Possum Rd
Author:	Richard Burk
Project manager:	Richard Burk
Name of organisation:	TBA
Name of project:	80 Possum Rd
Name of document:	80 Possum Rd
Document version:	Final #2
Project number:	

1. Introduction

1.1 Background

A 4-lot General Industrial subdivision at 80 Possum Road is proposed at Bridgewater. This report has been prepared to assess the traffic impact of the proposal.

This TIA has been prepared based on Department of State Growth (DSG) guidelines and responds to Tasmanian Planning Scheme – Brighton Code C3.

1.2 Objectives

A Traffic Impact Assessment is a means for assisting in the planning and design of sustainable development that considers:

- Safety and capacity
- Equity and social justice
- Economic efficiency
- The environment and future development.

This TIA considers the impact of the proposal on projected traffic volumes expected by 2034.

1.3 Scope of Traffic Impact Assessment (TIA)

This TIA considers in detail the impact of the proposal on Possum Road and the Possum Road / Glenstone Road junction.

1.4 References

- RTA Guide to Traffic Generating Development 2002
- Tasmanian Planning Scheme - Brighton
- Austroads Guide Road Design Part 4A: Unsignalised & Signalised Intersections 2021
- Guide to Traffic Management Part 6: Intersections, Interchanges & Crossings 2020.
- LGAT Tasmanian Standard Drawings

1.5 Statement of Qualifications and Experience

This TIA has been prepared by Richard Burk, an experienced and qualified traffic engineer in accordance with the requirements of the Department of State Growth's guidelines and Council's requirements. Richard's experience and qualifications include:

- 37 years professional experience in road and traffic engineering industry
    - Manager Traffic Engineering at the Department of State Growth until May 2017.
    - Previous national committee membership with Austroads Traffic Management Working Group and State Road Authorities Pavement Marking Working Group
- Master of Traffic, Monash University, 2004
- Post Graduate Diploma in Management, Deakin University, 1995
- Bachelor of Civil Engineering, University of Tasmania, 1987

Richard Burk

BE (Civil) M Traffic Dip Man. MIE Aust CPEng

Director Traffic and Civil Services Pty Ltd

1.6 Glossary of Terms

AADT	Annual Average Daily Traffic - The total number of vehicles travelling in both directions passing a point in a year divided by the number of days in a year.
Acceleration Lane	An auxiliary lane used to allow vehicles to increase speed without interfering with the main traffic stream. It is often used on the departure side of intersections.
Access	The driveway by which vehicles and/or pedestrians enter and/or leave the property adjacent to a road.
ADT	Average Daily Traffic – The average 24-hour volume being the total number of vehicles travelling in both directions passing a point in a stated period divided by the stared number of days in that period.
Austroads	The Association of Australian and New Zealand road transport and traffic authorities and includes the Australian Local Government Association.
Delay	The additional travel time experiences by a vehicle or pedestrian with reference to a vase travel time (e.g. the free flow travel time).
DSG	Department of State Growth – The Tasmanian Government Department which manages the State Road Network.
GFA	Gross Floor Area
Intersection Kerb	The place at which two or more roads meet or cross. A raised border of rigid material formed at the edge of a carriageway, pavement or bridge.
km/h	Kilometres per hour
Level of Service	An index of the operational performance of traffic on a given traffic lane, carriageway or road when accommodating various traffic volumes under different combinations of operating conditions. It is usually defined in terms of the convenience of travel and safety performance.
m	Metres
Median	A strip of road, not normally intended for use by traffic, which separates carriageways for traffic in opposite directions. Usually formed by painted lines, kerbed and paved areas grassed areas, etc.
Movement	A stream of vehicles that enters from the same approach and departs from the same exit (i.e. with the same origin and destination).
Phase	The part of a signal cycle during which one or more movements receive right-of -way subject to resolution of any vehicle or pedestrian conflicts by priority rules. A phase is identified by at least one movement gaining right-of-way at the start of it and at least one movement losing right-of-way at the end of it.

Sight Distance	The distance, measured along the road over which visibility occurs between a driver and an object or between two drivers at specific heights above the carriageway in their lane of travel.
Signal Phasing	Sequential arrangement of separately controlled groups of vehicle and pedestrian movements within a signal cycle to allow all vehicle and pedestrian movements to proceed.
SISD	Safe Intersection Sight Distance – The sight distance provides sufficient distance for a driver of a vehicle on the major road to observe a vehicle on a minor road approach moving into a collision situation and to decelerate to a stop before reaching the collision point.
Speed	Distance travelled per unit time.
85th Percentile	The speed at which 85% of car drivers will travel slower and 15% will travel faster.
Traffic-actuated Control	A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian tragic demands.
Traffic Growth Factor	A factor used to estimate the percentage annual increase in traffic volume.
Trip	A one-way vehicular movement from one point to another excluding the return journey. Therefore, a vehicle entering and leaving a land use is counted as two trips. (RTA Guide to Traffic generating Developments).
Turning Movement	The number of vehicles observed to make a particular turning movement (left or right turn, or through movement) at an intersection over a specified period.
Turning Movement Count	A traffic count at an intersection during which all turning movements are recorded.
Vehicle Actuated Traffic Signals	Traffic signals in which the phasing varies in accordance with the detected presence of vehicles on the signal approaches.
vpd	vehicles per day – The number of vehicles travelling in both directions passing a point during a day from midnight to midnight.
vph	vehicles per hour – The number of vehicles travelling in both directions passing a point during an hour.

1.7 Site Specific Glossary of Terms

BC	Brighton Council
SSA	Safe System Assessment

2. Site Description

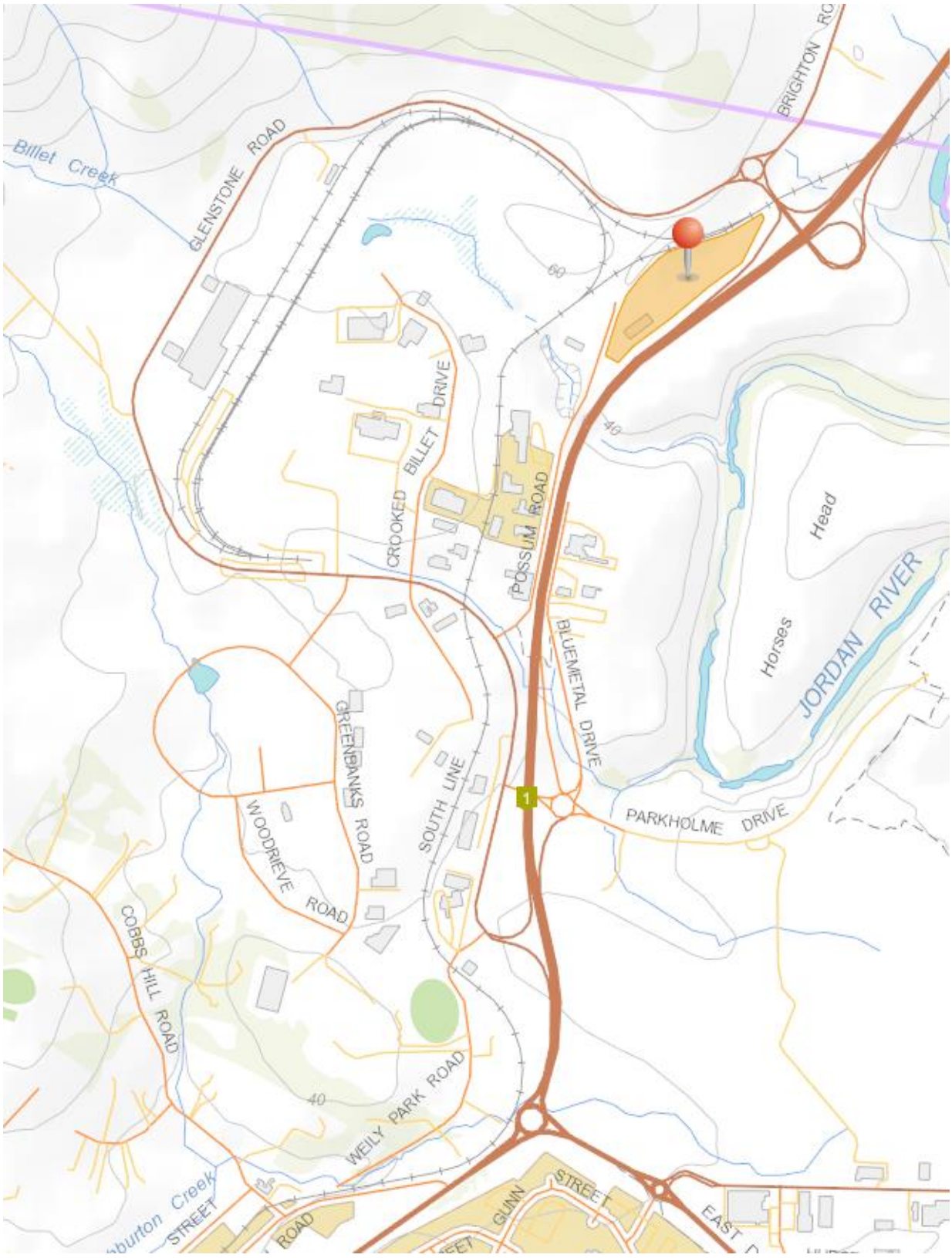
Figures 1 & 2 show the development location at 80 Possum Road.

The proposed subdivision site has access to Possum Road and involves a 130m extension for access, see Figure 4. The subdivision site is relatively flat and drains towards Crooked Billet Creek / Jordan River to the East of the site.

Figure 1 – Proposed development location

Source: The List, DPIPWE

Figure 2 – Road network surrounding 80 Possum Road



Source: The List, DPIPWE

3. Proposal, Planning Scheme and Road Owner objectives

3.1 Description of Proposed Development

The proposal is to subdivide 80 Possum Road into 4 lots. Figures 3,4 & 5 show the proposed lot layout and access, see Appendix A for the full Plan of Subdivision.

Figure 3 – Proposed subdivision layout at 80 Possum Road, Bridgewater

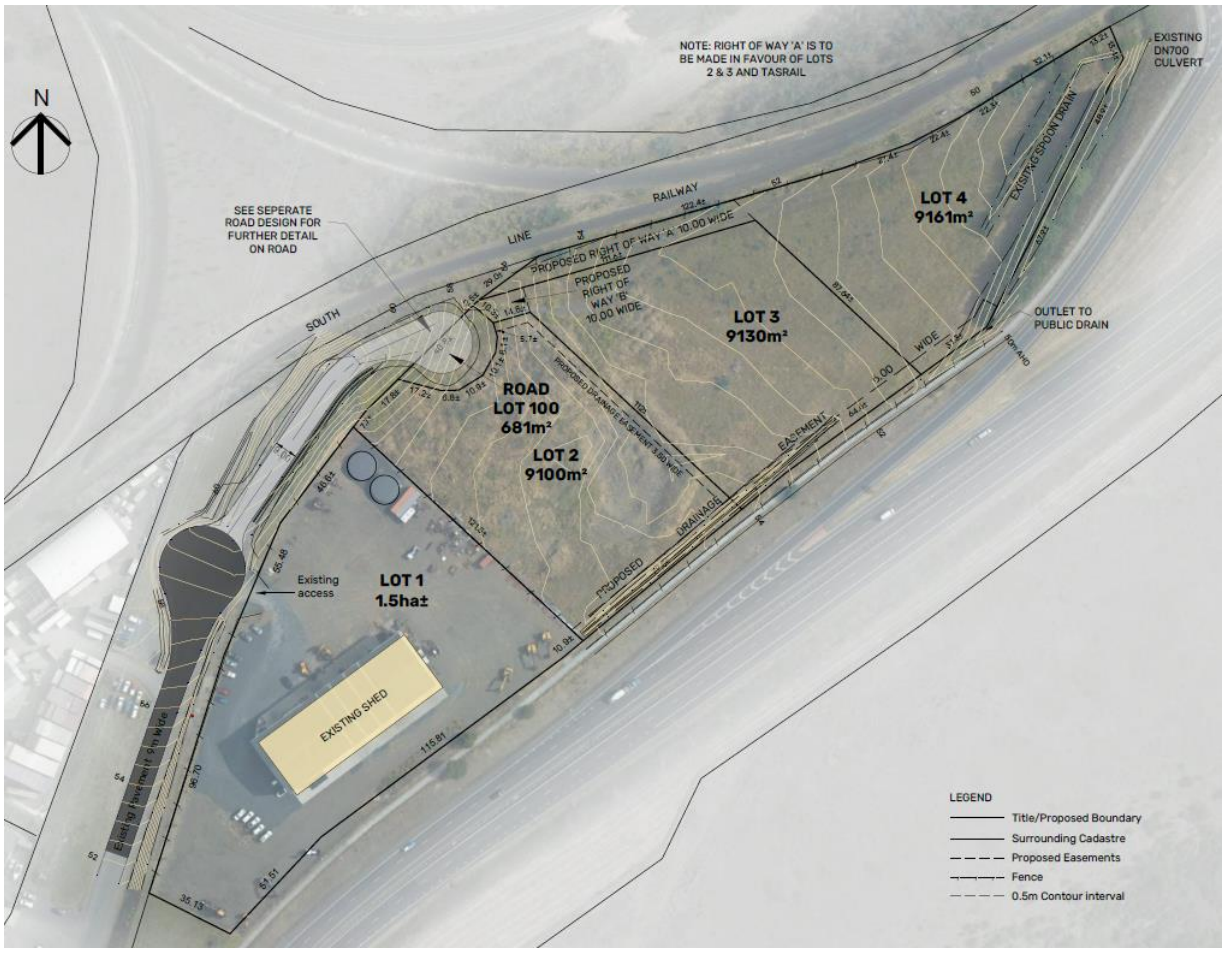


Figure 4 – Proposed subdivision layout at 80 Possum Road, Bridgewater

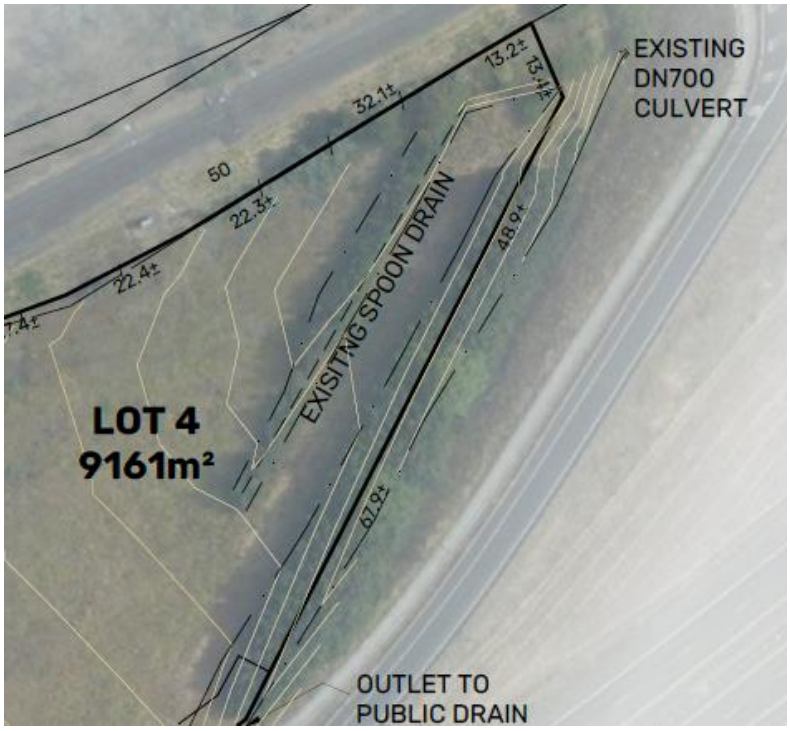
Proposed 130m. extension of Possum Road.

LGAT Guidelines:

Rural Sealed Road
(TSD-R02)
AADT is 300-2000 vpd
Code S4
Seal width 6.0m.

As Possum Road is 9.0m wide the same width is considered appropriate for the extension.

Figure 5 – Proposed subdivision layout at 80 Possum Road, Bridgewater



TPS – Brighton:

Code C13.6.2
Public & Firefighting access

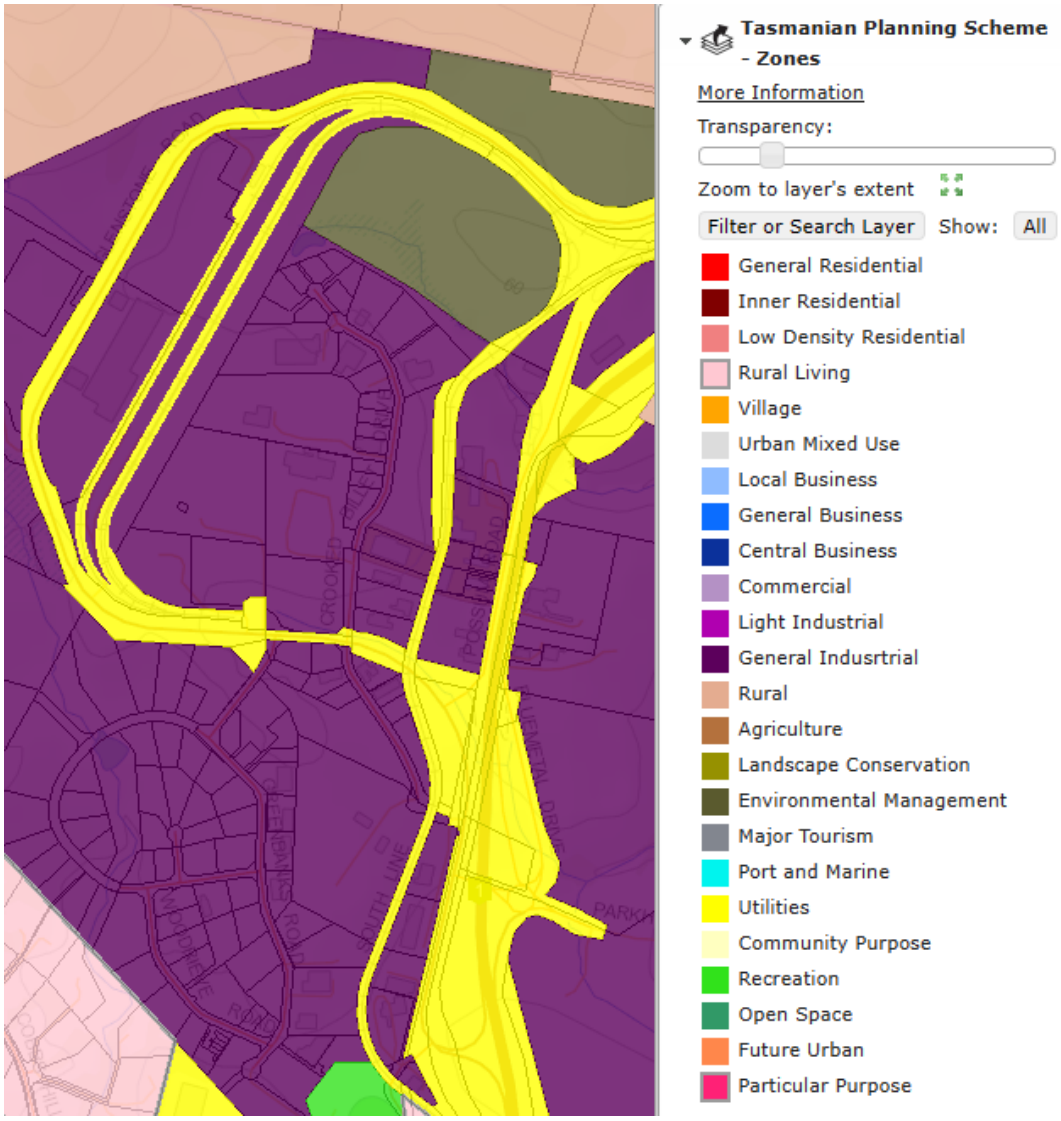
Table C13.1
Standards for Roads

Minimum width for Cul-De-Sac Road is 5.5m with 12m. radius

3.2 Council Planning Scheme

The proposed development site zoning is shown in Figure 6 as per the Tasmanian Planning Scheme - Brighton.

Figure 6 – 80 Possum Road is zoned General Industrial.



Source: The List, DPIPWE

3.3 State Road Network Objectives

DSG is the authority responsible for the State Road network impacted by the proposal. DSG objectives are to maintain traffic safety and capacity.

3.4 Local Road Network Objectives

Brighton Council (BC) is the authority responsible for the Council Road network impacted by the proposal. BC objectives are to maintain traffic safety and capacity.

4. Existing Conditions

4.1 Transport Network

The transport system adjacent the proposed development site consists of Midlands Highway, Glenstone Road and Possum Road.

4.2 Midlands Highway, Bridgewater

Midlands Hwy is a Category 1 Trunk Road in the State Road Hierarchy. The road has Limited Access status and is part of the Tasmanian 26m Double B Network, see Appendix C.

The highway has dual lane carriageways in each direction separated by median wire rope safety barrier The traffic lanes are 3.5m wide with 2m sealed shoulders. The highway has a 110Km/h Speed Limit and no footpaths.

4.3 Glenstone Road

Glenstone Road is a Category 2 Regional Freight Route in the State Road Hierarchy. The road does not have Limited Access status and is part of the Tasmanian 26m Double B Network, see Appendix C.

Glenstone Road has a speed limit of 70km/h on the approaches to the Possum Road junction, see Figure 7. The road has 3.5m traffic lanes each way , 1.5m sealed shoulders and a 3m wide median turn lane and is well delineated with Edge & Lane lines. Footpaths are provided on the West and East sides and a pedestrian refuge island within the median, see Figure 12 – 15.

Figure 7 – Glenstone Road Southern approach to Possum Road.

4.4 Midlands Highway / Glenstone Road Interchange - North

The existing interchange is within a 110km/h Speed Limit on the Midlands Highway approaches. Figure 8 show the interchange layout and access to Glenstone Road.

Figure 8 – Midlands Highway / Glenstone Road Interchange – North

Source: *The List, DPIPWE*

4.5 Midlands Highway / Glenstone Road Interchange - South

The existing interchange is within an 80km/h Speed Limit on the Midlands Highway approaches. Figure 9 shows the interchange layout and access to Glenstone Road.

Figure 9 – Midlands Highway / Glenstone Road Interchange – South

Source: The List, DPIPWE

4.6 Glenstone Road Level Crossing

There is a signalised rail level crossing on Glenstone Road 110m Northwest of the Possum Road junction, see Figure 10. The crossing is well signed and delineated with pavement markings and has boom gates and crossing signals.

Figure 10 – Glenstone Road Level Crossing

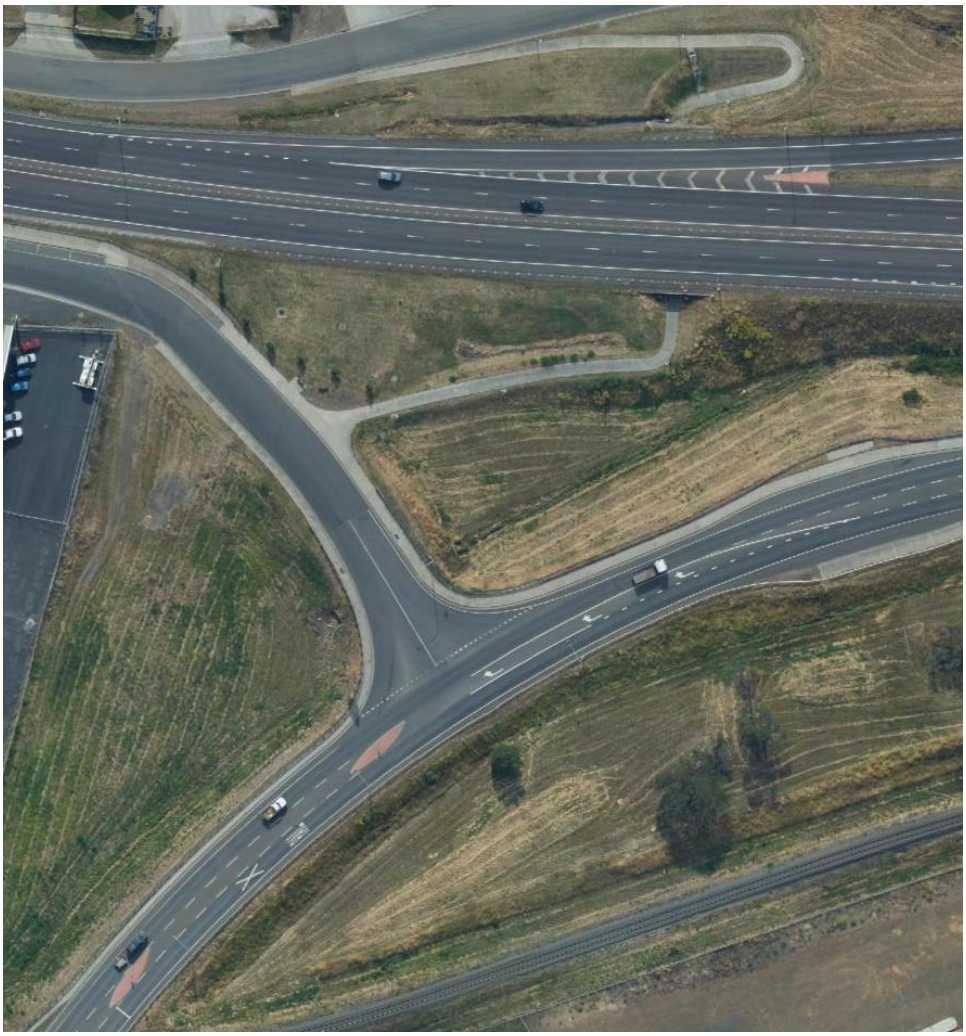
Source: The List, DPIPWE

4.7 Glenstone Road / Possum Road junction

The existing junction has a partially channelised layout and is situated on a gentle horizontal curve in the road. The Glenstone Road approaches to the junction have an estimated speed environment of 70km/h. Figures 11 - 16 show the nature of the junction.

Figure 11 – Possum Road approach to Glenstone Road

Figure 12 – Aerial view of Glenstone Road / Possum Road junction



Source: *The List, DPIPWE*

Figure 13 – Looking right from Possum Road along Glenstone Road

Sight distance right is 200m.

Figure 14 – Looking left from Possum Road along Glenstone Road

Sight distance left is 195m.

Figure 15 – Glenstone Road Southern approach to Possum Road

Pedestrian refuge island on Glenstone Road.

Figure 16 – Glenstone Road Northern approach to Possum Road

4.8 #80 Possum Road Access

Possum Road is a sealed No Through Road managed by Brighton Council and functions as a local access road to General Industrial land uses. The road does not have Limited Access status and is part of the Tasmanian 26m Double B Network, see Figure 17 and Appendix C.

The General Urban Speed Limit of 50km/h applies to Possum Road. The seal width varies from 9.3m where there is kerb and channel along the Southern section to 7.5m wide where there is no kerb and channel along the Northern section. Figures 17 - 19 show the nature of the existing Cul-De-Sac and approaches.

Figure 17 – Aerial view of access to #80 Possum Road

Source: The List, DPIPWE

Figure 18 – #80 Possum Road approach Possum Road

Forward sight distance is 120m.

Figure 19.1 – Possum Road approach to Possum Road Cul-De-Sac

Figure 19.2 – Possum Road Cul-De-Sac approach to #80 Possum Road

Figure 19.3 – Elevation view of entrance to #80 Possum Road

4.9 Traffic Activity

Traffic activity from DSG records is summarised as follows, see Appendix E for details:

Midlands Highway

- AADT: 22,900 vpd (2022)
- % CV: 17%
- 3.3% compound annual growth
- Projected AADT: 33,800 vpd (2034).

Glenstone Road (Southern end)

- AADT: 3,390 vpd (2022)
- % CV: 35%
- 0.9% compound annual growth
- Projected AADT: 3,775 vpd (2034) without proposal.

Glenstone Road (Northern end)

- AADT: 1,057 vpd (2022)
- % CV: 29%
- 6.6% compound annual growth
- Projected AADT: 2,275 vpd (2034) without proposal.

Possum Road (approaching Glenstone Road)

- AADT: 734 vpd (2024) (TCS estimate)
- % CV: 33%
- 0 % compound annual growth
- Projected AADT:734 vpd (2034) without proposal.
- Peak Hour 122vph i.e 61 vph each way (2034)

4.10 Crash History

The Department of State Growth is supplied with reported crashes by Tasmania Police. The Department maintains a crash database from the crash reports which is used to monitor road safety, identify problem areas and develop improvement schemes.

As of the 6thOctober 2024 the 5-year reported crash history records no crashes on Possum Road or at the junction with Glenstone Road. The crash history provides no evidence of a crash propensity in the vicinity of the proposal.

4.11 Services

No existing services are disaffected by the proposal.

4.12 Road Safety Review

Midlands Highway

No road safety issues were identified.

Midlands Highway / Glenstone Road Southern interchange

No road safety issues were identified.

Midlands Highway / Glenstone Road Northern interchange

No road safety issues were identified.

Glenstone Road

No road safety issues were identified.

Possum Road

No road safety issues were identified.

Proposed 80 Possum Road access

No road safety issues were identified.

4.13 Sight Distance Criteria

The proposed accesses satisfy sight distance guideline, see Figure 20.

Figure 20– Sight distance summary

Junction / Access	Speed		Road Frontage Sight Distance			
Major Rd - Minor Rd	Limit (km/h)	Environment (km/h)	Austroads SISD (m)	Available Left(m)	Available Right(m)	AS/NZS 2890.1 SSD (m)
Glenstone / Possum	70	70	151	195	200	
Proposed Lot accesses						
1	50	50	97	120	120	45
2	50	50	97	120		45
3	50	50	97	120		45
4	50	50	97	120		45

Austroads Junction Compliant

AS/ NZS 2890.1 Property Access Compliant

4.14 Austroads Safe System Assessment

Possum Road has been assessed in accordance with the Austroads Safe System assessment framework. This framework involves consideration of exposure, likelihood and severity to yield a risk framework score. High risk crash types and vulnerable road user crash types are assessed for each site and aggregated to provide an overall crash risk. Crash risk is considered in terms of three components:

- Exposure (is low where low numbers of through and turning traffic) i.e.1 out of 4
- Likelihood (is low where the infrastructure standard is high) i.e. 1 out of 4
- Severity (is low where the speed environment is low) i.e. 1 out of 4

The Austroads Safe System Assessment process enables the relative crash risk of an intersection or road link to be assessed. Vulnerable Road users are considered along with the most common crash types.

Crash risk score indicates how well infrastructure satisfies the *safe system objective which is for a forgiving road system where crashes do not result in death or serious injury.*

From safe system assessment, Possum Road is determined to be well aligned with the safe system objective with crash risk scores of 11 / 448. See Figure 21 and Appendix D for the assessment details.

Figure 21 – Austroads Safe System Assessment alignment between crash score and risk

5. Traffic Generation and Assignment

This section of the report estimates how traffic generated by the proposal is distributed within the adjacent road network now and ten years future.

5.1 Traffic Growth

Assumed background traffic compound annual growth of 0.9 % has been assumed for Glenstone Road based on the historic growth rate for the area.

Assumed background growth on Possum Road is 0% as there is limited future development.

5.2 Trip Generation

The following RTA traffic generation rates for General Industrial land use apply:

- Factories – 5 vpd / 100m2 GFA and peak operation of 1vph / 100m2 GFA
- Warehouses – 4 vpd / 100m2 GFA and peak operation of 0.5vph / 100m2 GFA

With 4* General Industrial lots a 50:50 mix of factories and warehouses has been assumed to estimate traffic generation summarised in Figure 22.

Figure 22 – Estimated Traffic Generation due to proposal

Lot	Area (m2)	Building GFA (m2)	AADT (vpd)	Peak (vph)
1	15,000	1800	81	14
2	9,692	1200	54	9
3	9,227	1100	50	8
4	7,591	900	41	7
Lots 2,3 & 4			**144**	**24**

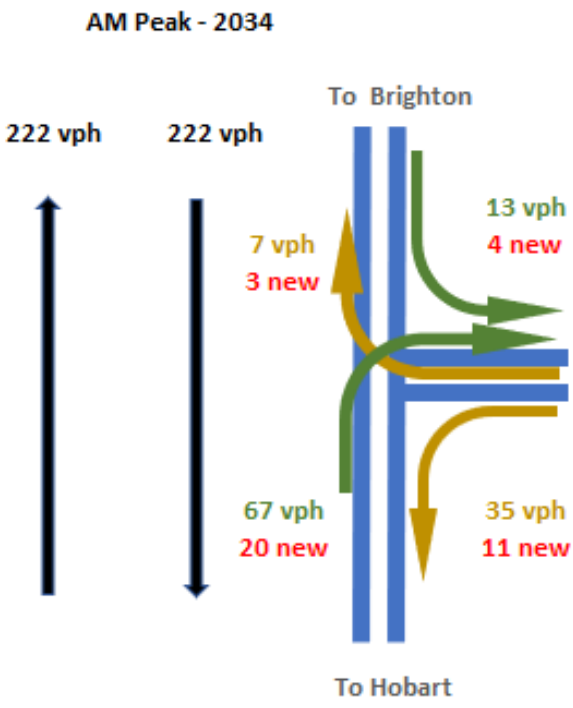
Existing Proposed

5.3 Trip Assignment

Traffic assignments at the impacted junction is summarised in Figure 23.

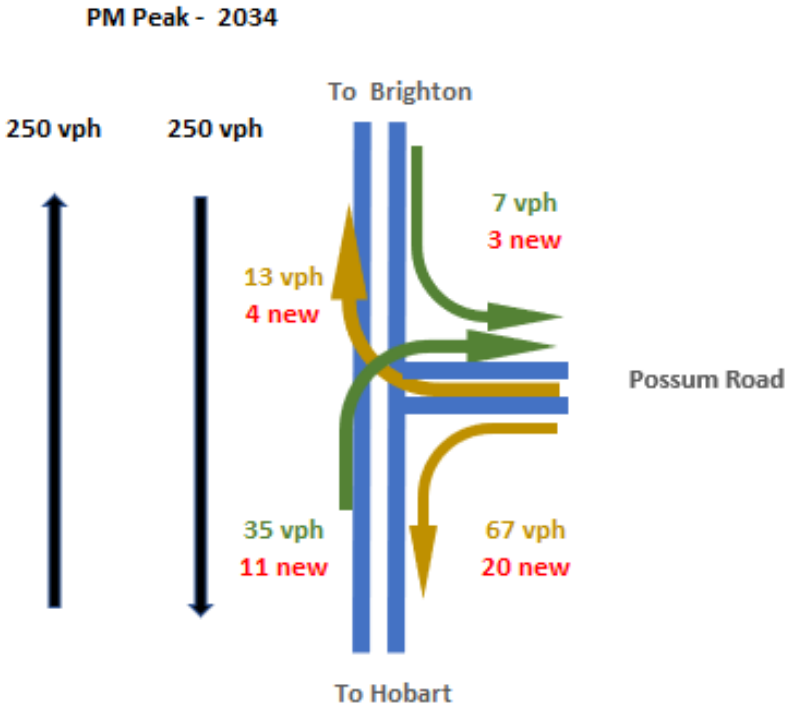
Figure 23 – 2034 Traffic Assignment at Glenstone Rd / Possum Rd Junction.

Figures in red are due to the proposal.



Peak Hour Movements		
AM	Turns	TEF
Left In	17	222
Right In	87	461
PM	Turns	TEF
Left In	10	250
Right In	46	510

TEF | Total Effected Flow



6. Impact on Road Network

6.1 Junction warrants

Junction layout requirements are based on Austroads Guidelines which take into account the standard of the road, speed limit, through & side road traffic i.e. Austroads Guide to Traffic Management Part 6: Intersections, Interchanges and Crossings – 2020.

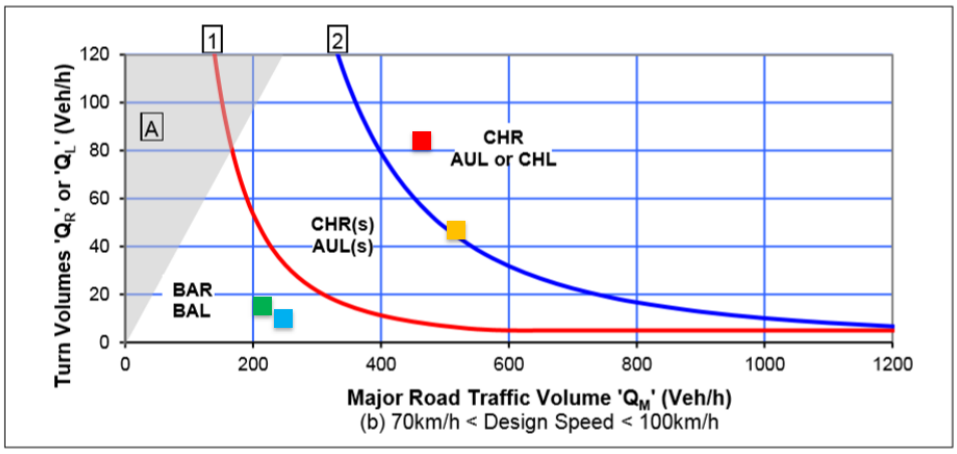
Glenstone Road / Proposed Road Junction

Figure 24 shows the relevant Austroads junction layout warrant for the Glenstone Road / Possum Road junction. Figure 24 demonstrates that the turning movements warrant:

- Channelised Right (CHR) right turn facility
- Basic Left (BAL) left turn facility.

The current junction layout satisfies the CHR and BAL junction layout.

Figure 24 – Austroads Warrant for Glenstone Rd / Possum Road junction 2034.



Source: Austroads GTM Part 6-2020

Peak Hour Movements		
AM	Turns	TEF
Left In	17	222
Right In	87	461
PM	Turns	TEF
Left In	10	250
Right In	46	510

TEF | Total Effected Flow

6.2 Impact of traffic generated by the proposal.

6.2.1 Midlands Highway

The proposal is estimated to have negligible impact on Midland Hwy / Glenstone Rd interchanges as existing ramps and roundabouts operate at low traffic volumes and the Midlands Hwy has ample capacity to absorb the estimated increase in Glenstone Rd traffic:

- 118 vpd i.e 31vph at the Southern interchange.
- 26 vpd i.e 7 vph at the Northern interchange.

Midland Hwy AADT of 35,000 vpd by 2034 with peak flow of 3,500 vph across 4 lanes would yield peak lane flows of 850 vph. Lane capacity on rural roads is some 2,000 vph.

Accordingly, the existing interchanges & roundabouts are estimated to operate between LOS A & B by 2034. See Appendix B for Austroads Levels of Service definitions.

6.2.2 Glenstone Road / Possum Road junction

The proposal is estimated to have a negligible impact on Possum Road and the Glenstone Road junction. The existing and forecast traffic volumes are low and Glenstone Road has ample capacity to absorb the estimated increase in Glenstone Road traffic of:

- 118 vpd i.e 31vph South of Possum Road.
- 26 vpd i.e 7 vph North of Possum Road.

Glenstone Rd projected AADT is 3,775vpd by 2034 with peak hour flow of 500 vph and peak lane flows of 250 vph. Lane capacity on rural roads is some 2,000 vph.

The junction is estimated to continue to operate at Level of Service A by 2034.

6.2.3 Possum Road

The proposal is estimated to have a negligible impact on Possum Road The existing and forecast traffic volumes are low and has ample capacity to absorb the estimated increase in traffic of 144 vpd. Currently traffic activity is estimated at 734 vpd.

6.3 Tasmanian Subdivision Guideline Considerations

From natural surface contours of the site the proposed 130m extension of Possum Road has a maximum a grade of some 5% which is within Tasmanian Subdivision Guidelines for allowable longitudinal grades.

6.4 Transport Planning Considerations

The proposed road standard and guidelines are shown in Figure 4.

Proposed road width is 9m which satisfies rural sealed road width & firefighting criteria.

This width is consistent with the Possum Road width approaching the cul-de-sac.

The minimum Cul-De-Sac of 12m radius is provided for firefighting, see Appendix F.

There are no transport planning issues.

6.5 Proposed Lot accesses

LGAT standard drawing TSD-R04 & 05 show truck access standard for rural properties.

LGAT standard drawings are available online at:
https://www.lgat.tas.gov.au/__data/assets/pdf_file/0027/813735/Tasmanian-Municipal-Standards-Drawings-v3-December-20202.pdf

6.6 Impacts on road users.

6.6.1 Public Transport

No impact.

6.6.2 Delivery Vehicles

Minimal impact. All proposed accesses have sufficient width for Fire fighting vehicles.

6.6.3 Pedestrians and Cyclists

Minimal impact.

6.6.4 Motorcyclists

Minimal impact.

6.7 Other impacts

6.7.1 Environmental

No applicable environmental impacts were identified in relation to:

- Noise, vibration or visual impact
- Community severance, pedestrian amenity
- Hazardous loads, air pollution or ecological impacts
- Heritage and Conservation

6.7.2 Street Lighting and Furniture

Street lighting may be required in accordance with Council Street Lighting Policy.

6.8 Liveability, Safety and Amenity Guidelines

Guidelines for the safety and amenity of a residential areas include:

- Residential precincts need to be bounded by traffic routes and/or natural barriers to minimise conflict.
- Direct vehicular and pedestrian access should be avoided from single dwelling units onto road with over 2,000 vehicles per day.
- Effective street lengths should be less than 200-250m in order to achieve typical vehicle speeds of 40km/h.
- Cyclist and pedestrian demands should be catered for separately using path or cycle networks.

To maximise the liveability, safety and amenity of the local area, road and street network layout should be such that:

- A minimum of 60% of lots should abut residential streets with less than 300vpd passing traffic.
- A minimum of 80% of lots should abut residential streets with less than 600 vpd passing traffic.
- A maximum of 5% of single dwelling lots should abut residential streets with between 1,000-2,000 vpd passing traffic.
- A maximum of 1% of single dwelling lots should abut local streets or collectors with less than 3,000 vpd passing traffic, and
- No single dwelling lot should abut a route with > 3,000 vpd passing traffic.

These guidelines are from *TE&M Chapter 2.2: Design of New Urban Networks*.

The proposal does not involve residential areas so there will be no impact on liveability, safety and amenity targets described above.

6.9 Tasmanian Planning Scheme – Brighton

Road and Railway Assets Code C3

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction.

Acceptable Solution A1.1 – **Not applicable** as the relevant roads are not Category 1.

Acceptable Solution A1.2 *– For a road, excluding a Category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.*

Written consent from the road owner, BC, has not been issued. This TIA has been prepared to assist BC with assessing the proposal. **A1.2 is currently not satisfied.**

Acceptable Solution A1.3 **– Not applicable** as no rail network is involved.

Acceptable solution A1.4: *Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing will not increase by more than:*

(a) The amounts in Table C3.1

(b) Allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road; and

The proposal involves traffic from 4 * General Industrial lots estimated at 144 vpd.

Table C3.1 allows up to 10 vpd increase for vehicles up to 5.5m in length on major roads. Glenstone Main Road is a major road.

Table C3.1 allows up to 40 vpd increase for vehicles up to 5.5m in length on minor roads. Possum Road is a minor road.

Accordingly, Table C3.1 criteria are not satisfied for Possum Road or Glenstone Main Road. **A1.4 is not Satisfied**.

Performance Criteria P1: *Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:*

(a) any increase in traffic caused by the use.

(b) the nature of the traffic generated by the use.

(c) the nature of the road.

(d) the speed limit and traffic flow of the road.
(e) any alternative access to a road.
(f) the need for the use.
(g) any traffic impact assessment; and
(h) any advice received from the rail or road authority.

(a) The increase in traffic due to the proposal is estimated at 144 vpd.
A suitable right turn facility & BAL junction layout exists at the Glenstone Road / Possum Road junction to cater for the increase is traffic. Possum Road can easily cater for the estimated increase of 144 vpd to 878 vpd by 2034

(b) The nature of the traffic generated by the use is estimated at 33% commercial vehicles consistent with existing use patterns.

(c) Glenstone Road and Possum Road are of suitable standard to cope with projected traffic activity in 2034, see Section 6. The proposed road satisfies the LGAT standard for a sealed rural road.

(d) Possum Road has a speed limit of 50km/h considered suitable for the situation.

(e) No suitable alternative accesses are available.

(f) The use is consistent with the Land Use zoning for the area.

(g) This TIA finds no reason to disallow the proposal due to traffic impacts.

(h) No specific advice on traffic management has been received from BC.

In summary there are no traffic safety or capacity issues due to the proposal. **P1 is satisfied.**

Acceptable solution A1.5: Vehicular traffic must be able to enter and leave a major road in a forward direction. **A1.5 is satisfied.**

C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area

Not applicable as habitable buildings (sensitive uses) are not proposed as part of the General Industrial subdivision.

C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area

Not applicable as habitable buildings (sensitive uses) are not proposed within a road or railway attenuation area.

6.10 Department of State Growth requirements

DSG review of TIA

These reviews are required to:

- consider proposals and whether the TIA prepared satisfies DSG requirements.
- resolve any issues so the TIA can be finalised.
- enable the TIA endorsement provided by DSG to be communicated to Council as part of the Development application process.

These reviews are usually arranged by the TIA author. The email address for submissions is: Development@stategrowth.tas.gov.au

Crown landowner consent

This is to provide DSG to opportunity to check alignment of proposals with DSG objectives for the road. If the proposal aligns with DSG objectives Crown Land Consent is issued by DSG. Crown Landowner Consent is required where there is a proposed change in use of property adjacent to a state road. The website for Crown Landowner Consent is: https://www.transport.tas.gov.au/road/permits/crown_landownerconsent\

Access works permits

Developers must obtain an access works permit from DSG for proposed work within a state road reservation. Applications need to include:

- suitably design plans detailing the proposal and services affected.
- relevant design calculations for stormwater management and pavement design
- a traffic impact assessment

The website for access works permit applications is: https://www.transport.tas.gov.au/road/permits/road-access

Summary of DGS requirements

DSG has been supplied a copy of this TIA for advice on acceptability and has replied advising it has no comment, see Appendix G.

7. Recommendations and Conclusions

This traffic impact assessment has been prepared to assess the proposed 4 lot General Industrial subdivision of 80 Possum Road, Bridgewater. It is estimated the proposal will generate up to 144 vpd once fully developed.

The assessment has reviewed traffic activity at the site, existing road conditions, road safety, crash history, Austroads junction warrants and Tasmanian Planning Scheme – Brighton - Road & Railway Assets Code C3 requirements.

The proposed access site has no recorded crashes over the last 5 years and from traffic safety review and Safe System Assessment, is considered a low crash risk.

The proposal is estimated to increase traffic activity on Possum Road by some 144 vpd to 878 vpd once fully developed at the approach to Glenstone Road.

The existing Glenstone Road / Possum Road junction has a Channelised Right turn facility with ample capacity to cope with the estimated increase in traffic.

The proposed Possum Road extension and Cul-De-Sac for firefighting access are considered suitable.

Evidence is provided to demonstrate the proposal can satisfy the Tasmanian Planning Scheme - Brighton - Code C3.

Recommendations:

- *Install truck accesses to proposed lots in accordance with LGAT standard drawings TSD-R04 & 05 for rural properties.*
- *Provide street lighting in accordance with Brighton Council policy for General Industrial subdivisions.*

DSG has advised it has no comment on this TIA, see Appendix G.

The proposal will have minimal impact on Possum Road which is expected to continue to operate safely and efficiently provided the above recommendations are implemented. Based on the findings of this report the proposal is supported on traffic grounds.

Appendices

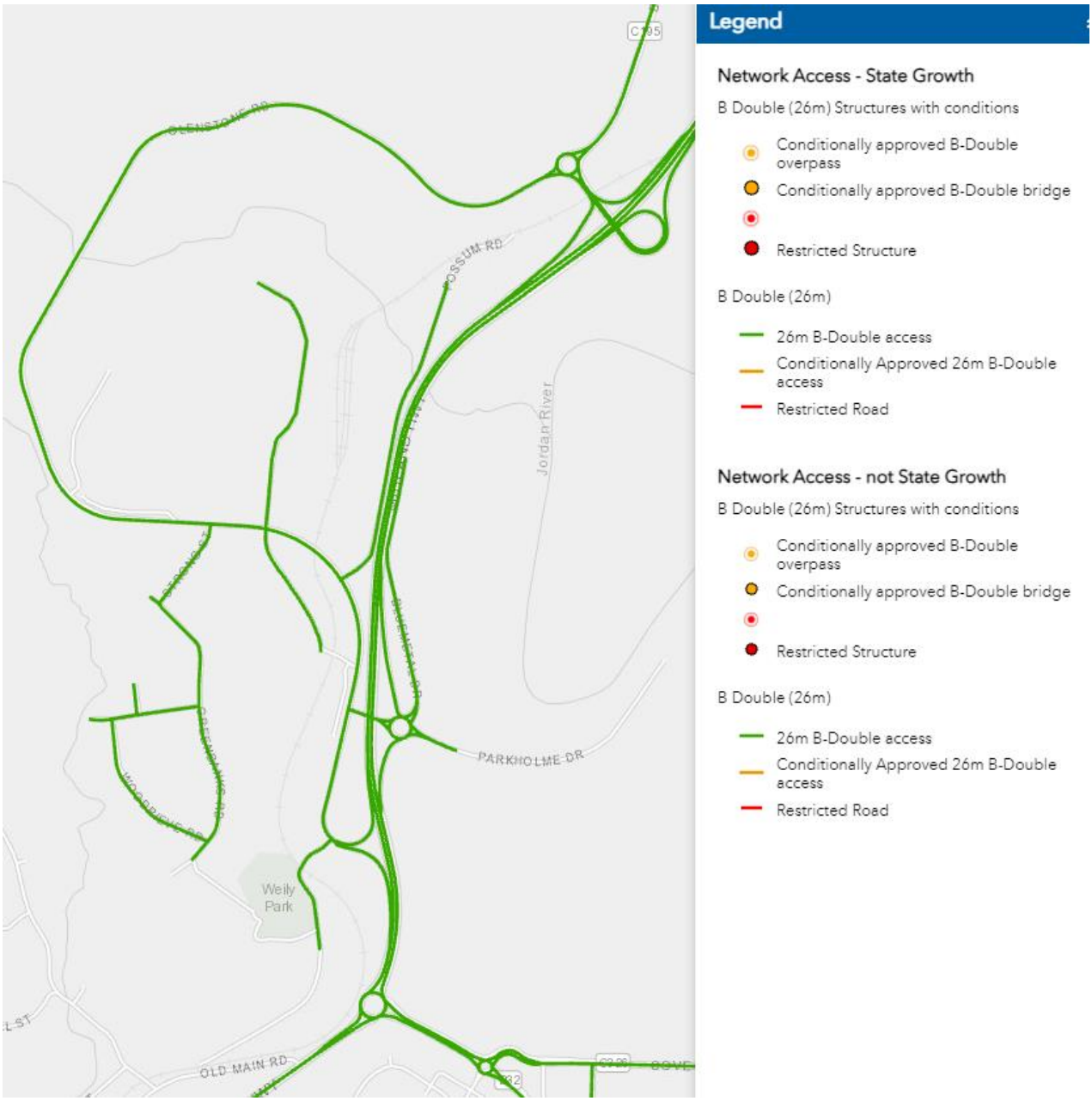
Appendix A – Proposed Plan of Subdivision

Appendix B – Austroads Level of Service descriptions

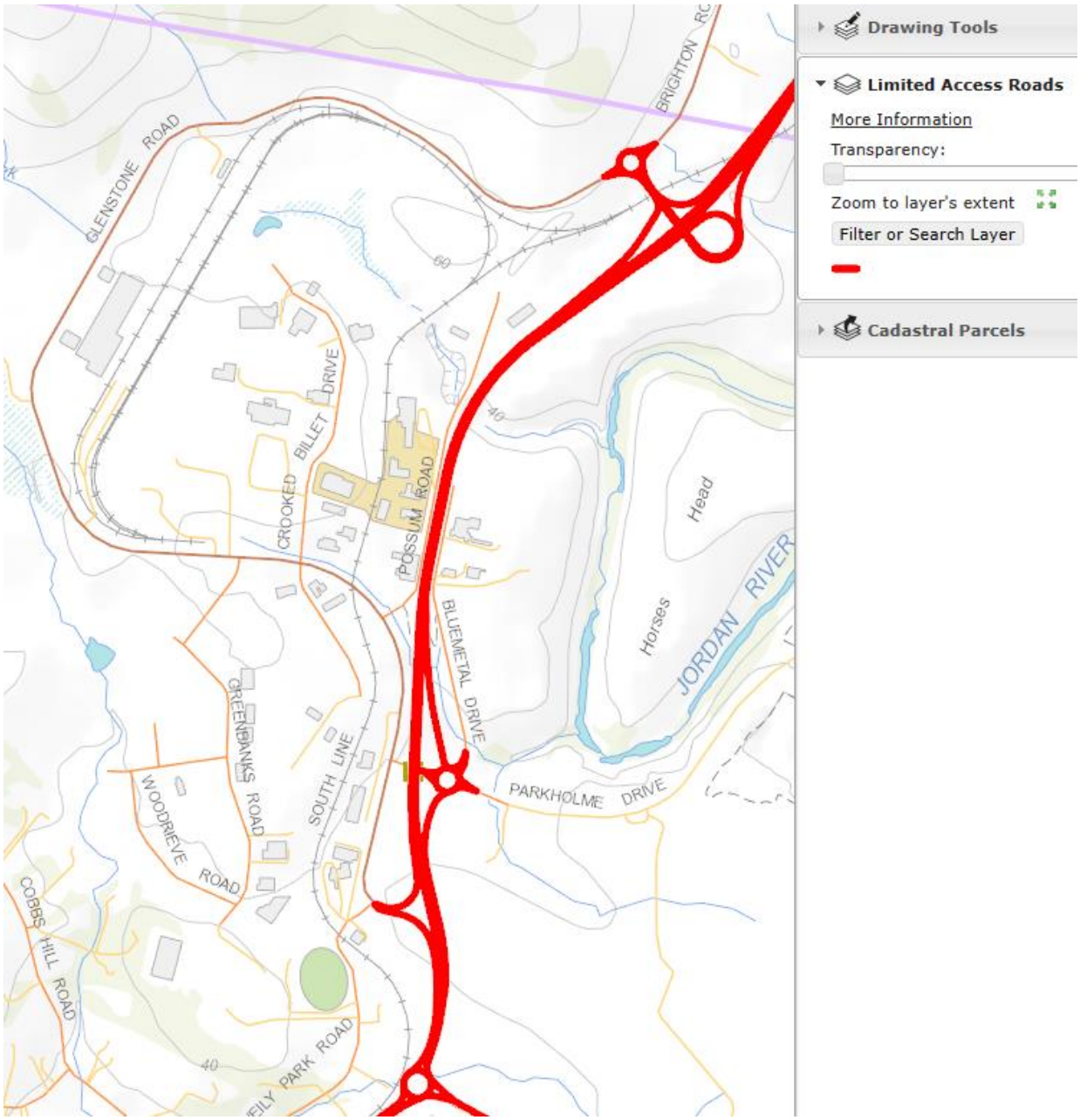
Level of service A	A condition of free-flow in which individual drivers are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to manoeuvre within the traffic stream is extremely high, and the general level of comfort and convenience provided is excellent.
Level of service B	In the zone of stable flow where drivers still have reasonable freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is a little less than with level of service A.
Level of service C	Also in the zone of stable flow, but most drivers are restricted to some extent in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of service D	Close to the limit of stable flow and approaching unstable flow. All drivers are severely restricted in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is poor, and small increases in traffic flow will generally cause operational problems.
Level of service E	Traffic volumes are at or close to capacity, and there is virtually no freedom to select desired speeds or to manoeuvre within the traffic stream. Flow is unstable and minor disturbances within the traffic stream will cause breakdown.
Level of service F	In the zone of forced flow, where the amount of traffic approaching the point under consideration exceeds that which can pass it. Flow breakdown occurs, and queuing and delays result.

Appendix C – State Road Information

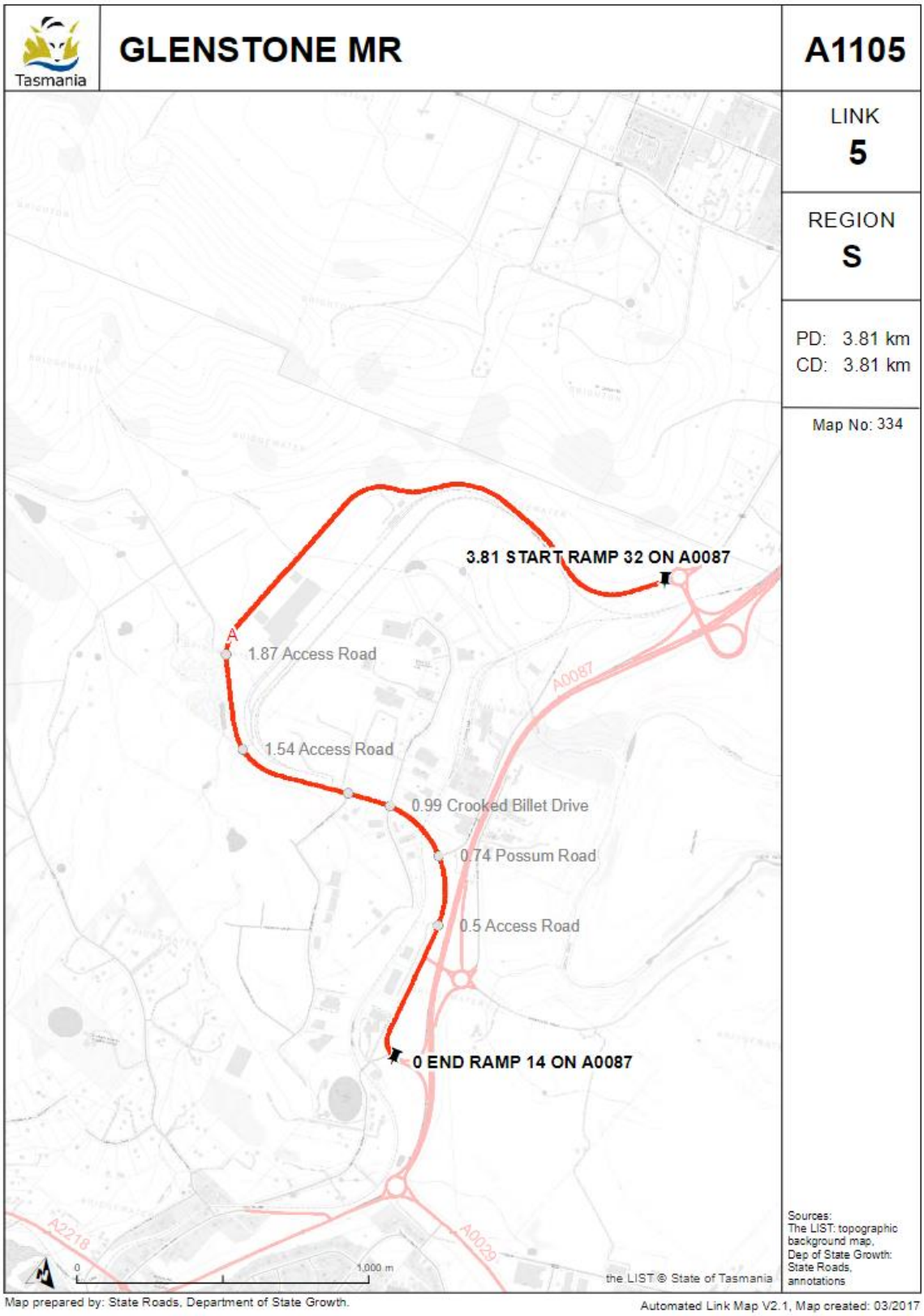
Tasmanian 26m B Double Network



Limited Access State Road Network



Department of State Growth Link Maps



Map prepared by: State Roads, Department of State Growth.

Automated Link Map V2.1, Map created: 03/2017

Appendix D – Safe Systems Assessment

Safe System Assessment **Existing situation Possum Road**

		Run-off-road	Head-on	Intersection	Trucks	Pedestrian	Cyclist	Motorcyclist	
Exposure	**Justification (AADT 1,000 vpd)**	Low traffic activity and no crash history	Low traffic activity and no crash history	Glenstone Rd (AADT 2,000 vpd) junction with Possum Rd has low traffic activity and 1 reported property damage crash	26m B Double Access	Virtually no predestrian activity	Very low cyclist activity	Low volumes	
	Score / 4	1	1	1	1	1	1	1	
Likelihood	**Justification**	870m long No Through Road with straight alignment varying in width between 9.3 and 7.5m. Delinaetion provided by kerb & channel & edge line.	870m long No Through Road with straight alignment varying in width between 9.3 and 7.5m. Delinaetion provided by kerb & channel & edge line.	High standard CHR and BAL junction.	870m long No Through Road with straight alignment varying in width between 9.3 and 7.5m. Delinaetion provided by kerb & channel & edge line.	Footpath along Eastern side and at Glenstone Road junction	870m long No Through Road with straight alignment varying in width between 9.3 and 7.5m. Delinaetion provided by kerb & channel & edge line.	870m long No Through Road with straight alignment varying in width between 9.3 and 7.5m. Delinaetion provided by kerb & channel & edge line.	
	Score / 4	1	1	1	1	1	1	1	
Severity	**Justification (50km/h speed limit)**	Low speed environment	Low speed environment	Moderate 70km/h speed limit on Glenstone Road approaches	Low speed environment	moderate speed for pedestrians	moderate speed for cyclists	moderate speed for motorcyclists	
	Score / 4	1	1	2	1	2	2	2	**Total /448**
Product	**Total Score /64**	**1**	**1**	**2**	**1**	**2**	**2**	**2**	**11**

Appendix E – Traffic Count Data

Midlands Hwy - DSG Data

A0087201 - Midland Highway 440m N Of East Derwent Hwy
City: Bridgewater
Route number: A0087

Site Data

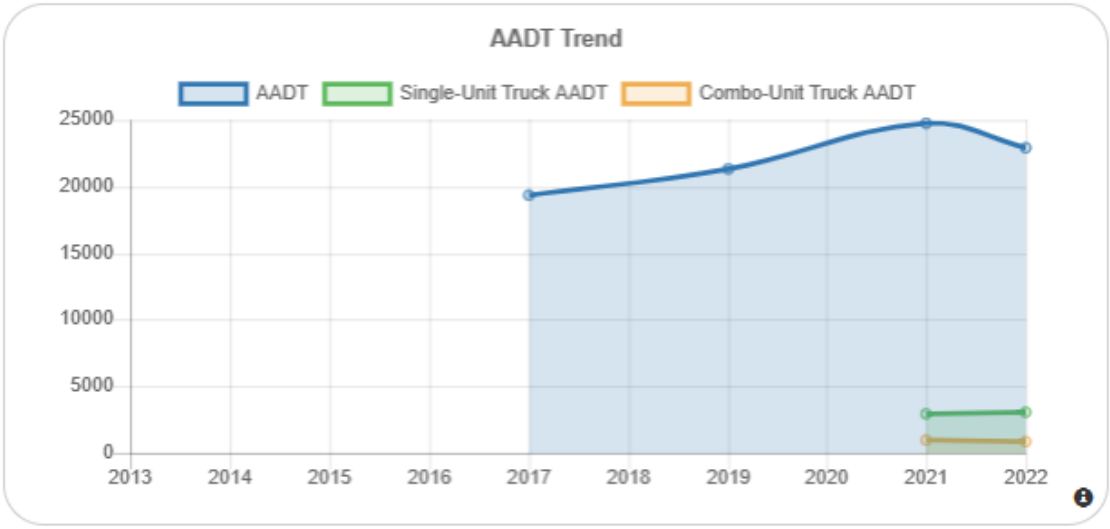
80 km/h

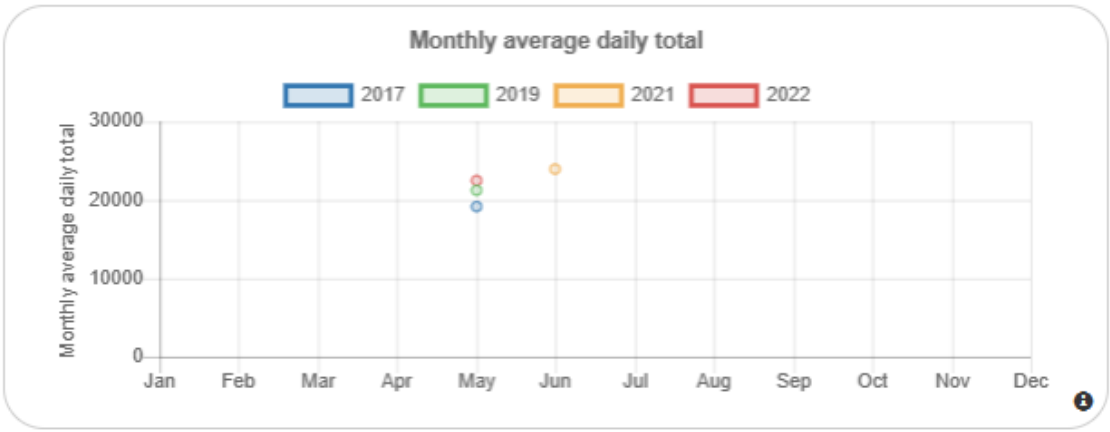
Traffic Statistics by Direction

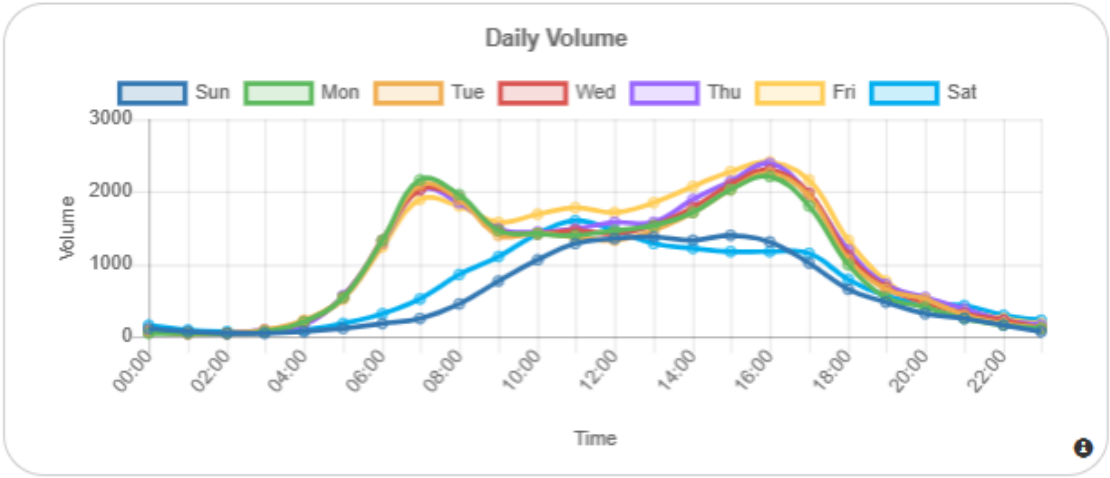
Direction	Weekday average total traffic	7-day average traffic	Weekly traffic total
North	12,185	9,464	75,709
South	12,760	9,980	79,836
Total	24,945	19,444	155,545

Annual Statistics

Data Item	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
AADT	-	-	-	-	19,407	-	21,308	-	24,740	**22,900**
% HV	-	-	-	-	12.5%	-	15.6%	-	15.5%	**17.2%**







Midlands Hwy AADT:

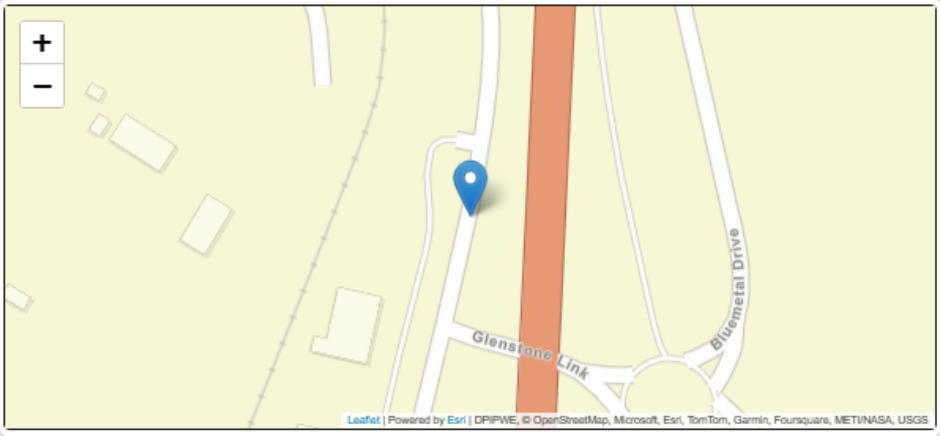
- **22,900 vpd (2022)**
- **2,400 vph (2022)**
- **17 % Trucks**
- **Compound Annual Growth Rate: 3.3%**
- **3,400 vph (2034)**

Glenstone Road, Southern end - DSG Data

A1105100 - Glenstone Main Road 70m N of Glenstone Link Rd
City: Bridgewater
Route number: A1105

Site Data

60 km/h

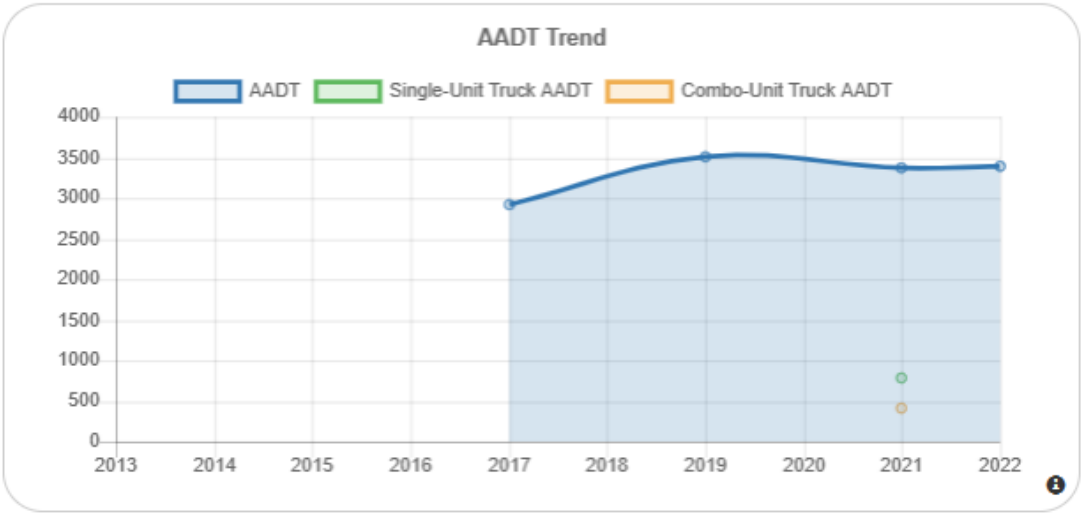


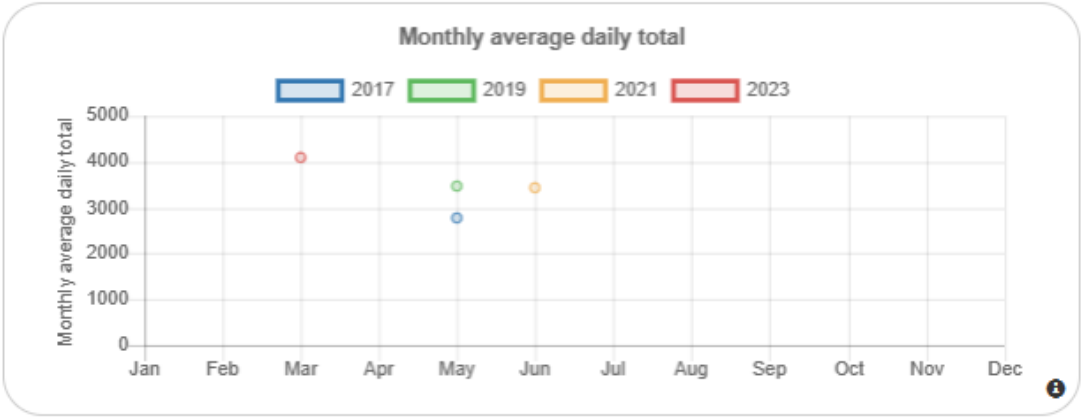
Traffic Statistics by Direction

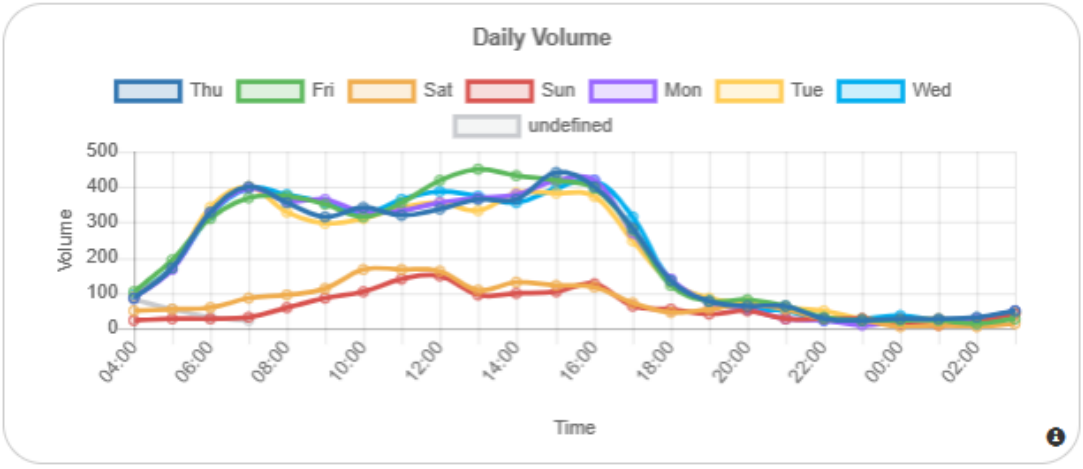
Direction	Weekday average total traffic	7-day average traffic	Weekly traffic total
North	2,093	1,765	14,117
South	2,175	1,835	14,683
Total	4,268	3,600	28,800

Annual Statistics

Data Item	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
AADT	-	-	-	-	2,918	-	3,505	-	3,373	3,390
% HV	-	-	-	-	33.8%	-	39.9%	-	35.3%	-







Glenstone Road (Sth. End) AADT:

- **2,918 vpd (2017)**
- **3,390 vph (2022)**
- **35 % Trucks**
- **Compound Annual Growth Rate: 0.9%**

Glenstone Road, Northern end - DSG Data

A1105120 - Glenstone Main Road 630m S of Midland Hwy
City: Brighton
Route number: A1105

Site Data

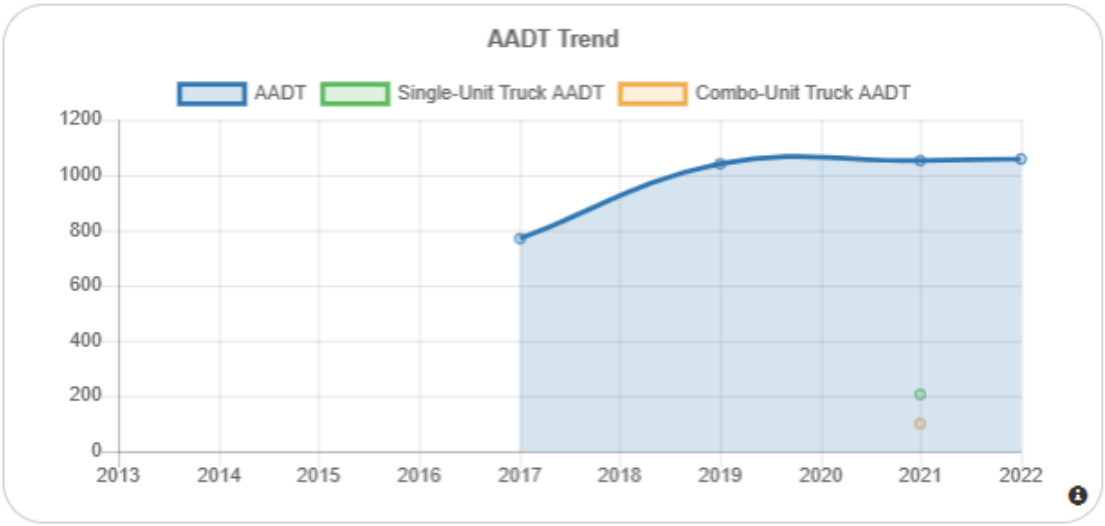
60 km/h

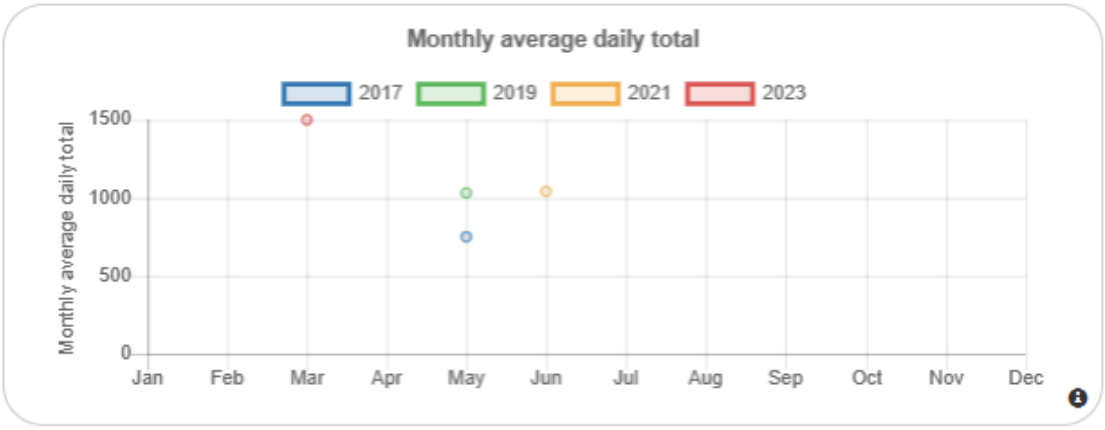
Traffic Statistics by Direction

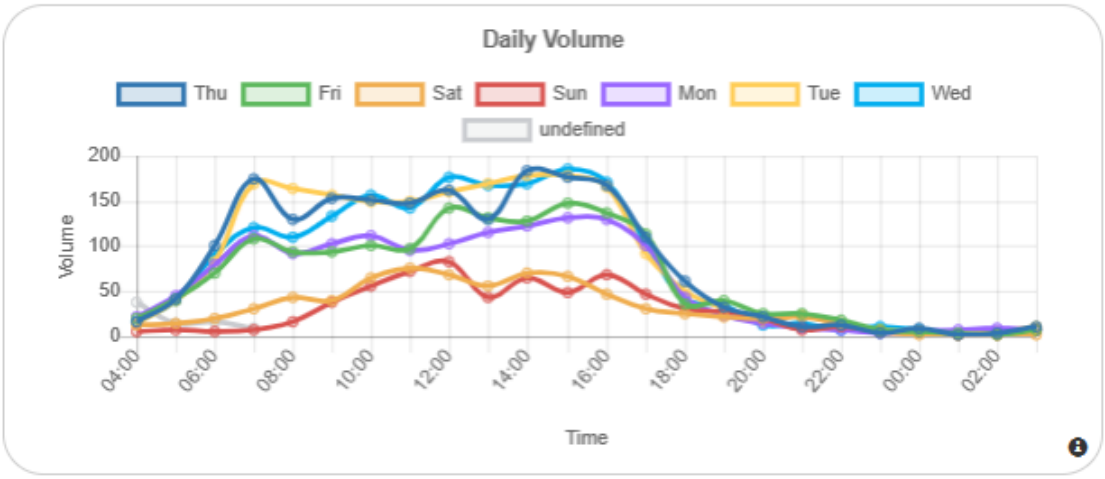
Direction	Weekday average total traffic	7-day average traffic	Weekly traffic total
East	752	653	5,221
West	773	667	5,338
Total	1,525	1,320	10,559

Annual Statistics

Data Item	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
AADT	-	-	-	-	772	-	1,041	-	1,052	1,057
% HV	-	-	-	-	35.6%	-	34.4%	-	28.8%	-







Glenstone Road (Nth. End) AADT:

- **772 vpd (2017)**
- **1,057 vph (2022)**
- **29 % Trucks**
- **Compound Annual Growth Rate: 6.6%**

Estimated Possum Road Traffic 2024 & 2034

Without proposal

Lot	Area (Ha)	Building GFA (m2)	AADT (vpd)	Peak (vph)
19	19.0	3000	135	23
25	1.5	400	18	3
27	1.5	700	32	5
29	3.3	1150	52	9
31	3.4	800	36	6
33	1.5	550	25	4
35	1.4	470	21	4
37	13.0	1500	68	11
39	28.0	4000	180	30
71	6.5	930	42	7
77	8.0	1000	45	8
80	15.0	1800	81	14
Total			**734**	**122**

Appendix F – Public & Fire Fighting Access

Tasmanian Planning Scheme - State Planning Provisions

C13.6.2 Public and fire fighting access

Objective:	That access roads to, and the layout of roads, tracks and trails, in a subdivision: (a) allow safe access and egress for residents, fire fighters and emergency service personnel; (b) provide access to the bushfire-prone vegetation that enables both property to be defended when under bushfire attack, and for hazard management works to be undertaken; (c) are designed and constructed to allow for fire appliances to be manoeuvred; (d) provide access to water supplies for fire appliances; and (e) are designed to allow connectivity, and where needed, offering multiple evacuation points.

Acceptable Solutions	**Performance Criteria**
A1 (a) TFS or an accredited person certifies that there is an insufficient increase in risk from bushfire to warrant specific measures for public access in the subdivision for the purposes of fire fighting; or (b) A proposed plan of subdivision showing the layout of roads, fire trails and the location of property access to building areas, is included in a bushfire hazard management plan that: (i) demonstrates proposed roads will comply with Table C13.1, proposed property accesses will comply with Table C13.2 and proposed fire trails will comply with Table C13.3 and (ii) is certified by the TFS or an accredited person.	**P1** A proposed plan of subdivision shows access and egress for residents, fire-fighting vehicles and emergency service personnel to enable protection from bushfires, having regard to: (a) appropriate design measures, including: (i) two way traffic; (ii) all weather surfaces; (iii) height and width of any vegetation clearances; (iv) load capacity; (v) provision of passing bays; (vi) traffic control devices; (vii) geometry, alignment and slope of roads, tracks and trails; (viii) use of through roads to provide for connectivity; (ix) limits on the length of cul-de- sacs and dead-end roads; (x) provision of turning areas; (xi) provision for parking areas; (xii) perimeter access; and (xiii) fire trails; and (b) the provision of access to: (i) bushfire-prone vegetation to permit the undertaking of hazard management works; and (ii) fire fighting water supplies; and (c) any advice from the TFS.

Tasmanian Planning Scheme - State Planning Provisions

Table C13.1: Standards for Roads

Element		Requirement
A.	Roads.	Unless the development standards in the zone require a higher standard, the following apply: (a) two-wheel drive, all-weather construction; (b) load capacity of at least 20 tonnes, including for bridges and culverts; (c) minimum carriageway width is 7m for a through road, or 5.5m for a dead-end or cul-de-sac road; (d) minimum vertical clearance of 4m; (e) minimum horizontal clearance of 2m from the edge of the carriageway; (f) cross falls of less than 3 degrees (1:20 or 5%); (g) maximum gradient of 15 degrees (1:3.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads; (h) curves have a minimum inner radius of 10m; (i) dead-end or cul-de-sac roads are not more than 200m in length unless the carriageway is 7m in width; (j) dead-end or cul-de-sac roads have a turning circle with a minimum 12m outer radius; and (k) carriageways less than 7m wide have 'No Parking' zones on one side, indicated by a road sign that complies with *Australian Standard AS1743:2018 Road signs-Specifications*.

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Table C13.2: Standards for Property Access

Element		Requirement
A.	Property access length is less than 30m; or access is not required for a fire appliance to access a fire fighting water point.	There are no specified design and construction requirements.
B.	Property access length is 30m or greater; or access is required for a fire appliance to a fire fighting water point.	The following design and construction requirements apply to property access: (a) all-weather construction; (b) load capacity of at least 20t, including for bridges and culverts; (c) minimum carriageway width of 4m; (d) minimum vertical clearance of 4m; (e) minimum horizontal clearance of 0.5m from the edge of the carriageway; (f) cross falls of less than 3 degrees (1:20 or 5%); (g) dips less than 7 degrees (1:8 or 12.5%) entry and exit angle; (h) curves with a minimum inner radius of 10m; (i) maximum gradient of 15 degrees (1:3.5 or 28%) for sealed roads, and 10 degrees (1:5.5 or 18%) for unsealed roads; and (j) terminate with a turning area for fire appliances provided by one of the following: (i) a turning circle with a minimum outer radius of 10m; or (ii) a property access encircling the building; or (iii) a hammerhead "T" or "Y" turning head 4m wide and 8m long.
C.	Property access length is 200m or greater.	The following design and construction requirements apply to property access: (a) the requirements for B above; and (b) passing bays of 2m additional carriageway width and 20m length provided every 200m.
D.	Property access length is greater than 30m, and access is provided to 3 or more properties.	The following design and construction requirements apply to property access: (a) complies with requirements for B above; and (b) passing bays of 2m additional carriageway width and 20m length must be provided every 100m.

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Table C13.3: Standards for Fire Trails

Element		Requirement
A.	All fire trails.	The following design and construction requirements apply: (a) all-weather, 4-wheel drive construction; (b) load capacity of at least 20t, including for bridges and culverts; (c) minimum carriageway width of 4m; (d) minimum vertical clearance of 4m; (e) minimum horizontal clearance of 2m from the edge of the carriageway; (f) cross falls of less than 3 degrees (1:20 or 5%); (g) dips less than 7 degrees (1:8 or 12.5%) entry and exit angle; (h) curves with a minimum inner radius of 10m; (i) maximum gradient of 15 degrees (1:3.5 or 28%) for sealed fire trails, and 10 degrees (1:5.5 or 18%) for unsealed fire trails; (j) gates if installed at fire trail entry, have a minimum width of 3.6m, and if locked, keys are provided to TFS; and (k) terminate with a turning area for fire appliances provided by one of the following: (i) a turning circle with a minimum outer radius of 10m; or (ii) a hammerhead "T" or "Y" turning head 4m wide and 8m long.
B.	Fire trail length is 200m or greater.	The following design and construction requirements apply: (a) the requirements for A above; and (b) passing bays of 2m additional carriageway width and 20m length provided every 200m.

Appendix G – DSG Advice on Acceptability

Mon 28/10/2024 9:59 AM

RE: TIA for proposed 80 Possum Road Subdivision near Glenstone Road

HA Head, Alex <Alex.Head@stategrowth.tas.gov.au>
To Richard Burk; Development

Hi Richard/Development Brighton,

The Department has no comment from an access/traffic engineering perspective.

Regards,

Alex Head | A/Traffic Engineering Liaison | Traffic Engineering (south)
Traffic Operations | State Roads | Department of State Growth
76 Federal St, North Hobart Tas 7000
Phone: (03) 6165 5205 | alex.head@stategrowth.tas.gov.au
www.transport.tas.gov.au

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In recognition of the deep history and culture of this island, I acknowledge and pay my respects to all Tasmanian Aboriginal people; the past, and present custodians of the Land.

From: Richard Burk <richard.burk@trafficandcivil.com.au>
Sent: Wednesday, October 23, 2024 10:35 AM
To: Development <Development@stategrowth.tas.gov.au>
Subject: TIA for proposed 80 Possum Road Subdivision near Glenstone Road

Hello Developments,

Please advise on acceptability of attached TIA

Regards

RICHARD BURK
BE(civil) MTraffic Dip.Man. MIE Aust CPEng
DIRECTOR
Traffic and Civil Services Pty Ltd

TRAFFIC & CIVIL SERVICES