



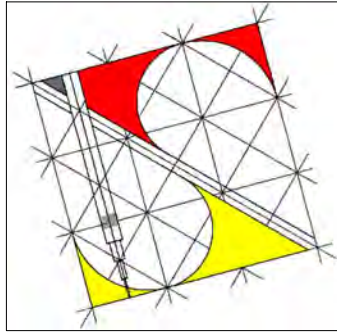
Brighton Council

ATTACHMENTS

Planning Authority

12th April 2023





urban space designers

Building Design And Drafting

DEVELOPMENT APPLICATION

(RENOVATE TO CONVERT EXISTING STRUCTURE INTO CHILD CARE AND COMMUNITY HALL)

3 - STAGES

MIXED USE DEVELOPMENT PROJECT

AT

23 MENIN DRIVE, BRIGHTON 7030

STAGE 1 - EARLY LEARNING CENTRE

STAGE 2 - PUBLIC COMMUNITY HALL

**STAGE 3 - FUTURE DEVELOPMENT (Not part of
current application)**

AREA MATRIX

		AREA SQM.
1	TOTAL SITE AREA	8202
2	GREEN AREA	5530
3	BLDG FOOT PRINT TOTAL BUILDING AREA EXISTING + PROPOSED (Child care + Community Hall)	900
4	CONCRETE/DRIVEWAY/PARKING	1772
5	DEVELOPMENT BUILT UP EARLY LEARNING CENTRE	
	EXISTING BUILDING	650.00
	PROPOSED OFFICE/RECEPTION	50.00
	COMMUNITY HALL	
	EXISTING BUILDING	200
	PROPOSED BUILDING	0
	TOTAL AREA	900.00

DEVELOPMENT SUMMARY

DEVELOPMENT SUMMARY		
SNO.	ITEM DESCRIPTION	AREA SQM.
	LOT AREA	8202.00
	GROUND COVER	900.00
	GREEN AREA	5530.00
1	IMPERVIOUS SURFACE	
	BUILDING GRD COVER	900.00
	CONCRETE/DRIVEWAY	1772.00
	TOTAL IMPERVIOUS SURFACE	2672.00
	PERCENTAGE %	32.58
2	PERVIOUS SURFACE	
	GREEN AREA	5530.00
	TOTAL PERVIOUS SURFACE	5530.00
	PERCENTAGE %	67.42

DESIGN COMPONENTS

STAGE WISE COMPONENTS		
SNO.	ITEM DESCRIPTION	BUILT UP AREA SQM.
STAGE 1 (EARLY LEARNING CENTRE) (EXISTING + PROPOSED)		700
	4 LEARNING AREAS, TOILETS, KITCHEN, LAUNDRY, INDOOR GAMES AREA, QUITE AREA STORAGES, STAFF ROOM, KIDS PLAY AREA	
STAGE 2 (COMMUNITY HALL)		200
	CONGREGATION HALL, KITCHEN, TOILET	
SNO.	ITEM DESCRIPTION	LAND AREA SQM.
STAGE 3 (LAND UNDER FUTURE DEVELOPMENT)		3958

NOTE: -
THE EXTERNAL WALLS, ROOF PROFILE AND 3 FIRE PLACES ARE AS PER EXISTING TO RETAIN THE HERITAGE ASPECT OF THE BUILDING, WHEREAS THE INTERNAL PARTITION WALLS HAVE BEEN ADJUSTED TO SUIT THE SPACES REQUIRED FOR EACH AREA.
EXISTING INTERNAL WALL AND ROOF CLADDING SHALL BE REMOVED TO VERIFY THE STRUCTURAL INTEGRITY, REMOVING ANY ASBESTOS AND UPGRADING THE STUCTURE TO INCREASE THE SERVICE LIFE OF THE BUILDING.

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KEY PROJECT STATISTICS & DATA

FOLIO PLAN	DATE OF ISSUE	REGISTERED NO.	FOLIO REFERENCE
2	(26-FEB-2015)	SP169004	C.T.168256-200
TITLE EDITION	DATE OF ISSUE	FOLIO	VOLUME
	(26-FEB-2015)	1	169004
LOCATION	23, MENIN DRIVE, BRIGHTON, TAS-7030		

FINISHES : COLOR / MATERIAL SCHEDULE

EXTERIOR WALLS :	
EARLY LEARNING CETRE :	RETAINING EXISTING FINISH (Weather Board, Butter Ridge Color)
COMMUNITY HALL :	RETAINING EXISTING FINISH (Weather Board, Butter Ridge Color)
WINDOWS:	All the Window Sizes and Design shall be Retained as per Original . The new windows and frames shall be of Aluminium with Double Glazed Low - E Glass.
INTERNAL WALLS:	PAINTED (Grandpiano Quarter Strength)
ROOF/GUTTER/FACIA:	Colorbond Corrugated (Roof Dulux wilderness and Facia White Color)
BOUNDARY FENCE:	1350 mm high Timber PICKET FENCE WITH TOP RAIL (Butter Ridge Color)

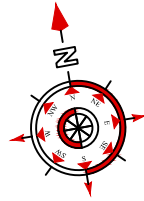
NOTE: EXTERIOR FACADE WILL NOT BE CHANGED AS CONSIDERING THE EXISTING BUILT UP IS A HERITAGE STRUCTURE. (Only change is Adding new doors by replacing existing windows.

DRAWING LIST

SHEET NO SHEET TITLE:	
DA-01	KEY DATA
DA-02	EXISTING SITE PLAN
DA-03	PROPOSED SITE PLAN
DA-04	PROPOSED SITE PLAN (Early Learning Centre+Community hall)
DA-04A	PROPOSED SITE PLAN (Early Learning Centre+Community hall+Parking+Landscaping)
DA-04B	PROPOSED PARKING PLAN (Early Learning Centre+Community hall)
DA-04C	PROPOSED LANDSCAPING PLAN (Childcare+Community hall)
DA-05	LAYOUT PLAN Early Learning Centre
DA-06	DIMENSIONED PLAN Early Learning Centre
DA-07	LAYOUT PLAN COMMUNITY HALL
DA-08	DIMENSIONED PLAN COMMUNITY HALL
DA-09	ROOF PLAN
DA-10	PARKING PLAN
DA-11	DEVELOPMENT STAGES
DA-12	ELEVATIONS
DA-13	DEMOLITION PLAN
DA-14	DEMOLITION PLAN
DA-15	DOOR WINDOW SCHEDULE
DA-16	SIGNAGE

DEVELOPMENT APPLICATION

NORTH



CLIENT:

BRIGHT CAMP PTY LTD

SITE ADDRESS:

23 MENIN DRIVE, BRIGHTON, TAS 7030

GENERAL NOTES:

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ISSUE	DESCRIPTION	DATE
R01	Relocation of Entry Driveway. Accessible parking revised. Additional 2 Wheeler Parking provided. Fence height revised from 1800mm to 1350mm	22-11-2022

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

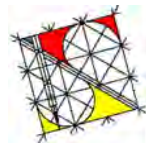
TITLE:

KEY DATA

Sheet Original:

A - 3

SCALE NTS



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Building Design And Drafting

TASMANIA . VICTORIA

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License number: 381246362

Drawn By:

G. KAUR

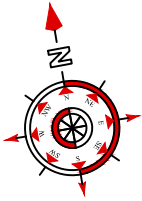
Drawing Number:

DA-01

PLOT DATE: 30-09-2022

DEVELOPMENT APPLICATION

NORTH



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SITE ADDRESS:

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ISSUE	DESCRIPTION	DATE
R01	Updated Existing Layout plan	25-03-2022

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

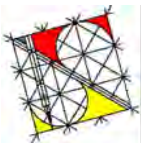
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EXISTING SITE PLAN

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Drawing Number:

DA-02

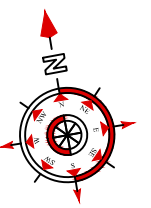
PLOT DATE: 30-09-2022



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NORTH



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PTY LTD

23 MENIN DRIVE,
BRIGHTON, TAS 7030

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File Number:
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PROPOSED SITE PLAN

A - 3

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TASMANIA . VICTORIA

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DA-03

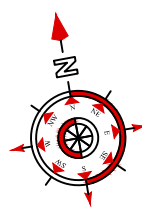
PLOT DATE: 30-09-2022



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NORTH



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SITE ADDRESS:

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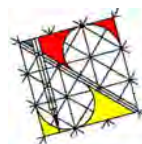
Brightcamp/MeninDrive/Arch/15/02/2022

TITLE:
PROPOSED SITE PLAN
EARLY LEARNING CENTRE & COMMUNITY HALL

Sheet Original:

A - 3

SCALE 1:250



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Drawn By:

G. KAUR

Drawing Number:

DA-04

PLOT DATE: 30-09-2022

Nail = 59.02
Gnd = 59.73

PROPOSED WIDENING
OF EXISTING CONCRETE
CROSSOVER BY 3000MM

1000mm HIGH SIGNAGE WALL
WITH 2.106x0.900mm SIGN PANEL

EXISTING
CROSSOVER

10095

3200

GRASS

GRASS

2500mm WIDE PEDESTRIAN ENTRANCE

1500mm WIDE WICKET GATE
PEDESTRIAN ENTRANCE

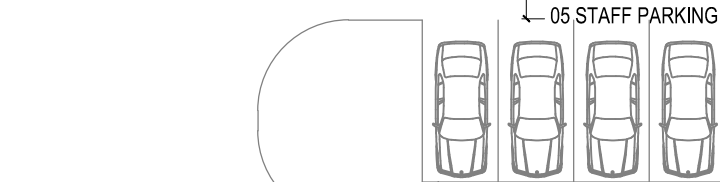
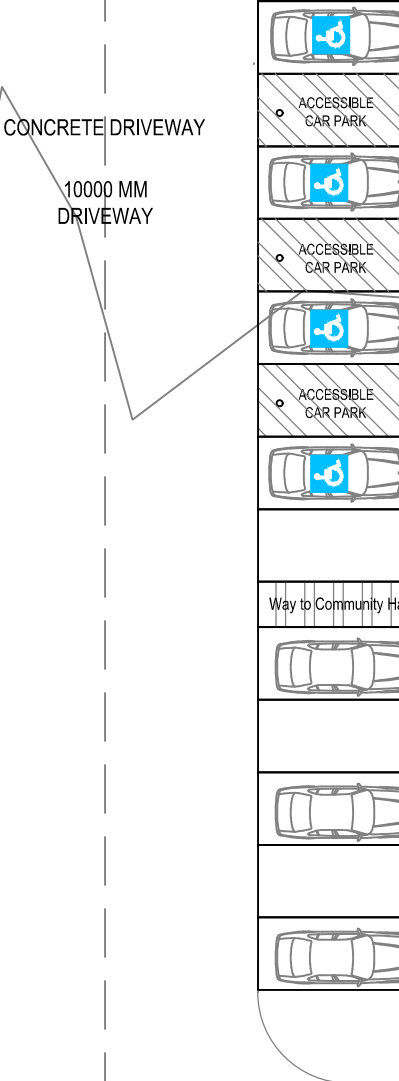
VEHICULAR ACCESS
TO COMMUNITY HALL, CHILDCARE &
STAGE 3 DEVELOPMENT

MAINTAINED GREEN AREA
REFER LANDSCAPE PLAN
(DA-04C) FOR DETAILS

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
955 SQM
(Not part of the current application)

CONCRETE DRIVEWAY

10000 MM
DRIVEWAY

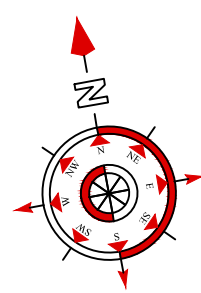


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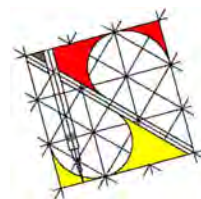
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PROPOSED SITE PLAN
EARLY LEARNING CENTRE & COMMUNITY HALL

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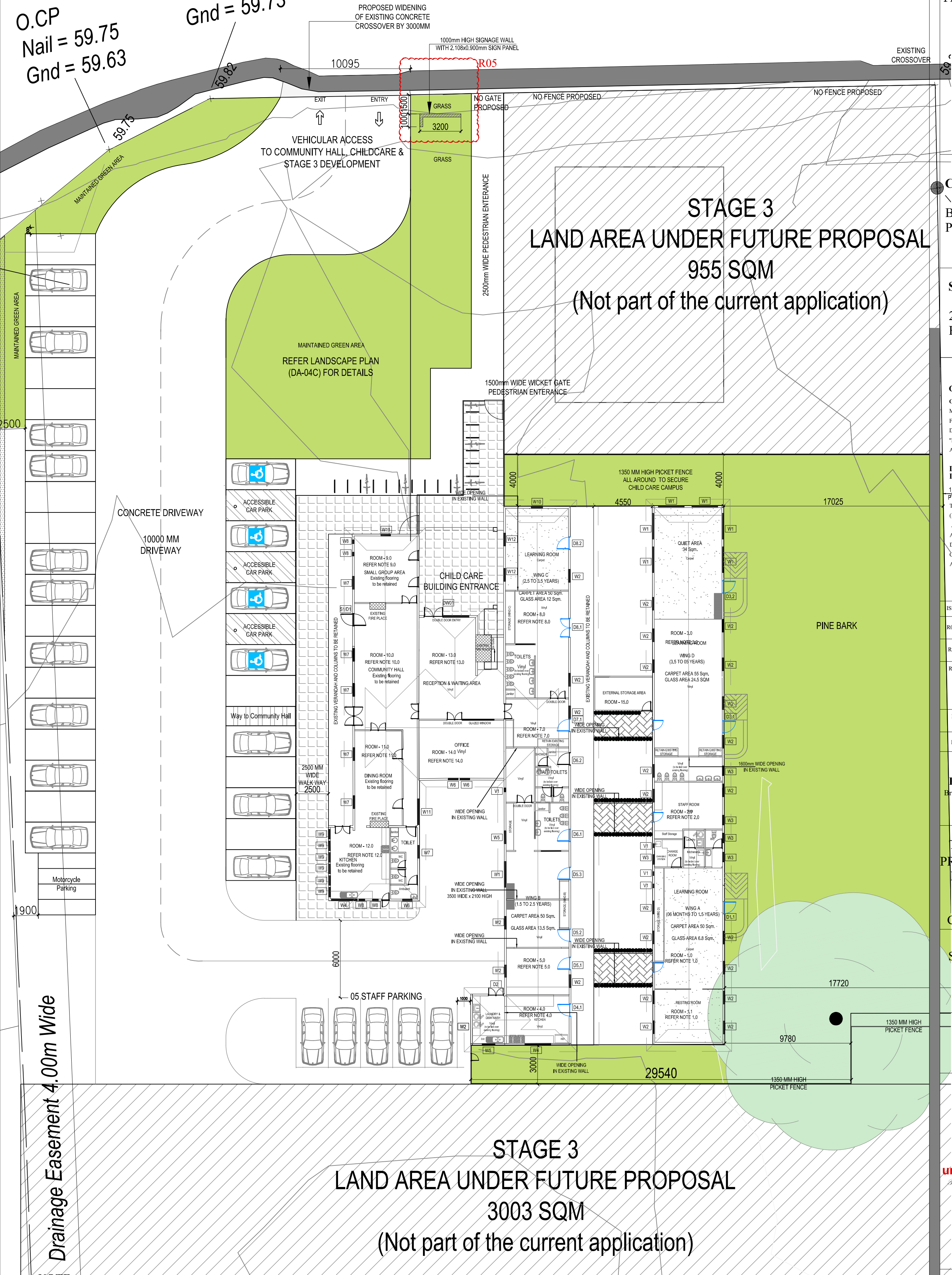
DA-04A

PLOT DATE: 30-09-2022

MENIN DRIVE

CP
Nail = 59.82
Gnd = 59.73

O.CP
Nail = 59.75
Gnd = 59.63



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PROJECT TEAM

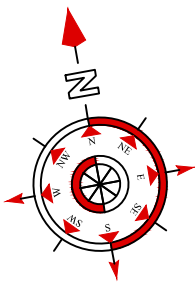
Land Surveyor: ROGERSON AND BIRCH
ph: 0362485898 REGISTERED SURVEYORS

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DEVELOPMENT APPLICATION

NORTH



CLIENT:

BRIGHT CAMP PTY LTD

SITE ADDRESS:

23 MENIN DRIVE, BRIGHTON, TAS 7030

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R05	Signage Wall updated	31-01-2023

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

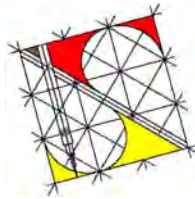
TITLE:

PROPOSED PARKING PLAN

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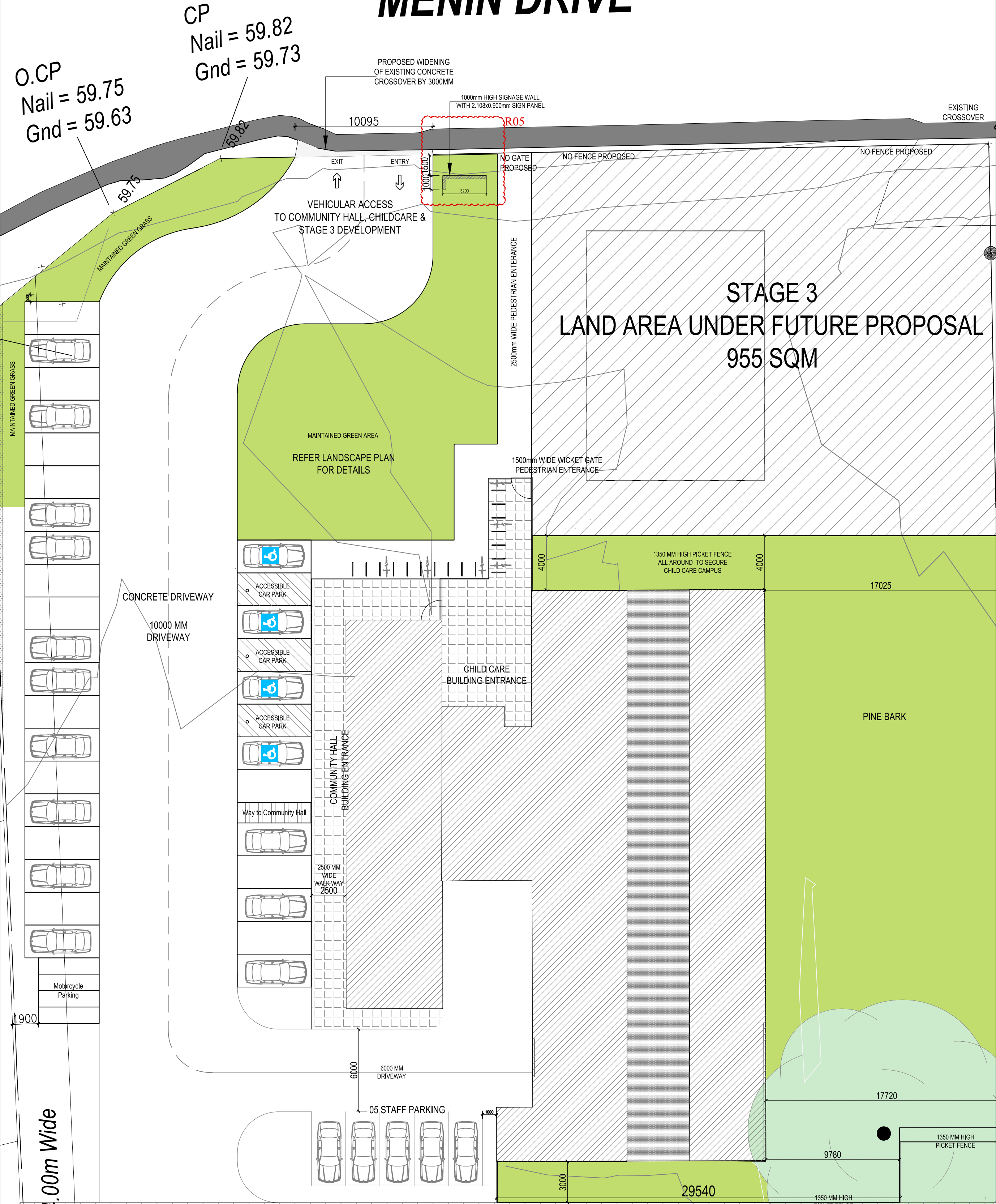
G. KAUR

Drawing Number:

DA-04B

PLOT DATE: 30-09-2022

MENIN DRIVE



TOTAL CAR PARKING MATRIX

CHILD CARE = 22 CAR PARK SPACES
Community Hall = 10 CAR PARK SPACES
STAFF Parking = 05 CAR PARK SPACES

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
3003 SQM
(Not part of the current application)

Drainage Easement 4.00m Wide

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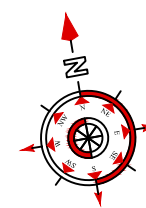
Land Surveyor: ROGERSON AND BIRCH
ph: 0362485898

REGISTERED SURVEYORS

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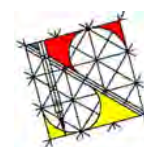
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R04	Fence Relocation Plantation updated	15-12-2022
R05	Signage Wall updated	31-01-2023

Brightcamp/Menindrive/Arch/15/02/2022

PROPOSED LANDSCAPE PLAN

A - 3

SCALE 1:250



TASMANIA . VICTORIA

TASMANIA . VICTORIA

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DOI: 10.1002/for

G. KAUR

DA-04C

PLOT DATE: 30-09-2022

Gnd = 59.73













PROPOSED WIDENING OF EXISTING CONCRETE CROSSOVER BY 3000MM

1000mm HIGH BLOCKWORK WALL
WITH 2100 x 900mm SIGN PANEL
R05

EXISTING CROSSOVER 36

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
955 SQM
(Not part of the current application)

LEGEND

SNO.	SPECIES	HEIGHT	NOTATION	MARK
1	COTULA HISPIDA	10CM		(P1)
2	HELICHRYSUM ARGYROPHYLLUM	20CM		(P2)
3	BANKSIA	150CM		(P3)
4	PIS.PACE (CARPENTERUTUS)	5CM		(P4)
5	CALISTEMON LITTLE JOHN	180CM		(P5)
6	THYME HONEYMYRTLE (MELALEUCA THYMIFOLIA)	120CM		(P6)
7	MYOPORUM PARVIFOLIUM	30CM		(P7)
8	ALOE VERA	80CM		(P8)
9	KINGDZATYTHOR - KANGAROO SAW	100CM		(P9)
10	FESTUGA BLAUCA	90CM		(P10)
11	DYMONDIA	5CM		(P11)
12	LAVENDER	100CM		(P12)

NOTE: PICKET FENCE OF 1800MM HIGH PAINTED TO MATCH THE EXTERIOR OF EXISTING BUILDING IS PROPOSED ON THE EASTERN, SOUTHERN AND NORTHERN SIDE OF THE PROPERTY.

REFER LANDSCAPE DRAWING TO FOLLOW FENCE DETAIL

WOODEN WICKED GATE 1500MM WIDE IS PROPOSED AT THE FRONT BOUNDARY FENCE FOR THE PEDESTRIAN ACCESS.

TE|DRIVEWAY

000 MM
IVEWAY

NOTE: -

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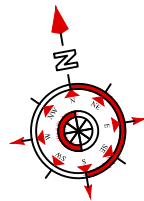
PROJECT TEAM

Land Surveyor: ROGERSON AND BIRCH
ph: 0362485898

REGISTERED SURVEYORS

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PTY LTD

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ISSUE	DESCRIPTION	DATE
R01	Updated Layout plans	25-03-2022

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

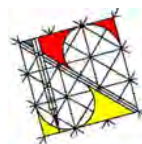
TITLE:

LAYOUT PLAN
EARLY LEARNING
CENTRE

Sheet Original:

A - 3

SCALE 1:200



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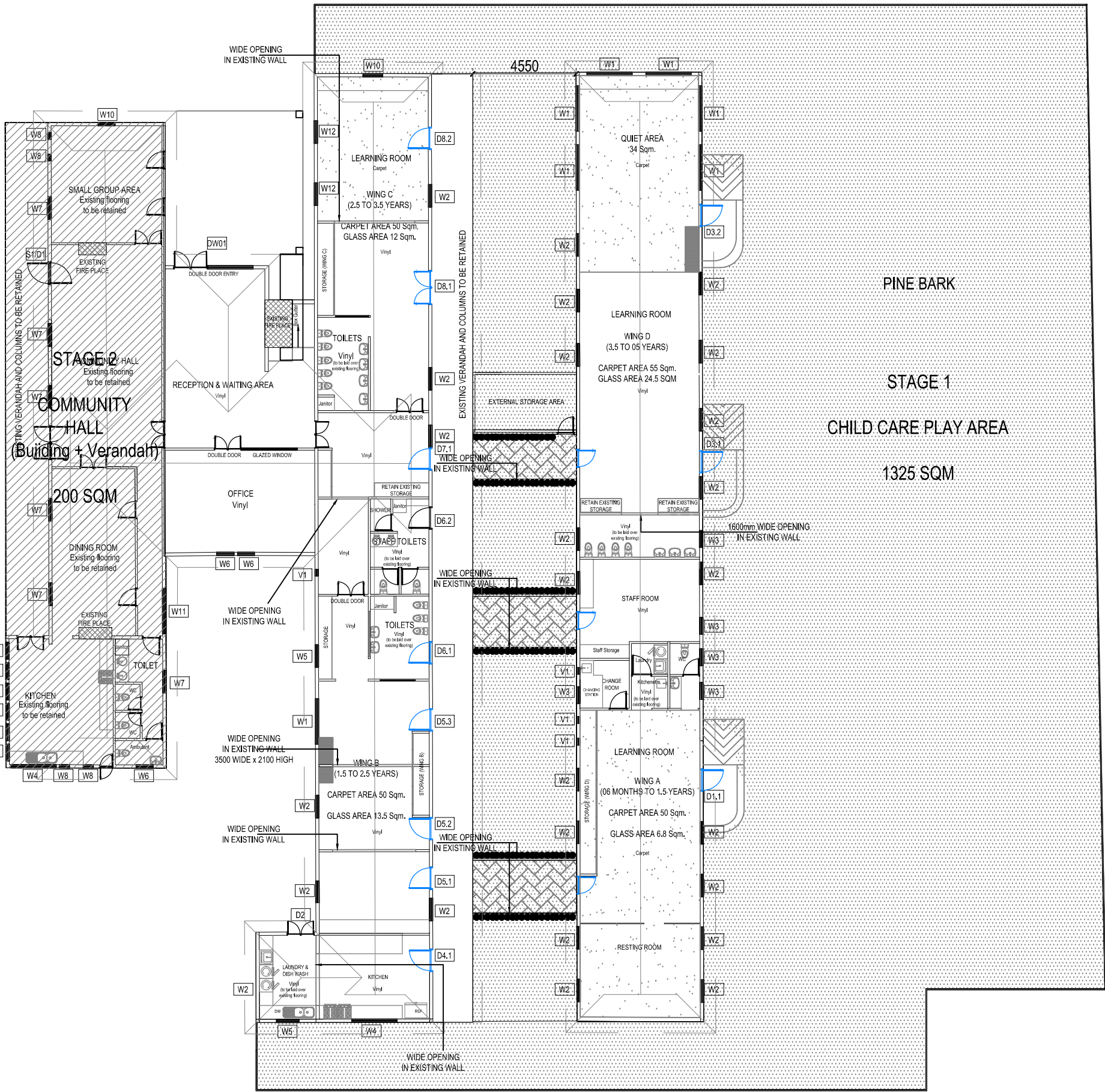
G. KAUR

Drawing Number:

DA-05

PLOT DATE: 30-09-2022

STAGE 1

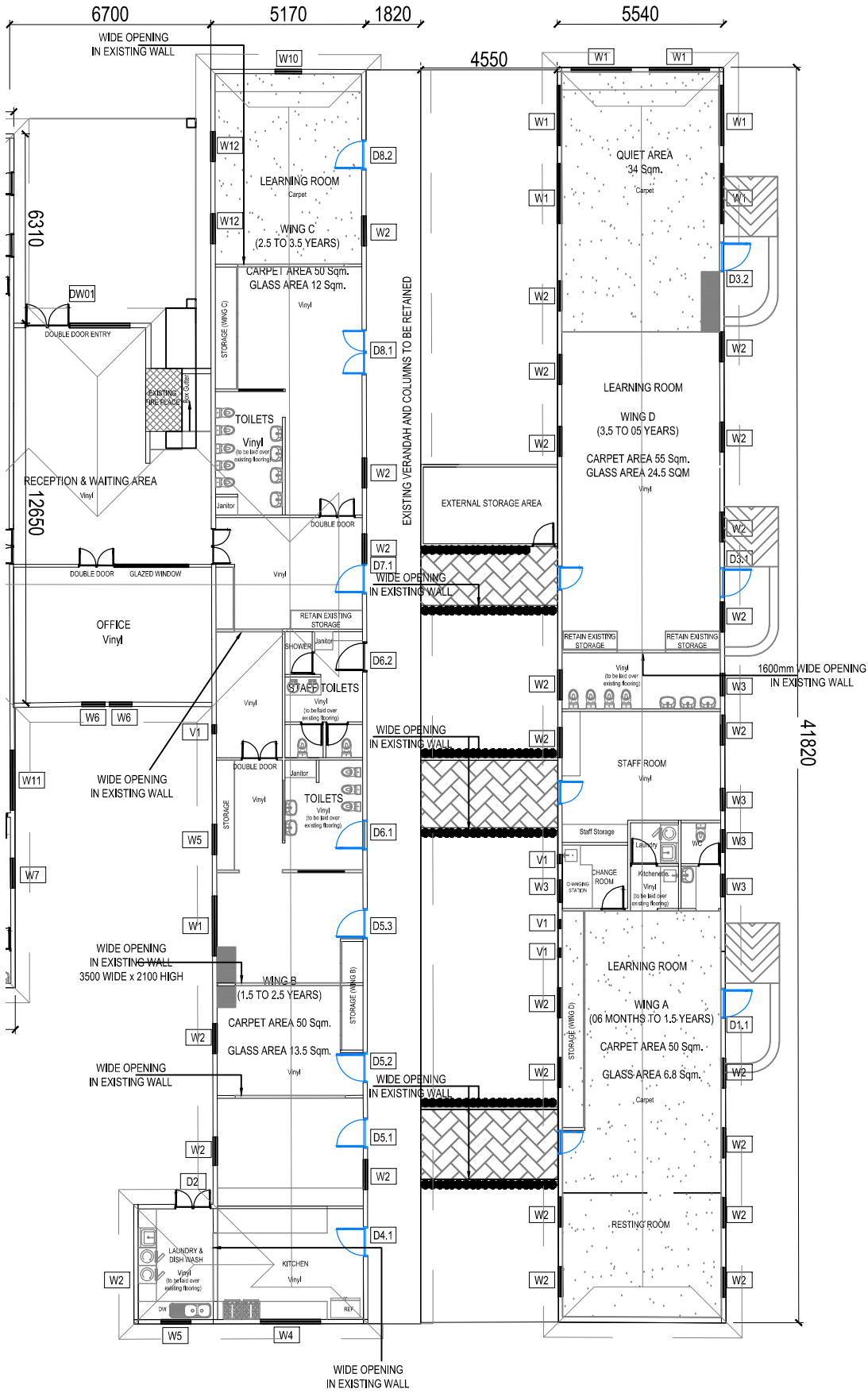


LAYOUT PLAN
EARLY LEARNING CENTRE
SCALE 1:200

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STAGE 1



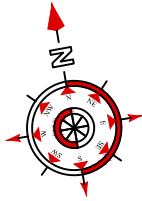
DIMENSION PLAN
EARLY LEARNING CENTRE
SCALE 1:200

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ISSUE	DESCRIPTION	DATE
R01	Updated Layout plans	25-03-2022

File Number:

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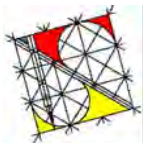
TITLE:

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EARLY LEARNING
CENTRE

Sheet Original:

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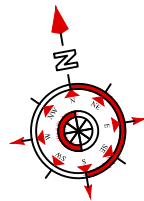
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Drawing Number:

DA-06

PLOT DATE: 30-09-2022





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ISSUE	DESCRIPTION	DATE
R01	Updated Layout plans	25-03-2022

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

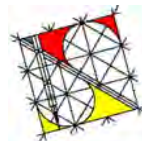
TITLE:

LAYOUT PLAN
COMMUNITY HALL

Sheet Original:

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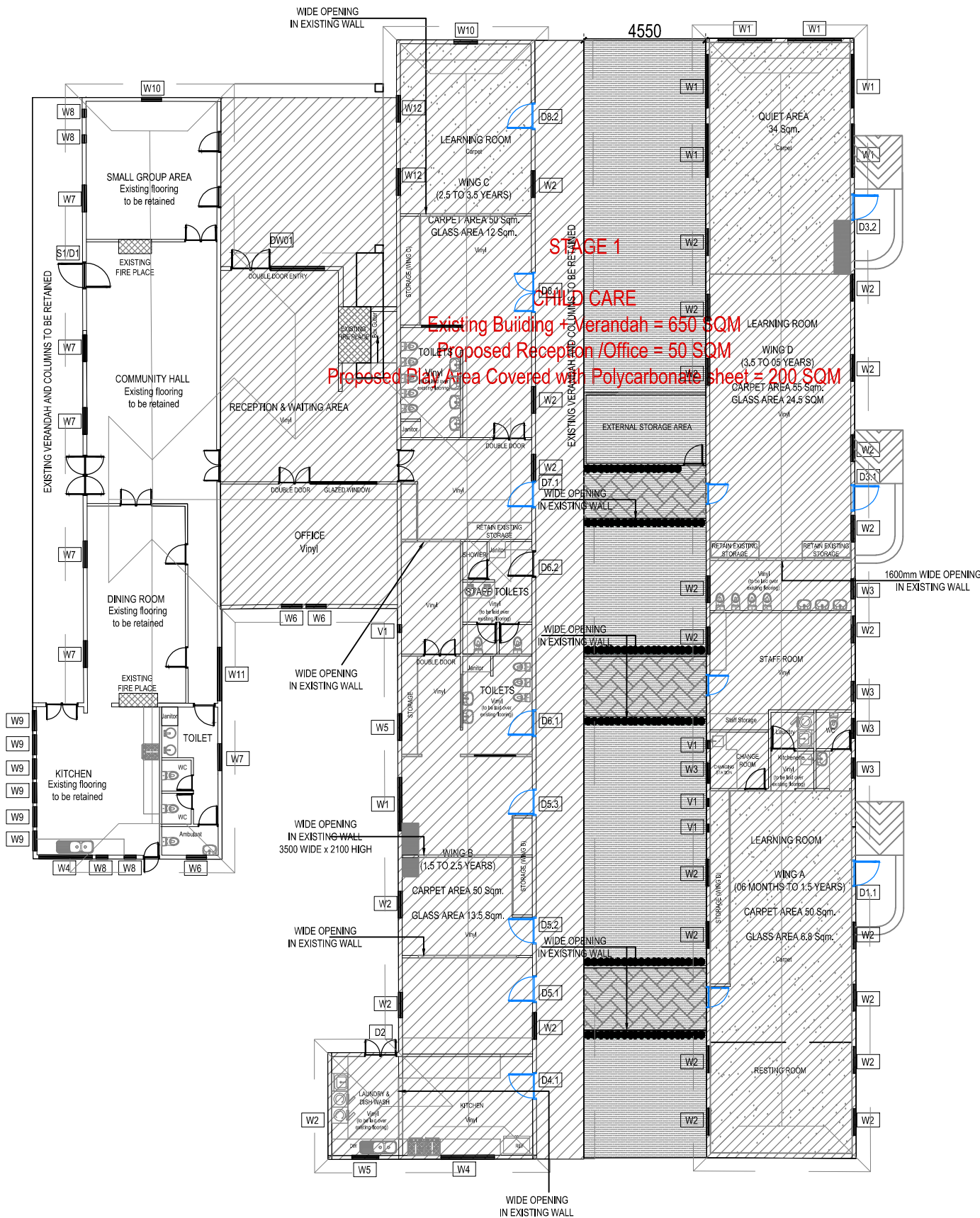
G. KAUR

Drawing Number:

DA-07

PLOT DATE: 30-09-2022

STAGE 2

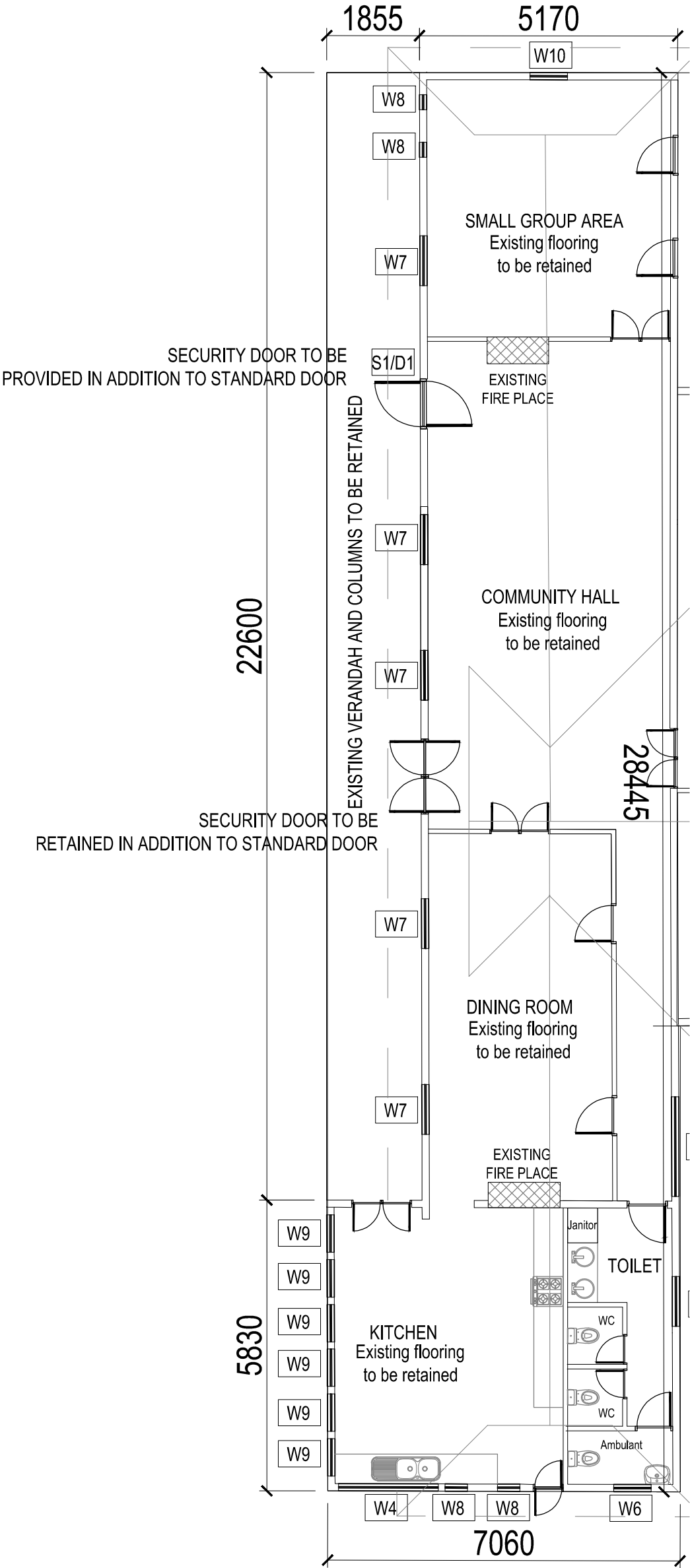


LAYOUT PLAN COMMUNITY HALL
SCALE 1:200

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STAGE 2



DIMENSION PLAN COMMUNITY HALL
SCALE 1:100

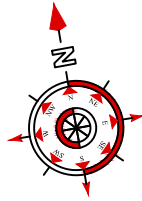
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ISSUE	DESCRIPTION	DATE
R01	Updated Layout plans	25-03-2022

File Number:

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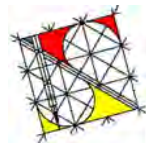
TITLE:

DIMENSIONED PLAN
COMMUNITY HALL

Sheet Original:

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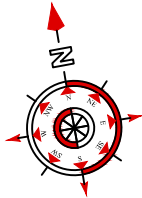
DA-08

PLOT DATE: 30-09-2022



DEVELOPMENT APPLICATION

NORTH



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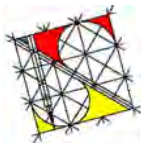
TITLE:

ROOF PLAN

Sheet Original:

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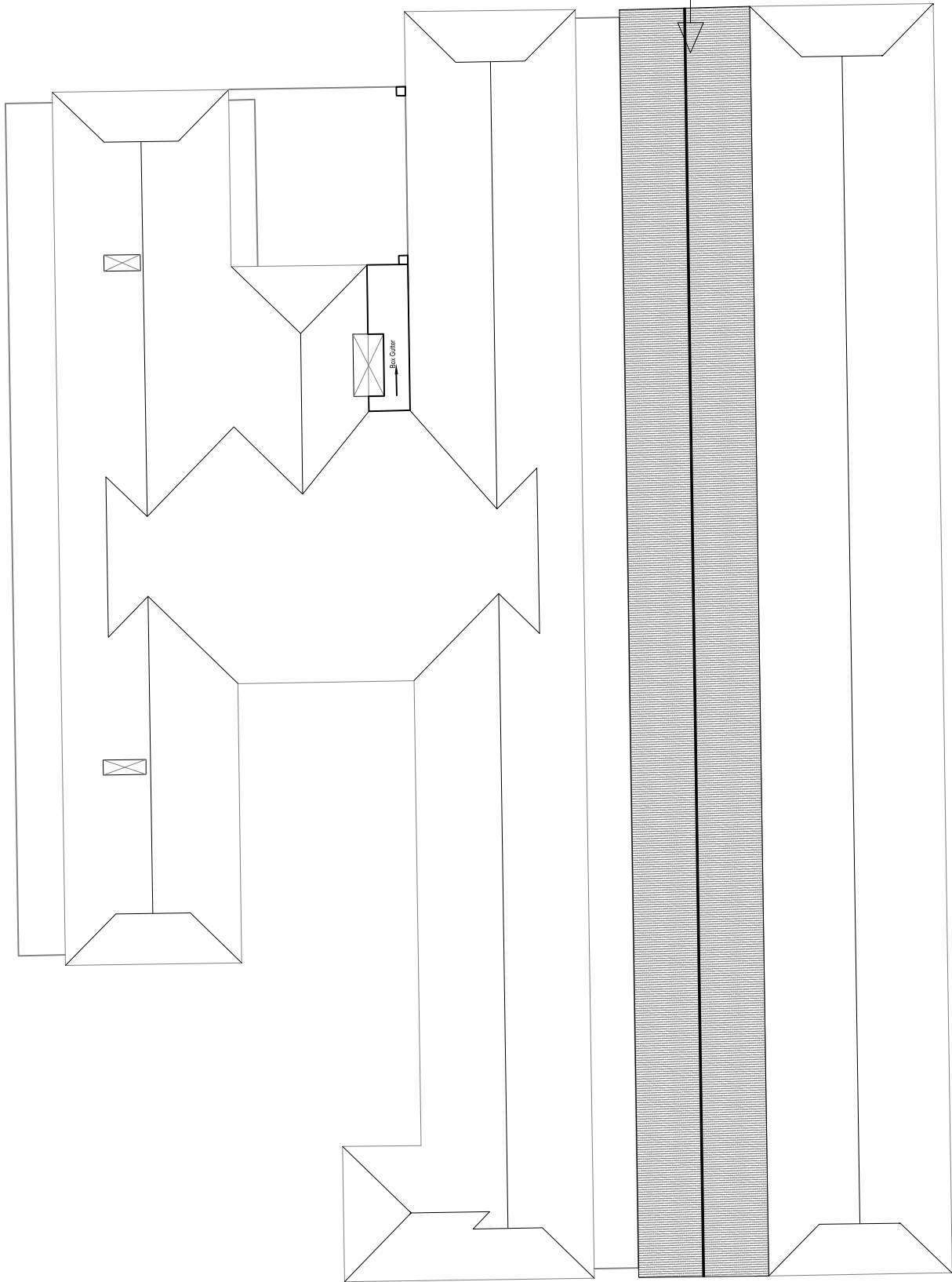
G. KAUR

Drawing Number:

DA-09

PLOT DATE: 30-09-2022

PROPOSED
POLYCARBONATE SHEET
COVERED PLAY AREA

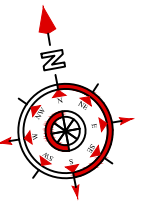


ROOF PLAN
SCALE 1:200

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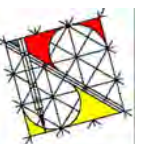
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PARKING PLAN
LANDSCAPING
LEGEND

A - 3

SCALE 1:250



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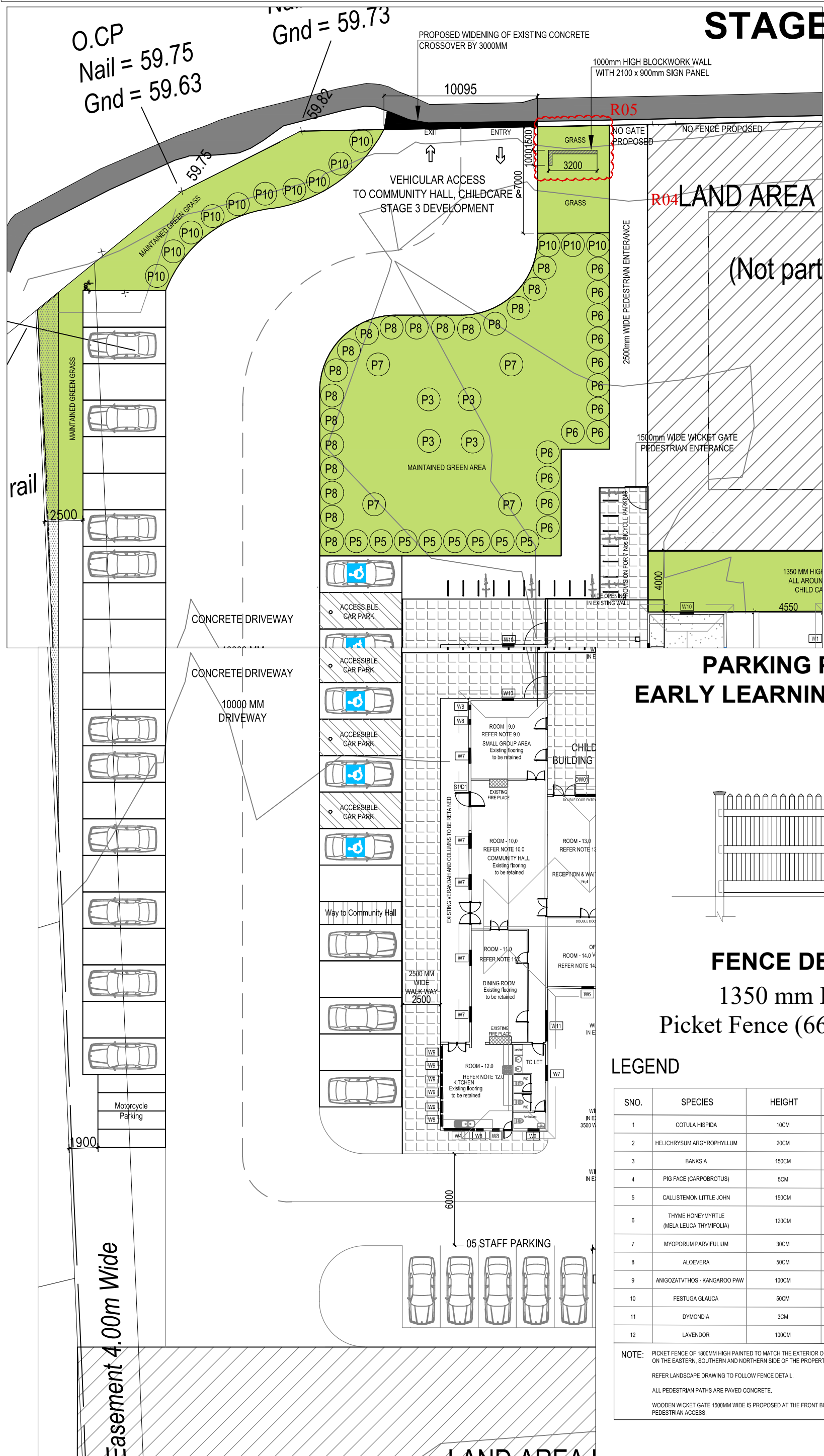
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DA-10

PLOT DATE: 30-09-2022



Land Surveyor: ROGERSON AND BIRCH
ph: 0362485898

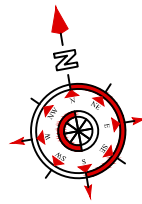
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DEVELOPMENT APPLICATION

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ISSUE	DESCRIPTION	DATE
R01	Relocation of Entry Driveway. Accessible parking revised. Additional 2 Wheeler Parking provided. Fence height revised from 1800mm to 1350mm	22-11-2022
R02	Fence Relocation	15-12-2022
R03	Signage Wall updated	31-01-2023

File Number:

Brightcamp/Menin Drive/Arch/15/02/2022

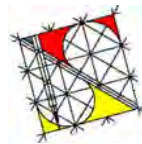
TITLE:

DEVELOPMENT STAGES

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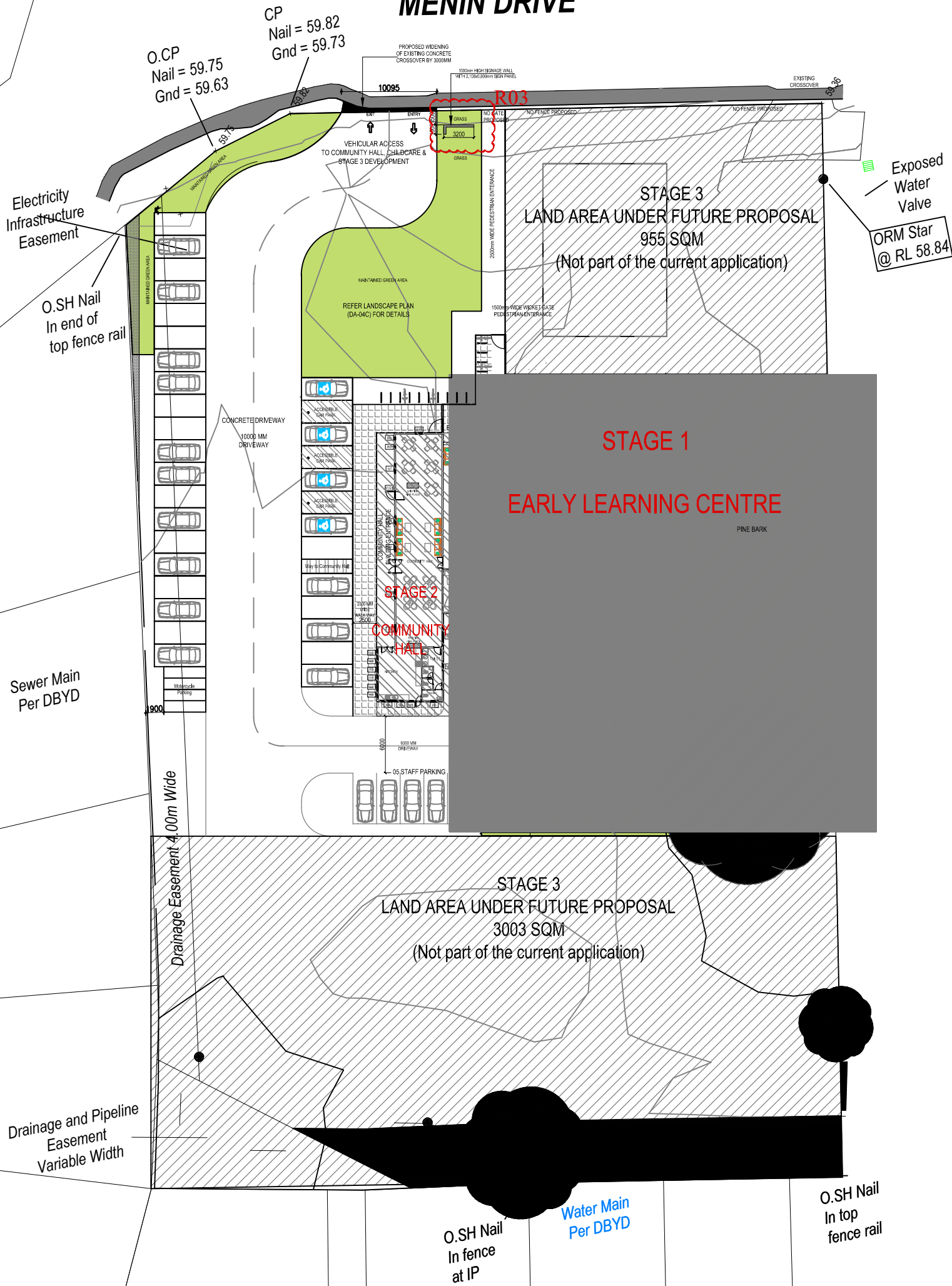
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Drawing Number:

DA-11

PLOT DATE: 30-09-2022

MENIN DRIVE



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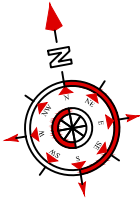
PROJECT TEAM

Land Surveyor:

ROGERSON AND BIRCH
ph: 0362485898

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ISSUE	DESCRIPTION	DATE
R01	Updated Elevations	25-03-2022
R02	Signage of Front Fence shown towards North Elevation	31-12-2022
R03	Signage Revised	31-01-2023

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

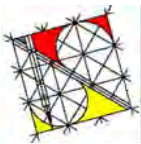
TITLE:

ELEVATIONS

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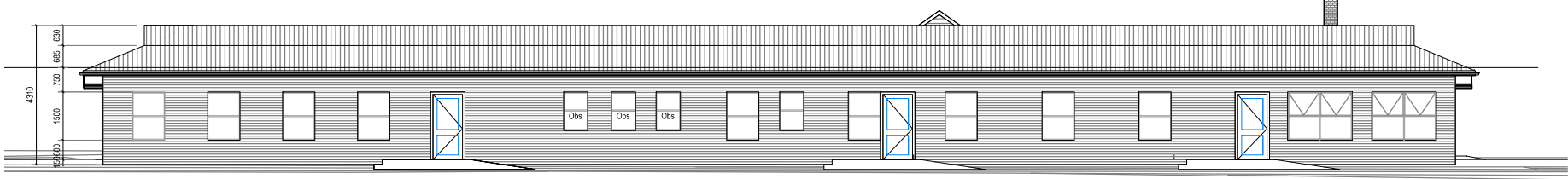
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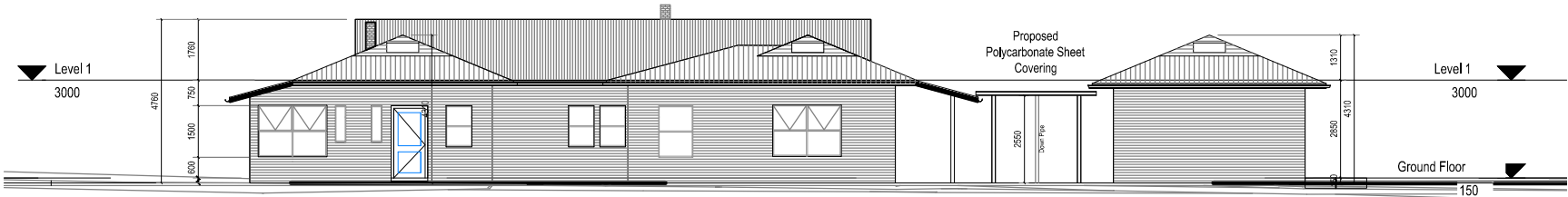
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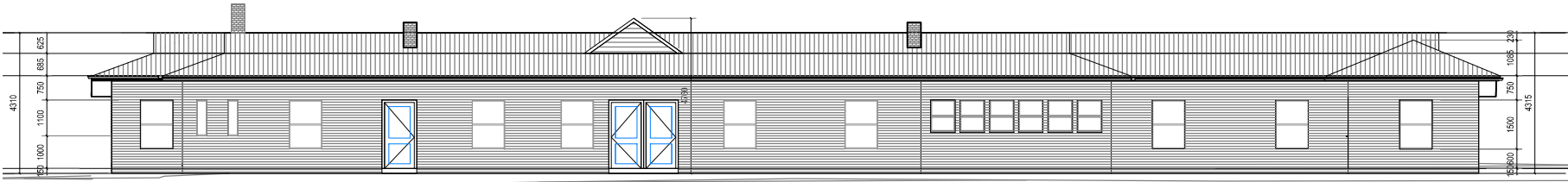
PLOT DATE: 30-09-2022



EAST ELEVATION
SCALE:- 1:200



SOUTH ELEVATION
SCALE:- 1:200



WEST ELEVATION
SCALE:- 1:200



NORTH ELEVATION
SCALE:- 1:200



SIGNAGE - NORTH ELEVATION
SCALE:- 1:200

NOTE: -

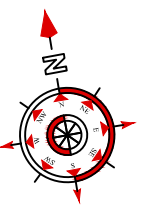
ALL THE WINDOW SIZES AND DESIGN SHALL BE RETAINED AS PER ORIGINAL. THE NEW WINDOWS AND FRAMES SHALL BE OF TIMBER WITH DOUBLE GLAZED LOW - E GLASS.
THE EXTERNAL WALLS, ROOF PROFILE AND 3 FIRE PLACES ARE AS PER EXISTING TO RETAIN THE HERITAGE ASPECT OF THE BUILDING, WHEREAS THE INTERNAL PARTITION WALLS HAVE BEEN ADJUSTED TO SUIT THE SPACES REQUIRED FOR EACH AREA.
EXISTING INTERNAL WALL AND ROOF CLADDING SHALL BE REMOVED TO VERIFY THE STRUCTURAL INTEGRITY, REMOVING ANY ASBESTOS AND UPGRADING THE STUCTURE TO INCREASE THE SERVICE LIFE OF THE BUILDING.

Doors shown in Blue color to be changed to Glazed Timber Doors.

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ISSUE	DESCRIPTION	DATE

Brightcamp/Menindrive/Arch/15/02/2022

DEMOLITION PLAN

A - 3

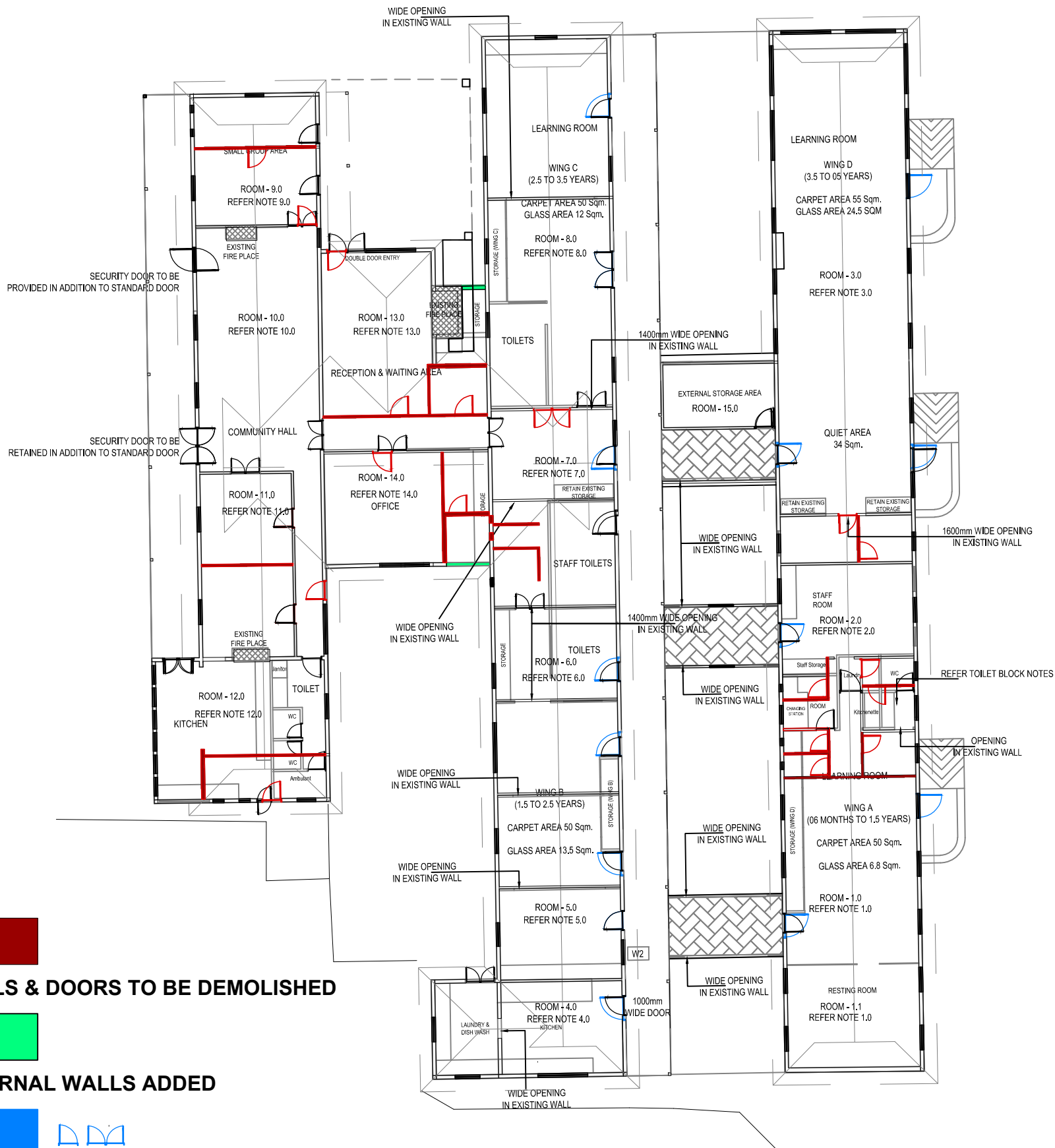
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license number: 381246362

G. KAUR

DA-13

PLOT DATE: 30-09-2022



WALLS & DOORS TO BE DEMOLISHED

EXTERNAL WALLS ADDED

DOORS SHOWN IN BLUE COLOR
TO BE CHANGED TO 1m GLAZED
TIMBER DOORS.

STAFF TOILET DOOR D 6.2
TO BE CHANGED TO 1m
TIMBER DOOR.


NOTE: -

- ALL WALLS SHOWN IN RED COLOR IN THE EXISTING LAYOUT AND SUPERIMPOSED LAYOUT TO BE DEMOLISHED.
- FOR DOOR/ WINDOW SIZES, PLEASE REFER DOOR WINDOW SCHEDULE.
- DEMOLITION WORK TO BE CARRIED IN ACCORDANCE WITH THE TASMANIAN BUILDING ACT 2016

NOTE: -

THE EXTERNAL WALLS, ROOF PROFILE AND 3 FIRE PLACES ARE AS PER EXISTING TO RETAIN THE HERITAGE ASPECT OF THE BUILDING, WHEREAS THE INTERNAL PARTITION WALLS HAVE BEEN ADJUSTED TO SUIT THE SPACES REQUIRED FOR EACH AREA.

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13

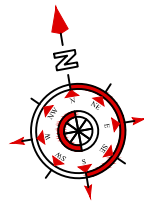
PROJECT TEAM

Land Surveyor: ROGERSON AND BIRCH
ph: 0362485898

REGISTERED SURVEYORS

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CLIENT:

BRIGHT CAMP
PTY LTD

SITE ADDRESS:

23 MENIN DRIVE,
BRIGHTON, TAS 7030

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ISSUE	DESCRIPTION	DATE

File Number:

Brightcamp/Menindrive/Arch/15/02/2022

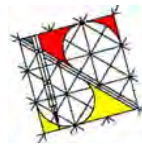
TITLE:

DEMOLITION PLAN

Sheet Original:

A - 3

SCALE 1:200



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Building Design And Drafting

TASMANIA . VICTORIA

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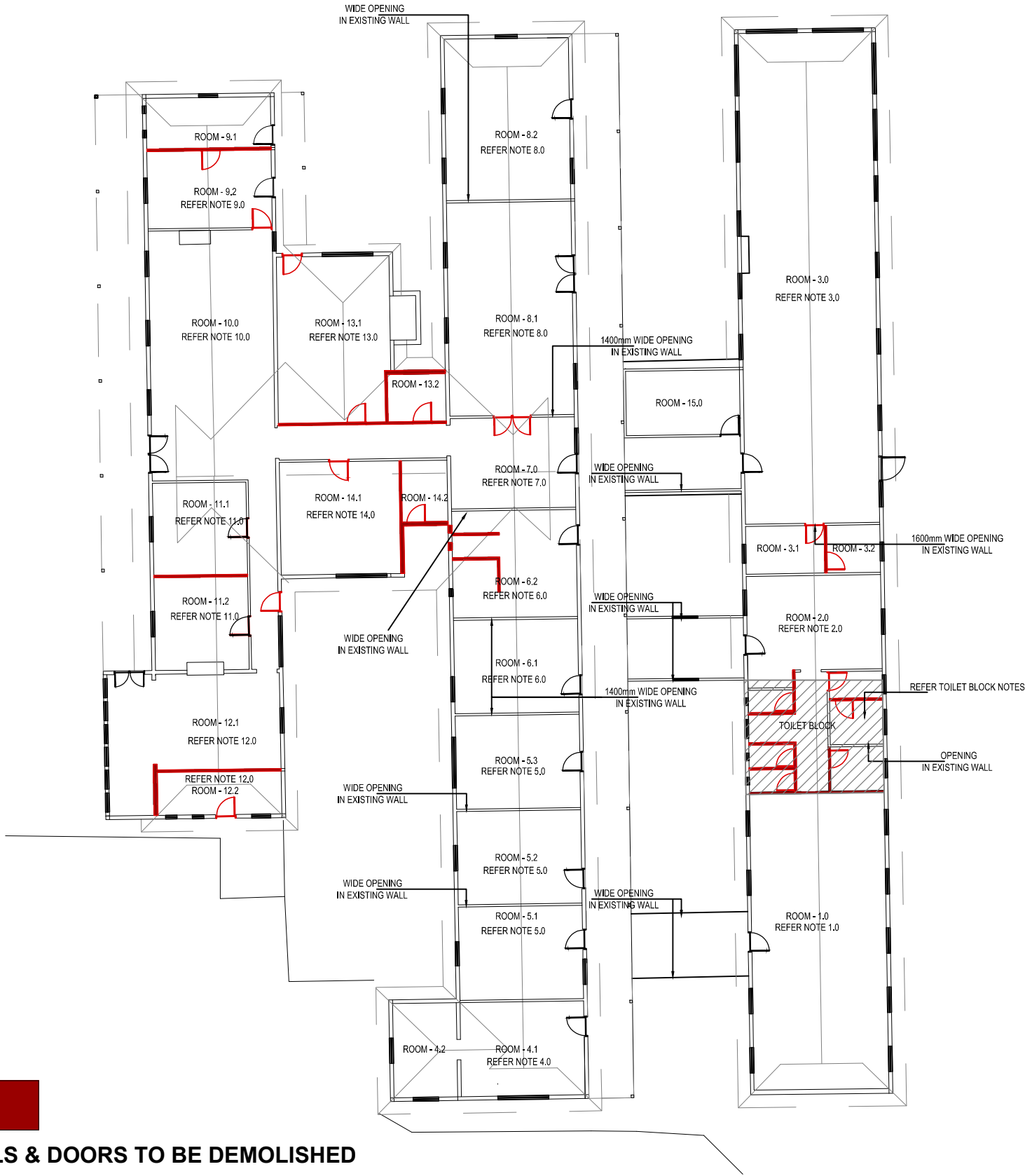
Drawn By:

G. KAUR

Drawing Number:

DA-14

PLOT DATE: 30-09-2022



WALLS & DOORS TO BE DEMOLISHED

DEMOLITION PLAN

NOTE: -

- ALL WALLS SHOWN IN RED COLOR IN THE EXISTING LAYOUT AND SUPERIMPOSED LAYOUT TO BE DEMOLISHED.
- FOR DOOR/ WINDOW SIZES, PLEASE REFER DOOR WINDOW SCHEDULE.
- DEMOLITION WORK TO BE CARRIED IN ACCORDANCE WITH THE TASMANIAN BUILDING ACT 2016

NOTE: -

THE EXTERNAL WALLS, ROOF PROFILE AND 3 FIRE PLACES ARE AS PER EXISTING TO RETAIN THE HERITAGE ASPECT OF THE BUILDING, WHEREAS THE INTERNAL PARTITION WALLS HAVE BEEN ADJUSTED TO SUIT THE SPACES REQUIRED FOR EACH AREA.

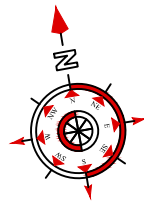
EXISTING INTERNAL WALL AND ROOF CLADDING SHALL BE REMOVED TO VERIFY THE STRUCTURAL INTEGRITY, REMOVING ANY ASBESTOS AND UPGRADING THE STUCTURE TO INCREASE THE SERVICE LIFE OF THE BUILDING.

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ISSUE	DESCRIPTION	DATE

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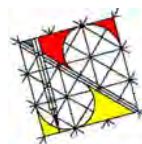
TITLE:

LAYOUT PLAN & DOOR WINDOW SCHEDULE

Sheet Original:

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SCALE 1:200



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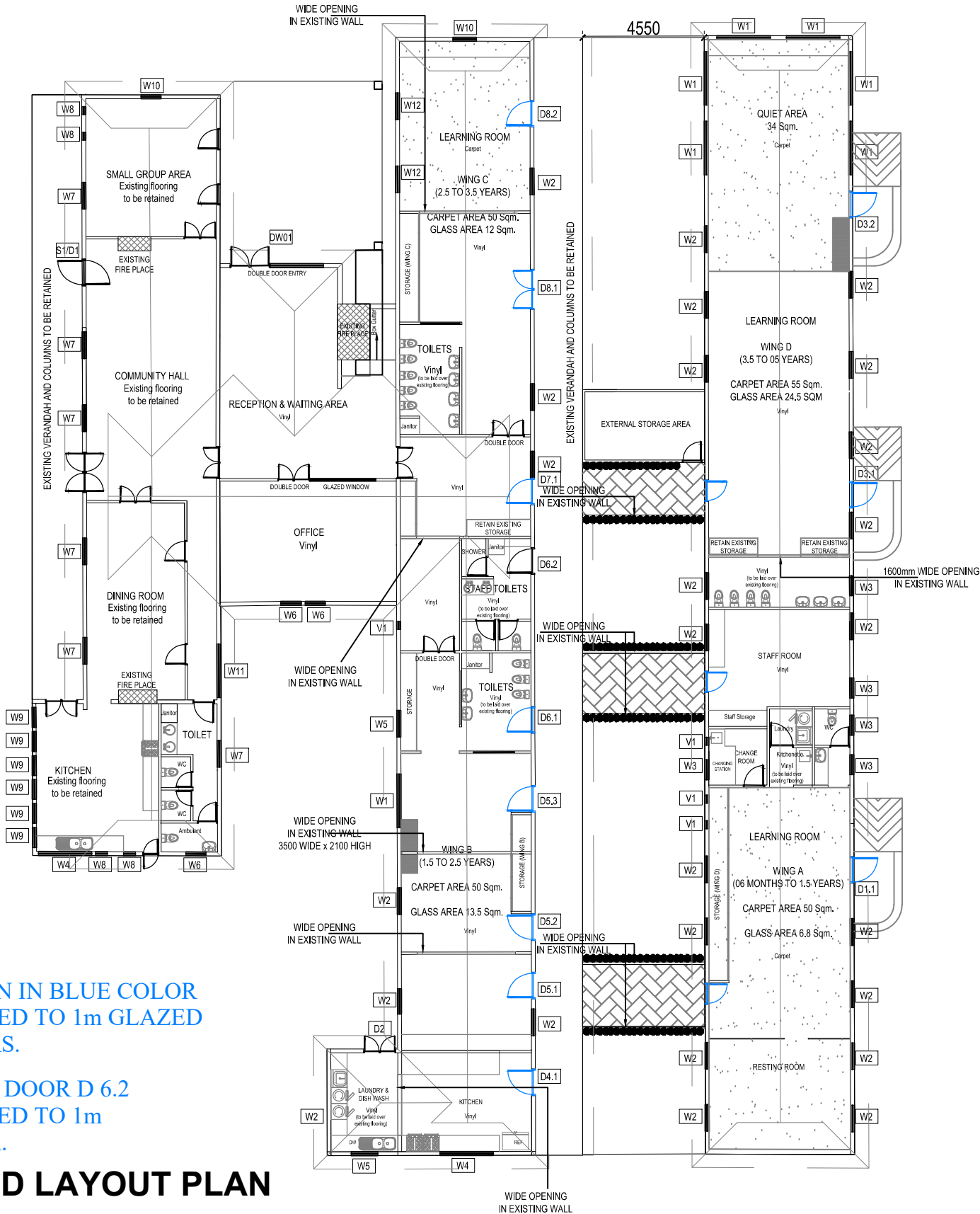
Drawn By:

G. KAUR

Drawing Number:

DA-15

PLOT DATE: 30-09-2022



DOORS SHOWN IN BLUE COLOR TO BE CHANGED TO 1m GLAZED TIMBER DOORS.

STAFF TOILET DOOR D 6.2 TO BE CHANGED TO 1m TIMBER DOOR.

PROPOSED LAYOUT PLAN

ALL THE WINDOW SIZES AND DESIGN SHALL BE RETAINED AS PER ORIGINAL. THE NEW WINDOWS AND FRAMES SHALL BE OF TIMBER WITH DOUBLE GLAZED LOW - E GLASS.

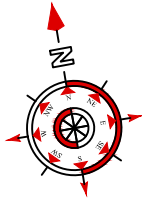
Door - Window Schedule												
ID	W1	W2	W3	W4	W5	W6	W7	W8	W9			
Type	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Awning+ Fixed	Fixed	Awning+ Fixed	Awning+ Fixed
Glazing	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e
*U-Value	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx
Frame Material	Timber	Timber	Timber	Timber	Timber	Timber	Timber	Timber	Timber	Timber	Timber	Timber
Width	2000	1000	750	2000	1000	750	1000	300	750			
Height	1500	1500	1200	1500	1500	1200	1500	1100	1000			
Sill Height (from FFL)	600	600	600	600	600	900	600	1000	1100			
Area	3.0 sqm	1.50 sqm	0.90 sqm	3.0 sqm	1.50 sqm	0.90 sqm	1.50 sqm	0.33 sqm	0.75 sqm			
Proposed Glass Area												
NOTES	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	New Windows Proposed		
Elevation												

ID	W10	W11	W12	V1	EXTERNAL GLAZED DOORS		D6.2	D8.1	DW01	S1
Type	Awning+ Fixed	Awning+ Fixed	Fixed	Fixed	Standard door		Standard door	Standard door	Standard door	Security door
Glazing	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e		Doubled glazed low-e	Doubled glazed low-e	Doubled glazed low-e	N/A
*U-Value	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	3.7 max SHGC 0.60 approx	D1.1 - D7.1, D8.2					
Frame Material	Timber	Timber	Timber	Timber	Timber		Timber	Timber	Timber	Aluminium
					Existing Proposed		Existing Proposed	Existing Proposed	Door Window	Proposed
Width	800	2000	1000	300	800 1000	800 1000	800 1000	800 1000	1500 2000	1000
Height	1100	1500	1425	580	2100 2100	2100 2100	2100 2100	2100 2100	2100 1030	2100
Sill Height (from FFL)	1000	600	675	1520	0 0	0 0	0 0	0 0	0 1070	0
Area	0.88 sqm	3.00 sqm	1.42 sqm	0.174 sqm	1.68 sqm 2.10 sqm	1.68 sqm 2.10 sqm	1.68 sqm 2.10 sqm	1.68 sqm 2.10 sqm	3.15 sqm 2.06 sqm	2.10 sqm
NOTES:	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	No Change in size. As/ Existing	D1.1 - D6.1 D7.1, D8.2 Proposed Doors to be Located in existing window openings				Proposed Doors for Child Care Entry	
Elevation										

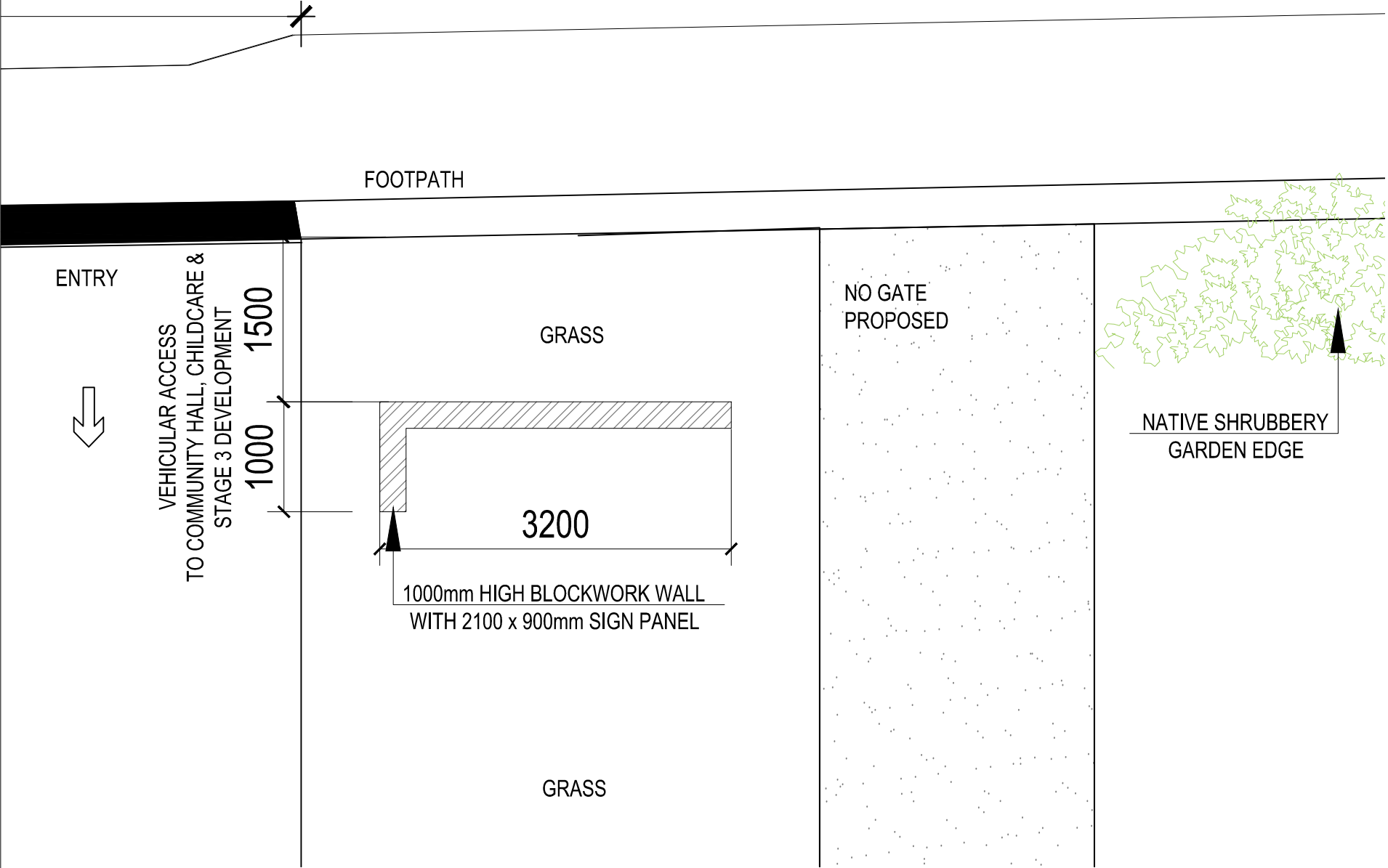
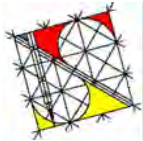
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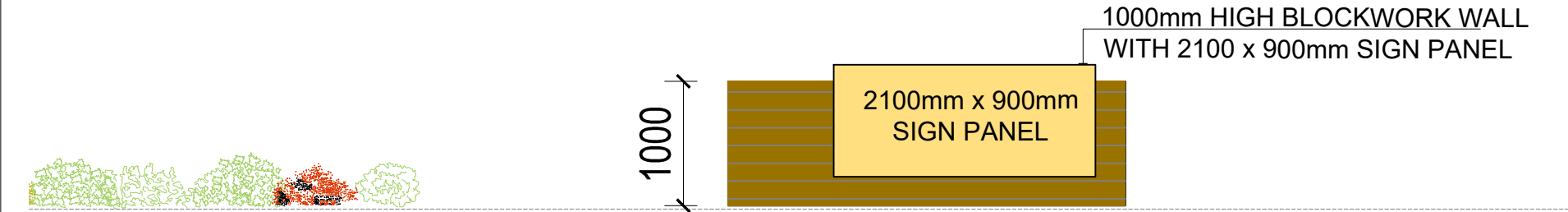


ISSUE	DESCRIPTION	DATE



SIGNAGE WALL- PLAN

SCALE:- 1:50



SIGNAGE WALL- ELEVATION

SCALE:- 1:50

NOTE: -

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Staff arriving time: 8 educators + cook + director = 10 staff

2 Educators arrive at 7 AM to open the centre,
Director and 2 educator arrive at 8 AM,
2 Educators arrive between 8.30 - 9.30 AM,
The Cook and the remaining 2 Educators arrive after 9.30 AM

Staff departure time: 8 educators + cook + director = 10 staff

The cook leaves at 2 PM,
Director and 2 educators leave at 3.30 PM,
2 educators leave at 4.30 PM,
2 educators leave at 5 PM,
The remaining 2 Educators leave at 6 PM.

Children arriving between 7am and 8am
Maximum 8 children



Outdoor play areas positioned such that the building structures act as a noise barrier

Play areas located away from neighbouring homes

The separation between the active outdoor play area and the façade of any neighbouring residential premises maximised.

Majority of the operable windows of the child care centre and external play areas do not have a direct line of sight to neighbouring houses.

Access ramps located away from neighbouring homes.

Low noise features such as self closing gates with soft closure hinges, selection of low noise air-conditioning condensers and car park surfaces and access ways are smooth.

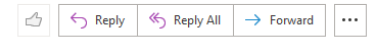
Thickest possible laminated glazing will be used in windows and doors.

Acoustically absorbent panels will be used where possible.

Re: 23 Menin Drive, Brighton



Ishvinder Singh Samra
To: Jo Blackwell



Tue 14/02/2023 10:15 PM

Hi Jo,

Thanks for the email.

We will be providing a capacity to care for 8 children between 7am and 8am in the morning. As per the view of Child Care consultants who have taken into consideration the current population of Brighton, we are looking at a maximum of 4 children to arrive between 7am and 8am at present. In future if the population of Brighton doubles then there are chances of maximum 8 children arriving between 7am and 8am.

Thanks

Kind Reagrds

Ishvinder Singh Samra



BRIGHTON ARMY CAMP HOSPITAL

HERITAGE IMPACT STATEMENT

BRIGHTON ARMY CAMP
HOSPITAL
HERITAGE IMPACT
STATEMENT

PAUL JOHNSTON
ARCHITECTS + HERITAGE CONSULTANTS

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ISSUE : Jan 31 2023

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1.0 INTRODUCTION

Bright Camp Pty Ltd have proposed the redevelopment of the former hospital building of the former Brighton Army Camp. The proposal adapts the existing building and its immediate surrounds for its new use as an Early Learning Centre (stage 1) and Community Hall (stage 2). Part of the adjacent land area has been set aside as stage 3 and does not form part of the proposal.

Paul Johnston architects and heritage consultants have been engaged to provide a Heritage Impact Statement (HIS) regarding the proposal that will be submitted to the Brighton Council for a Development Application. The Brighton Council are the current owners of the site and commissioned Paul Johnston architects and heritage consultants to provide a Conservation Management Plan for the place to assist in its conservation when a change of use proposal was initiated. This HIS is part of this process, and it should be acknowledged that Paul Johnston architects and heritage consultants have had input into refining the proposal as it has been developed.

This HIS has proceeded in accordance with the articles and definitions set out in The Burra Charter; the Australia ICOMOS Charter for Places of Cultural Significance 2013, and guided by Heritage Impact Guide of the State Heritage Office of the government of Western Australia, Nov 2012.

This assessment has been based on the drawings provided by Bright Camp Pty Ltd.
Proposed Early Learning Centre and Community Hall Development at Brighton,
Urban Space Designers,
DA -01 to DA-16,
RO 5 31.01.2023

2.0 OUTLINE DESCRIPTION OF THE PROPOSAL

The former hospital building is currently not in use. It has previously been used for administrative purposes in the 1990's when the place was used by the Army 6th Group for training.

The proposal intends to adapt the existing structure for use as an Early Learning centre and Community Hall. The proposal changes the use of the building with works to both the exterior and interior.

The proposal includes landscaping and car parking area, a clear roofed internal play area and an external fenced play area.

A small extension is provided to the former kitchen to provide a new entry and minor alterations to the rear provide expanded office area associated with the entry.

The proposal will create new circulation internally with the partial removal of walls for larger room sizes and new openings for internal circulation. However, circulation between wings is retained at the annex connections.

New ramped entries to are provided from internal child spaces to play areas externally with new glazed doors to existing openings.

New toilet services are provided with new partitions walls.

Doors to the verandah are replaced with glazed doors.

New electrical and hydraulic services are provided and insulation to walls and ceilings.



fig 1. View of the former Hospital entry wing

The redevelopment of this wing for a Community Hall is supported by the CMP. The main facade will retain all its significant fabric.

3.0 STATUTORY CONTROLS

3.1 Previous Significant Assessments

The Brighton Army Camp site has been the subject of previous applications to enter it on the Tasmanian Heritage Register. While these have not progressed, the THC have produced a draft data sheet outlining the assessment of significance against the criteria of the Heritage Act, however, it is not currently registered on either the Tasmanian Heritage Register or the Brighton Planning Scheme as a Heritage Place. It was entered as an Indicative Place on the Register of the National Estate in 2001, however, this list no longer has any statutory authority. While no statutory heritage requirements exist for the place, despite the recognition of heritage values associated with the place, the current owners, the Brighton Council, require that those heritage values be maintained in any future development on the site. The Brighton Council engaged Paul Johnston architects and heritage consultants to provide a Conservation Management Plan (CMP) that would guide the future development of the place and retain its cultural heritage significance. The CMP provided a significance assessment and identified significant fabric that should be conserved. Policies were provided to guide the management of change to the place. The following Statement of Significance from the CMP provides an understanding of the historic cultural significance of the place.

The Brighton Army Camp is of cultural heritage significance for its historic, representative and social heritage values.

The origins of the place as a military training facility for Tasmanians serving in armed conflicts in World War Two and other conflicts across the globe for sixty years is significant to the Tasmanian community and the Military community. This association is memorialised in the creation of Remembrance Park and commemorated each Anzac Day.

The remaining former Hospital, open grounds, roads and entry gates, are a rare example of a purpose built military training camp established at the beginning of World War Two and is evidence of the original greater camp site.

The Brighton Army Camp has historic significance as the location of the first aerodrome in southern Tasmania visited by aviator Charles Kingsford Smith who established an airbase for the Australian National Airways. It was the site where the first Australia to Britain airmail service departed.

The Brighton Army Camp has social significance for the Tasmanian migrant

community for its role as a housing facility for immigrants to Tasmania in the post-war period and Kosovo refugees in 1999. It also has historic significance as a place for temporary housing between 1947-1951, and its use as an emergency shelter in the 1967 bushfires.

The 2015 Remembrance park memorial as a place for the commemoration of Anzac Day is evidence of the on-going association between the Brighton community and the Brighton Army Camp.

The Conservation Management Plan for the site provided the following objective for a Conservation Strategy;

Heritage management at the Brighton Army Camp will:

Retain the site as a remnant of the former Camp site and allow for interpretation of the area's history and connection to army training.

Retain the Hospital building and carry out restoration works

Continue maintenance of the grounds, entry gates, remembrance park and garden

Maintain the relationship between the open grounds, entry and Hospital building

Find a new use for the hospital building that will continue the connection to the local Brighton Community

Maintain the open grounds as space for sports and recreation Heritage management of the Brighton Army Camp will ensure that the place maintains its significance through increased use of the hospital building, as well as encouraging use of the grounds and remembrance park. It will encourage local use of the site and retain its connection to the Brighton area.

This Heritage Impact Assessment will consider the heritage impact of the proposal against the Policies in the Conservation Management Plan.

4.0 ASSESSMENT of HERITAGE IMPACT

4.1 Demolition and the Disturbance of Significant Fabric

The proposal aims to provide a new use to the existing buildings. This will necessitate an upgrading of services and the modification of fabric to achieve compliance to standards as well as functionality.

4.1.1 Changes to the Exterior

New openings are created in the external walling.

A new opening is provided for the main entry to the Early Learning Centre on the northern elevation of the former kitchen. The new glazed timber double doors are located to retain the existing windows in this wall.

New openings are provided in the eastern elevation that provide access from internal rooms to external play areas. These doors are located at existing window openings and door openings.

New doors are provided to the internal walls of the two wings of the early learning centre.

These doors are to be timber framed glazed doors that facilitate safe circulation with visibility and increased width. Each opening will require removal of framing, cladding and lining with a new frame.

All existing windows not removed for new openings, are to be retained and refurbished with new glazing.

There is one new opening provided in the Community Hall where an existing window is proposed for removal.

The service entry on the southern elevation behind the former kitchen will be modified with the removal of a door and cladding.

roofing is to be removed and replaced to facilitate the installation of thermal insulation.



fig 2. View of the former kitchen



fig. 3. View of the former service yard

The proposal provides a new entry for the Early Learning Centre whilst retaining the existing elements of the kitchen. The service yard will be retained with some minor infill alteration to the entry. The concrete pavement should be extant recorded before any removal of covering up.

Comment

The changes to the external envelope of the building are minor. New doors are located at existing openings and related doors will facilitate safe movement.

The new entry to the Early Learning Centre attempts to retain fabric and the removal of the verandah structure may be required.

The removal of the window and replacement with a door to the Community Hall is the only change to this elevation which is the original entry and main facade.

The removal of the rear door and cladding wall is discreet from public view.

Recommendations

The replacement of door openings with wider doors should be investigated to ensure existing framing and architraves are retained as much as possible. CMP policy 17.

The design of the new entry may need to be reviewed when detailed designs are provided that consider the entry levels and forecourt materials. Should the works require adjustment of the opening or the removal of the verandah structure then this can be considered with expert advice. CMP policy 14.

The new door opening to the west elevation of the Community Hall should not be approved unless it can be demonstrated that it is required by applicable standards or construction codes. CMP policy 23.

New works should not mimic the style of the existing structure but simplified detailing can provide a subtle contrast to distinguish original with new fabric. CMP policy 20.

New paint finish colour should be subdued in tone and compatible with existing surfaces including the existing roofing which should be retained with a new paint finish of a subdued paint colour. CMP policy 20.

Replacement of the eaves gutters should be considered with a simple D mould profile in zincalume finish.

4.1.2 Changes to the Interior

The removal of internal walls are proposed to facilitate new uses. These include opening up areas for more usable open areas and upgrading of toilet facilities.

The removal of walls are designed to retain a bulkhead at 2100 afl and

side wall nibs. Doors located within these walls are also removed. Fireplaces and joinery cupboards are retained. Wall linings are to be removed and replaced following the installation of thermal insulation.

comment

The retention of joinery and fireplace elements and wall and original ceiling linings is in accordance with CMP policy 17. The existing circulation follows the barrack layout where circulation is external to the building and access is gained via the verandah. While this doesn't present as an issue for the larger rooms, the smaller rooms become impracticable. While the openings are wider than suggested in the CMP this is acceptable given the nature of the proposed use. The removal of internal walls that retain bulkheads and nibs meets the intent of CMP policy 17.



fig 4. Fireplaces are retained in the proposal



fig 5. Significant panelled wall, dado and ceiling linings, flooring and joinery are retained



fig 6. Window and door joinery will be retained although some doors widened and glazed for safety and access reasons. Careful detailing of new openings is required for minimize disturbance of joinery fabric.

Recommendations

The removal and reinstatement of the wall linings may not be achievable without disturbance to the original fabric. Expert advice should be sought to ensure reinstatement of fabric is achieved. Making good to walls and ceilings and floors following the removal of fabric should be simply detailed to reveal the location of original walls.

4.2 New Work

No new buildings are proposed for the site.

Car parking is provided to the west of the buildings with a vehicle entry and exit road from Menin Drive. Staff parking is located to the southern area of the existing buildings.

New pedestrian pavements are proposed for the northern and western entry area with an extended path to Menin Drive and 1800 high timber picket fences and gates provide a secure entry to the Early Learning Centre.

Landscaping is proposed to the entry fronting Menin Drive with a 'green area' fringed with small scale plants. A low 1.000 metre high signage wall is located at the pedestrian and vehicle entry from Menin Road.

Additional roof structures are provided to the entry of the Early Learning Centre and in between wings as a covered play area.

New timber ramps are provided to the new doors that provide access to the external play area which has an 1800 high timber picket fence enclosing the yard.

Comment

The location of car parking areas are in accordance with the CMP policy 21. The provision of open space landscaping to the frontage of Menin Drive is in accordance with CMP policy 19.

Landscaping is minimal with low level shrubs and 'green area' is assumed to be grassland. New structures are designed at a low level to allow the existing buildings to maintain prominence. The signage wall at the Menin Road entry is detached from the building and is designed to be part of the landscape forecourt. In this respect the sign meets the performance criteria of clause C1.6.4 P1 of the Tasmanian Planning Scheme - Brighton.

- a) the heritage building retains its prominence.
- b) the size and design is appropriate to its location and use.
- c) the sign is not on the building.
- d) there are no historic signs.

The proposed sign also meets the requirements of Policy 25 of the CMP.

Recommendations

The carparking in front of the Community Hall should be adjusted to allow a pedestrian pavement entry in line with the main entry. The 1800 high picket fences should be lowered to a height that meets regulation in order to reduce the visual intrusion. Roof cover structures to the entry and play area should be detailed to ensure they are connected to the existing buildings. Pavement areas to the perimeter of the buildings should ensure sub floor ventilation is not compromised. Landscaping to the forecourt should be designed to allow the area for community passive recreation with consideration of a design that enhances the presentation of the existing buildings.



fig 7. External window and door details are retained in the proposal.



fig 8. The space between wings will be used for play ground with a clear roof over that is detached from the existing walls.



fig 9 Replacement of gutters and downpipes with more traditional profiles and materials is recommended. The external envelope is largely retained and refurbished. Subfloor ventilation should be provided.

5.0 HERITAGE IMPACT STATEMENT

The proposed use of the former Brighton Army Camp hospital for an Early Learning Centre and Community Hall provides for community access and use and ensures the ongoing maintenance of the building and its cultural heritage significance.

The layout of the proposed use for a Community Hall maintains the original entry and supports the presentation of the heritage values of the place to the community. The use of the hospital wings for the early learning centre maintains the form of the building and minimizes the need for new structures which allows the existing building to retain its prominence.

The new use requires internal circulation and larger room sizes than the hospital and new openings are proposed that allow for the part retention of walls that provide an interpretation of the original room layout. The new openings to the exterior are proposed to allow for safe and equal access as well as access to external play areas.

The original panelled walls and ceilings and dado lining, joinery cupboards and fireplaces are all retained and incorporated into the new works with reinstatement following service upgrade.

The proposal maintains the historic cultural heritage of the former Army hospital building as it allows for on-going community visitation to the place, maintains the visual prominence of the place and minimizes disturbance of fabric.

6.0 RECOMMENDATIONS

The place should have an extant photographic recording prior to any works and measured drawings of any ground elements that will be covered up by new work.

The replacement of gutters and downpipes should be in profiles and materials that are in keeping with the era of the buildings construction. Paint colours should be subdued and paint samples undertaken to determine an original paint colour scheme.

Expert advice should be sought to ensure that fabric removed for service installation can be successfully reinstated.

The height of the picket fencing should be lowered if regulations allow. Landscaping of the forecourt should encourage and facilitate community use.

The design of the entry should be reviewed when the retention of the existing verandah is considered. Expert advice should be sought to consider detailing of this area and new roofing structures.

A2216488T 23 Menin Drive, Brighton - Car Parking and Traffic Impact Assessment 2.1

12th March 2023

Brighton Council
1 Tivoli Road,
Old Beach TAS 7017

Dear Sir / Madam,

Car Parking and Traffic Impact Assessment – Proposed Community Centre and Child Care Centre at 23 Menin Drive, Brighton

1. Overview

We have been commissioned by Ishvinder Singh Samra to undertake a car parking and traffic impact assessment for a proposed community centre and child-care centre located at 23 Menin Drive, Brighton. Version 2.1 replaces A2216488T 23 Menin Drive, Brighton - Car Parking and Traffic Impact Assessment 1.4.pdf dated 15th December 2022, which Council has sighted and issued a request for further information.

The proposed child care centre will accommodate 10 full-time staff and 70 children. Proposed opening hours are 7am to 6pm. The proposed community centre occupies 150m². 31 standard and 4 disabled parking spaces are proposed.

2. C2.5 Use Standards of Parking and Sustainable Transport Code

2.1 C2.5.1 Car Parking Numbers

The objective is that an appropriate level of car parking spaces is provided to meet the needs of the use. 35 car parking spaces are provided, against a requirement of 32 car spaces as per Table C2.1.

2.2 C2.5.2 Bicycle Parking Numbers

The objective is that an appropriate level of bicycle parking spaces is provided to meet the needs of the use. 17 bicycle parking spaces are provided, against a requirement of 5 bicycle spaces as per Table C2.1.

Level 3, 85 Macquarie Street, Hobart TAS 7000

Telephone: 03 6237 0012
hobart@mltraffic.com.au

Facsimile: 1300 739 523
www.mltraffic.com.au

2.3 C2.5.3 Motorcycle Parking Numbers

The objective is that an appropriate level of motorcycle parking spaces is provided to meet the needs of the use. 4 motorcycle parking spaces are provided, against a requirement of 1 motorcycle spaces as per Table C2.4.

2.4 C2.5.4 Loading Bays

The objective is that adequate access for goods delivery and collection is provided, and to avoid unreasonable loss of amenity and adverse impacts on traffic flows. A loading bay must be provided for uses with a floor area of more than 1000m² in a single occupancy. The development is under 1,000m². A dedicated loading bay is not required.

2.5 C2.5.5 Car Parking Bays within Residential Zones

Criteria is not applicable - Number of car parking spaces within the General Residential Zone and Inner Residential Zone.

2.6 Development Standards for Buildings

2.6.1 C2.6.1 Construction of parking areas

Criteria	Design Response
<p>A1</p> <p>All parking, access ways, manoeuvring and circulation spaces must:</p> <p>(a) be constructed with a durable all weather pavement.</p> <p>(b) be drained to the public stormwater system, or contain stormwater on the site; or</p> <p>(c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conversation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise water to the pavement.</p>	<p>Construction matters is outside the scope of a traffic engineer's ability to comment on.</p> <p>It is expected that Council's construction specifications will be adhered to by the applicant's construction contractors.</p>

2.6.2 C2.6.2 Design and layout of parking areas

The objective is that parking areas are designed and laid out to provide convenient, safe and efficient parking. This is achieved.

Criteria	Design Response
<p><u>A1.1</u> Parking, access ways, manoeuvring and circulation spaces must either:</p> <ul style="list-style-type: none"> (a) comply with the following: <ul style="list-style-type: none"> (i) have a gradient in accordance with Australian Standard AS 2890 – Parking Facilities, Parts 1 to 6; (ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 spaces. (iii) have an access width not less than the requirements in Table C2.2; (iv) have car parking space dimensions which satisfy the requirements in Table C2.3; (v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3, where there are 3 or more car parking spaces. (vi) have a vertical clearance of not less than 2.1m above the parking surface level; and (vii) excluding a single dwelling, be delineated by line marking or other clear physical means. 	<p>Car space dimensions and combined access and manoeuvring width are satisfactory from a performance-based criteria and compliance with AS2890.1:2004 (which differs from Table C2.3).</p> <p>Vertical clearance is not applicable.</p>
<p><u>A1.2</u> Parking spaces provided for use by persons with a disability must satisfy the following:</p> <ul style="list-style-type: none"> (a) be located as close as practicable to the main entry point of the building. (b) be incorporated into the overall car park design. (a) be designed and constructed in accordance with AS/NZS 2890.6:2009 Parking facilities, Off- Street parking for people with disabilities. 	<p>Complies</p>

2.6.3 C2.6.3 Number of accesses for vehicles

The objectives are access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses. Accesses to not cause an unreasonable loss of amenity of adjoining use. The number of accesses to minimise impacts on the streetscape.

These objectives are all achieved.

Criteria	Design Response
<u>A1</u> The number of accesses provided for each frontage must: (a) be no more than 1, or (b) no more than the existing number of accesses, whichever is greater.	Only 1 access is provided. Complies.
<u>A2</u> Within the Central Business Zone or in a pedestrian priority street, no new access is provided unless an existing access is removed	Not applicable.

2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone

The objective is that parking and vehicle circulation roads and pedestrian paths within the General Business Zone and Central Business Zone, which are used outside daylight hours, are provided with lighting to a standard which enables easy and efficient use, promotes the safety of users, minimises opportunities for crime or anti-social behaviour, and prevents unreasonable light overspill impacts.

Lighting matters are outside of our field of expertise. No comments are offered.

2.6.5 Pedestrian access

The objective is that pedestrian access within parking areas is provided in a safe and convenient manner. This is achieved.

Criteria	Design Response
<p><u>A1.1</u> Uses that require 10 or more car parking spaces must:</p> <p>(a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:</p> <p>(i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or</p> <p>(ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and</p> <p>(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.</p>	<p>High level of compliance is achieved.</p> <p>A 2.5m wide footpath runs between the frontage road and the entrance of the building. The footpath continues along the western periphery of the building, between the car park and the building, abutting 10 car parking spaces.</p> <p>A separate footpath with a nominal 1.5m width (varies between 1m and 1.8m) abuts 14 car parking spaces and the western boundary.</p> <p>A maintained grassed area around 1.8m wide abuts 6 parking spaces located at the north-western portion of the car park. It is anticipated that these spaces will be used less frequency as they are located further away from the building.</p> <p>A 1.2m foot path abuts 5 staff parking spaces and the southern boundary.</p>
<p><u>A1.2</u> In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.</p>	<p>A 2.5m wide footpath abuts the accessible car parking spaces and the building.</p> <p>The gradient of the footpath leading into the building does not exceed 1 in 14.</p>

2.6.6 Loading bays

The area and dimensions of loading bays are adequate to provide safe and efficient delivery and collection of goods.

Given that the combined leasable area for the community centre and child care centre is under 1,000m², an on-site loading bay is not required.

Criteria	Design Response
<u>A1</u> The area and dimensions of loading bays and access way areas must be designed in accordance with Australian Standard AS 2890.2–2002, Parking facilities, Part 2: Off-street commercial vehicle facilities, for the type of vehicles likely to use the site.	Not applicable.
<u>A2</u> The type of commercial vehicles likely to use the site must be able to enter, park and exit the site in a forward direction in accordance with Australian Standard AS 2890.2 – 2002, Parking Facilities, Part 2: Parking facilities - Off-street commercial vehicle facilities.	Not applicable.

2.6.7 Bicycle parking and storage facilities

The objective is that parking for bicycles are safe, secure and convenient, within the General Business Zone and Central Business Zone. This is achieved.

Criteria	Design Response
<u>A1</u> Bicycle parking for uses that require 5 or more bicycle spaces in Table C2.1 must: <ul style="list-style-type: none"> (a) be accessible from a road, cycle path, bicycle lane, shared path or access way; (b) be located within 50m from an entrance; (c) be visible from the main entrance or otherwise signed; and (d) be available and adequately lit during the times they will be used, in accordance with Table 2.3 of Australian/New Zealand Standard AS/NZS 1158.3.1: 2005 Lighting for roads and public spaces - Pedestrian area (Category P) lighting - Performance and design requirements. 	Complies. Matters relating to lighting is outside of our field of expertise.

<p><u>A2</u> Bicycle parking spaces must:</p> <ul style="list-style-type: none"> (a) have dimensions not less than: <ul style="list-style-type: none"> (i) 1.7m in length; (ii) 1.2m in height; and (iii) 0.7m in width at the handlebars; (b) have unobstructed access with a width of not less than 2m and a gradient not steeper than 5% from a road, cycle path, bicycle lane, shared path or access way; and (c) include a rail or hoop to lock a bicycle that satisfies Australian Standard AS 2890.3-2015 Parking facilities -- Part 3: Bicycle parking. 	Assumed to be compliant. Architect to ensure compliance.
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2.6.8 Siting of parking and turning areas

The objective is that the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties. This is achieved.

Criteria	Design Response
<p><u>A1</u> Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas must be located behind the building line of buildings, excluding if a parking area is already provided in front of the building line.</p>	<p>Parking is located to the side of the building line and the rear of the building line. This is due to the shape of the land, and the location of the existing building.</p>

<p><u>A2</u> Within the Central Business Zone, on-site parking at ground level adjacent to a frontage must:</p> <ul style="list-style-type: none"> (a) have no new vehicle accesses, unless an existing access is removed; (b) retain an active street frontage; and (c) not result in parked cars being visible from public places in the adjacent roads. 	<p>Not applicable.</p>
---	------------------------

2.7 Requirements

2.7.1 Table C2.1 Parking Space Requirements

Car Parking

Brighton's Planning Scheme Table C2.1 specifies the following car parking rates:

- An educational and occasional care at 1 car parking space per employee and 1 car parking space per 6 tertiary education students.
- Community Meeting and Entertainment, which includes place of worship, function centre and public hall at 1 car parking space per 15m² or 1 car parking space per 3 seats whichever is greater.

The proposed 10 staff and 70 children child care centre's car parking requirement is 20 spaces, comprising 10 x 1 space per staff and 12 spaces for parents to drop-off or pick-up their children. A comparison is made against a different municipality's car parking rate – specifically Hobart City Council's rate of 0.25 space per child. If the subject child-care centre were located in Hobart, it would require 18 spaces (the total being applied to both staff and drop-off parking). The proposed 150m² community centre requires 10 car parking spaces.

The combined uses require 32 car parking spaces. The proposed centre provides 31 standard spaces and 4 disabled spaces – which is in excess of requirement. Note that the peak parking demand of the child care centre occurs during drop-off and pick-up periods. The community centre is likely to be busy in the evenings and on weekends.

Bicycle Parking

Brighton's Planning Scheme Table C2.1 specifies the following bicycle parking rates:

- An educational and occasional care at 1 bicycle parking space per 5 employees and tertiary education students.
- Community Meeting and Entertainment, which includes place of worship, function centre and public hall at 1 bicycle parking space per 50m² or 1 bicycle parking space per 40 seats whichever is greater.

The proposed 10 staff and 70 children child care centre's bicycle parking requirement is 2 spaces, comprising 2 x 1 space per staff and 0 space for children (as they are not tertiary education aged). The proposed 150m² community centre requires 3 bicycle parking spaces.

The combined uses require 5 bicycle parking spaces. The proposed centre provides 18 bicycle parking spaces – in excess of requirements

2.7.2 Table C2.2 Internal Access Way Widths Requirements

A car park with 21 or over 21 car parking spaces requires a driveway width not less than 5.5m. The proposed development fully complies. Driveway is 10m wide for the majority of its length.

2.7.3 Table C2.3 Dimensions of Car Parking Spaces Requirements

90-degree car parking space that is 2.6m wide and 5.4m long requires a combined 6.4m access and manoeuvring width. Table C2.3 does not specify 2.4m wide car parking spaces. 26 attendee / visitor spaces measuring 2.4m wide and 5.4m long are accessed via a 10m wide driveway. The access and manoeuvring width of 10m is considerably greater than the 6.4m requirement. As such, attendee / visitor car space dimensions are satisfactory from a performance-based perspective.

5 staff spaces measuring 2.5m wide and 5.4m long are accessed via a 6.0m driveway. Table C2.3 does not specify 2.5m wide car parking spaces. AS2890.1:2004 permits the use of 2.4m (slightly narrower than 2.5m) wide spaces for employee parking. As such, staff car space dimensions are satisfactory from a performance-based perspective.

4 disabled spaces with 4 accessible (hatched / no-parking) spaces, each measuring 2.6m wide and 5.4m long complies with (exceeds) AS2890:6:2009's requirement of 2.4m wide and 5.4m long.

2.7.4 Table C2.4 Motorcycle Parking Space Requirements

1 motor cycle parking space is required where the car parking requirement is between 21 and 40 car parking spaces (the proposal requires 32 car parking spaces). Proposed 4 motor cycle spaces provision exceeds this requirement.

3. Traffic Impact Assessment against C3.5.1 Traffic Generation at a Vehicle Crossing, Level Crossing or New Junction

Traffic generation rates for the proposed uses are referenced to the NSW Guide to Traffic Generating Developments V2.2. For a child-care centre, the traffic generation rates are 0.8 trip (2-way) per hour per child in the morning drop-off period and 0.7 trip (2-way) per hour per child in the afternoon pick-up period. The majority of these rates relate to parents' traffic movements, with some allowance for staff arrival in the morning and staff departure in the evening.

The proposed 78-children child care centre will generate 62 trips (33 in, 29 out) in the morning peak hour (8am to 9am) and 55 trips (26 in, 29 out) in the afternoon peak hour (4pm to 5pm).

The level of additional traffic generation will not adversely impact on the operation of Menin Drive / Brighton Road intersection – given the roundabout control. It is expected that around 80 percent of the proposed child-care centre's traffic will pass through this intersection. The other 20 percent will filter in via a series of local streets to the west of the site.

The proposed community centre with a car parking requirement of 10 spaces will generate around 5 to 10 cars per hour during evenings and on weekends. Such low levels of traffic generation will not adversely impact on the operation of surrounding streets and intersections.

4. Vehicle Accessibility at Site's Carpark Access Point

Council's information request letter states "Provide turning path diagrams for the access to the public road in accordance with AS2890.2 clearly demonstrating that the largest regular use vehicles can enter and exit the site without crossing the centreline of the public road to the extent that there is any interaction with the opposing direction of travel.

Advice: It may be acceptable for occasional use traffic to occupy the full width of the access.” My view is Council’s performance target is ambitious, and cannot be achieved for the following reasons:

- The ability to contain the left-turn exit manoeuvre out of a site within the centreline for a service vehicle (8.8m MRV for example) generally requires 10m to 12m of frontage road carriageway width. Such width is found in business parks, and not in residential areas. A 12.5m HRV would typically cross the centre line of a 10m to 12m wide frontage road.
- Carriageway width of Menin Road where the site’s access point is, is 10m. However, this includes provision for indented kerbside parking, meaning the effective trafficable carriageway width is 6m.
- Carriageway width of Menin Road to the left / west of the site’s access point is 6m, due to the presence of a kerb extension. Given so:
 - A very infrequent (once a week) 10m long refuse truck (ones used by Council; private waste contractor has fleets as small as 6.4m SRVs) will take up the entire 6m of carriageway width to turn left from the site, with the 10m long vehicle occupying the full 10m width of the crossover / driveway of the subject site.
 - An infrequent (several times a week) 7m long Toyota Coaster mini bus will be able to contain the left turn within the centreline of the 6m carriageway width of Menin Road (to the left of the site’s access point). However, the 7m long Toyota Coaster mini bus will need to occupy the full 10m width of the crossover / driveway of the subject site. The mini bus will occasionally be used for the community centre only, and not the child care centre.
- The largest regular use vehicle is a B99 car, with 85 percent of all cars accessing and egressing the site being the B85 car. A B99 car and a B85 car travelling in the opposite direction (one is turning left in, the other is turning left out) are able to do so using entire width of the crossover, and without crossing the centreline of the frontage road.

5. Conclusions

Based on the considerations presented in this report, the proposal does not impose material impacts on the surrounding streets and intersections. On-site parking provision satisfies statutory requirement. Occasional larger vehicle accessibility (larger than a B99 car) involving using the full width of the crossover and/or crossing the centreline of the frontage road is acceptable given the constraints associated with the width of the frontage road (varies from 6m to 10m), width of the crossover (10m is not narrow) and the very low frequency of larger vehicles accessing and egressing the site. The largest regular use vehicles (B85 car, with occasional B99 car) are able to turn left into and left out of the site without requiring the full width of the crossover and without crossing the centreline of the frontage road.

If you have any questions, please do not hesitate to contact me on 0413 295 325 and/or mlee@mltraffic.com.au.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'M Lee'.

Michael Lee, BEng (Monash, 1989)
Principal

10m Refuse Collection Vehicle

MENIN DRIVE

total: 10.0m

CP
Nail = 59.82
Gnd = 59.73

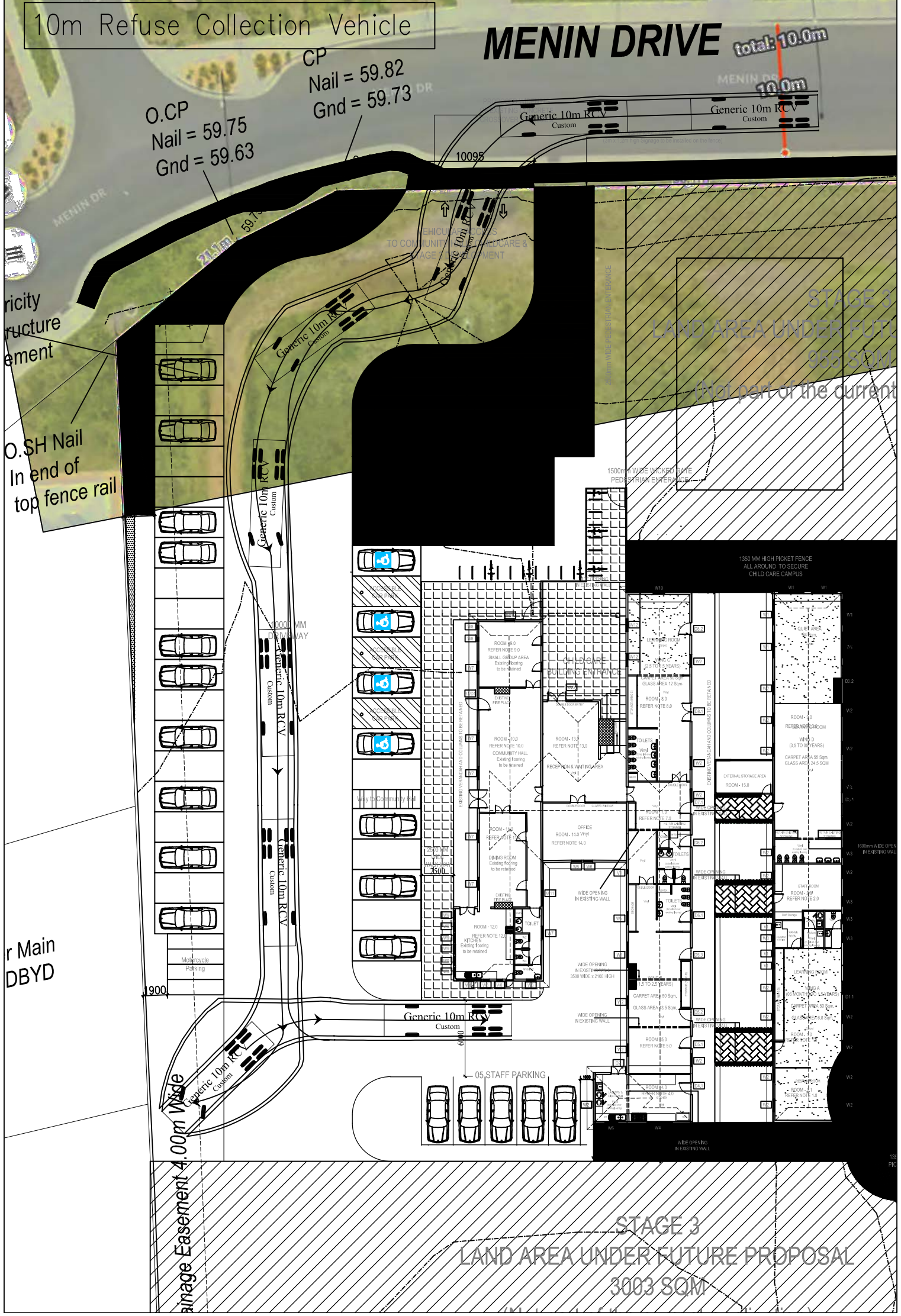
O.CP
Nail = 59.75
Gnd = 59.63

Electricity
structure
element

O.S.H Nail
In end of
top fence rail

r Main
DBYD

Damage Easement 4.00m Wide



10m Refuse Collection Vehicle

MENIN DRIVE

total: 10.0m

10.0m

O.CP

Nail = 59.75

CP = 59.63

CP

Nail = 59.82

Generic 10m RCV

CP = 59.73

Generic 10m RCV

Custom

Generic 10m RCV

Custom

Generic 10m RCV

Custom

Generic 10m RCV

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Generic 10m RCV

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Generic 10m RCV

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Generic 10m RCV

Custom

Generic 10m RCV

Custom

Electricity structure element

O.S.H Nail
In end of
top fence rail

r Main
DBYD

1000 MM DRIVEWAY

Generic 10m RCV

Custom

Generic 10m RCV

Custom

Generic 10m RCV

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Generic 10m RCV

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Generic 10m RCV

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Generic 10m RCV

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Generic 10m RCV

Custom

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
3003 SQM

10.0m

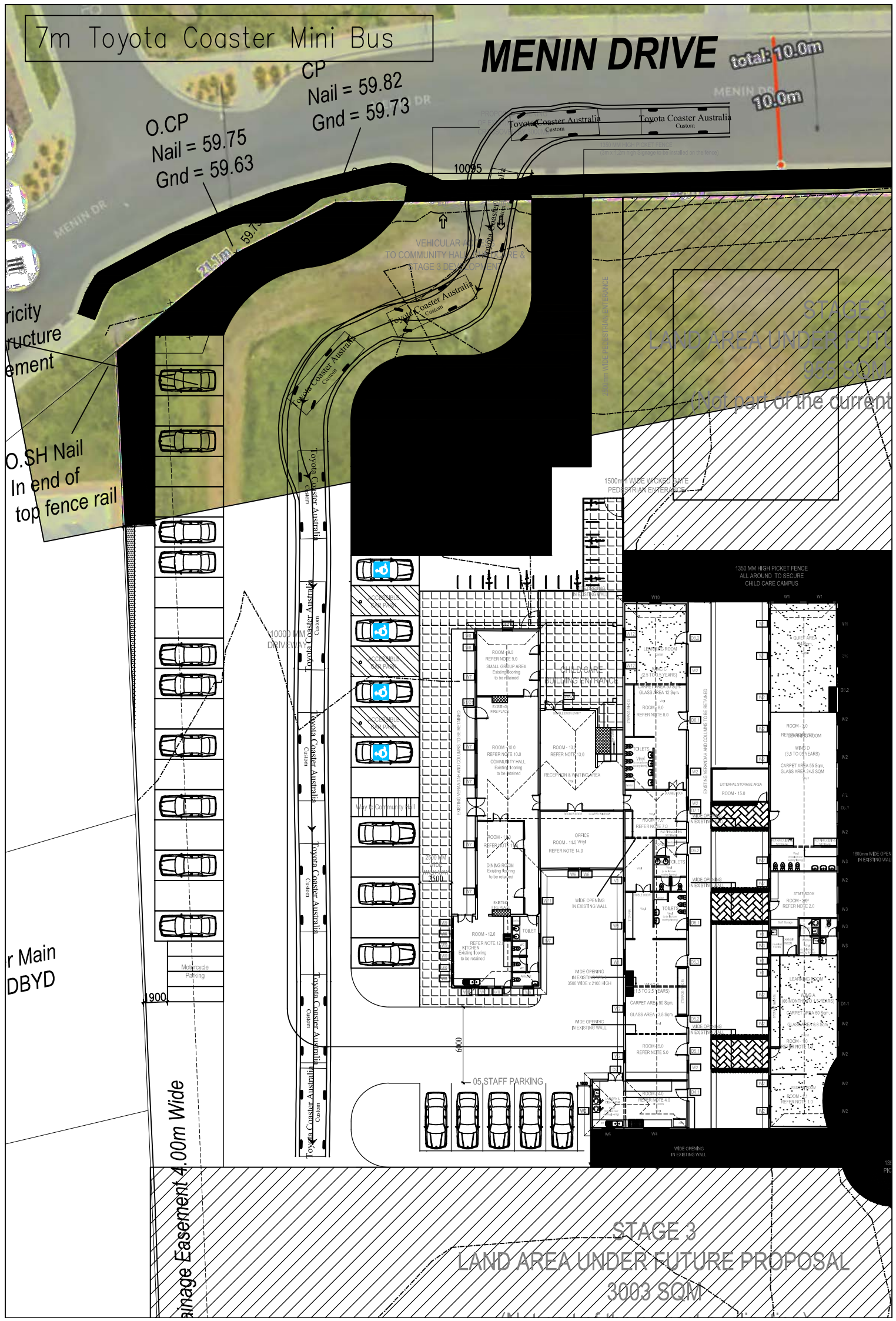
CP
Nail = 59.82
Gnd = 59.73

O.S.H Nail
In end of
top fence rail

r Main
DBYD

sidewalk Easement 4.00m Wide

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
3003 SQM



7m Toyota Coaster Mini Bus

MENIN DRIVE

total: 10.0m

10.0m

O.CP
Nail = 59.75
Gnd = 59.63

CP
Nail = 59.82
Gnd = 59.73

Electricity
structure
element

O.SH Nail
In end of
top fence rail

r Main
DBYD

Drainage Easement 4.00m Wide

STAGE 3
LAND AREA UNDER FUTURE PROPOSAL
3003 SQM

B99 Car Exit / B85 Car Entry

MENIN DR

CP

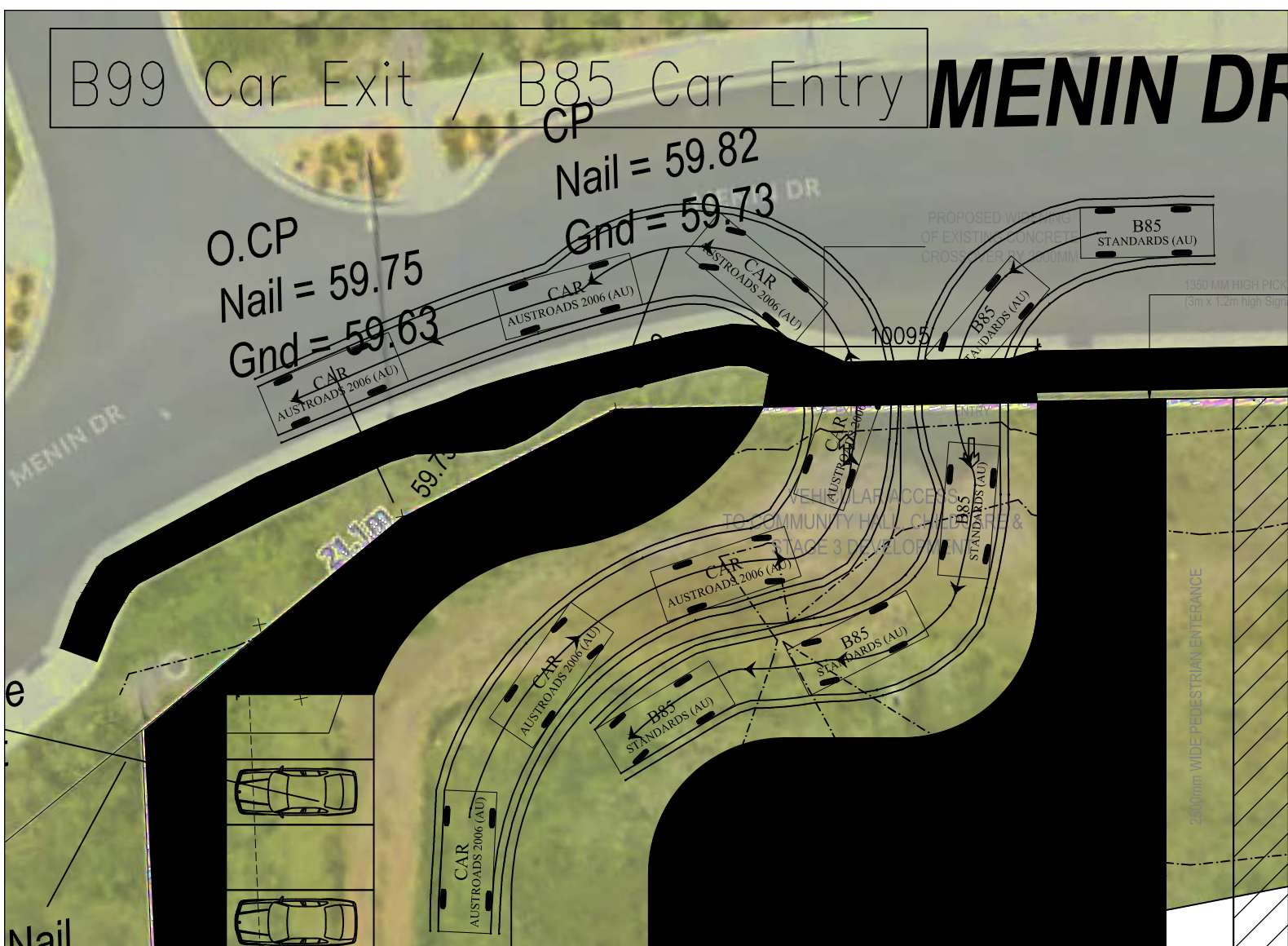
Nail = 59.82

Gnd = 59.73

O.CP

Nail = 59.75

Gnd = 59.63



B85 Car Exit / B99 Car Entry

MENIN DR

CP

Nail = 59.82

Gnd = 59.73

O.CP

Nail = 59.75

Gnd = 59.63

B85
STANDARDS (AU)

B85
STANDARDS (AU)

B85
STANDARDS (AU)

CAR
AUSTROADS 2006 (AU)

1350 MM HIGH PICK
(3m x 1.2m high Sign)

10095

PROPOSED WIDENING
OF EXISTING CROSSING
CROSSOVER

TO COMMUNITY HALL, CHILDREN'S &
STAGE 3 DEVELOPMENT

2500mm WIDE PEDESTRIAN ENTERANCE

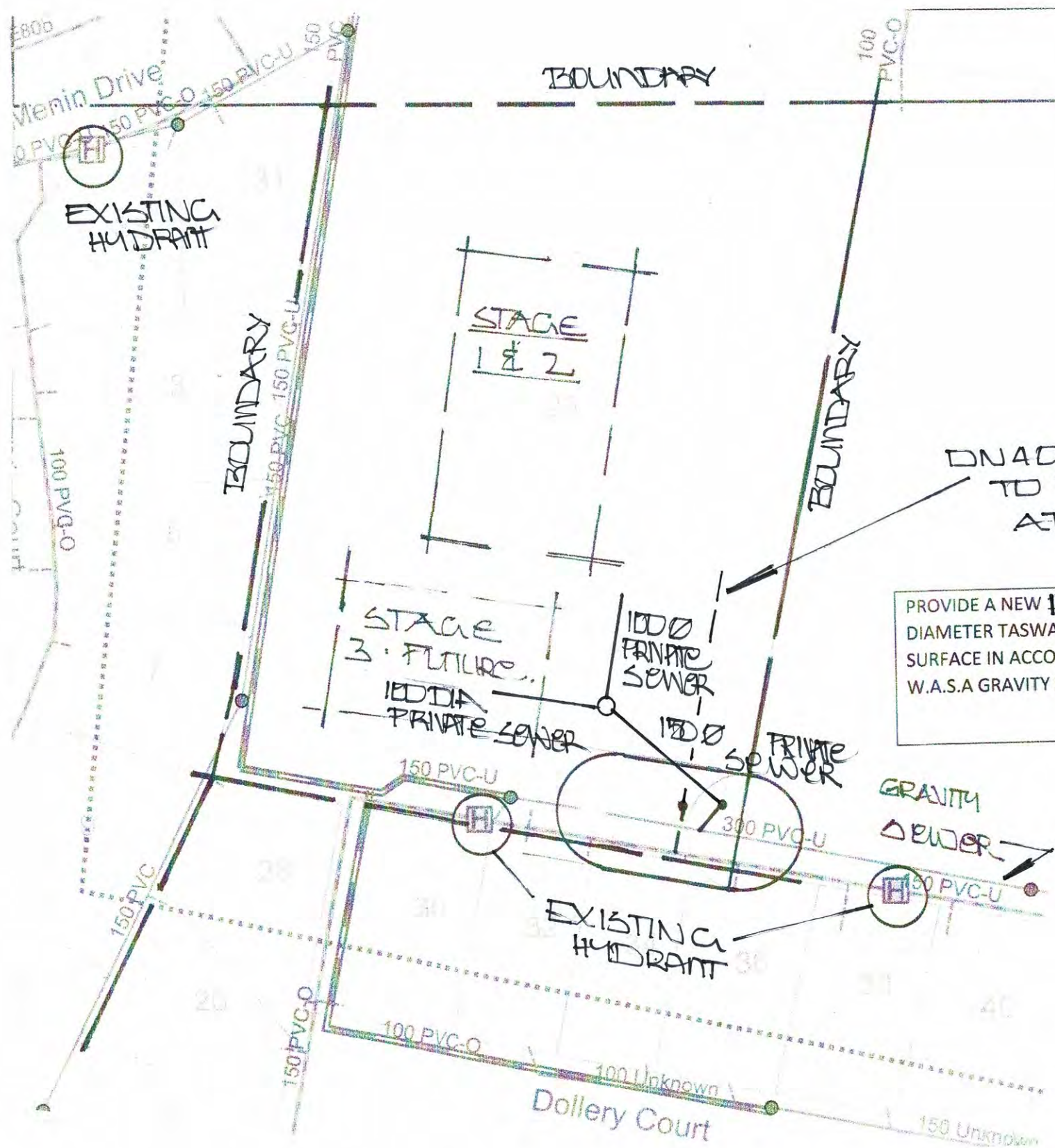


B85
STANDARDS (AU)

CAR
AUSTROADS 2006 (AU)

CAR
AUSTROADS 2006 (AU)

Nail



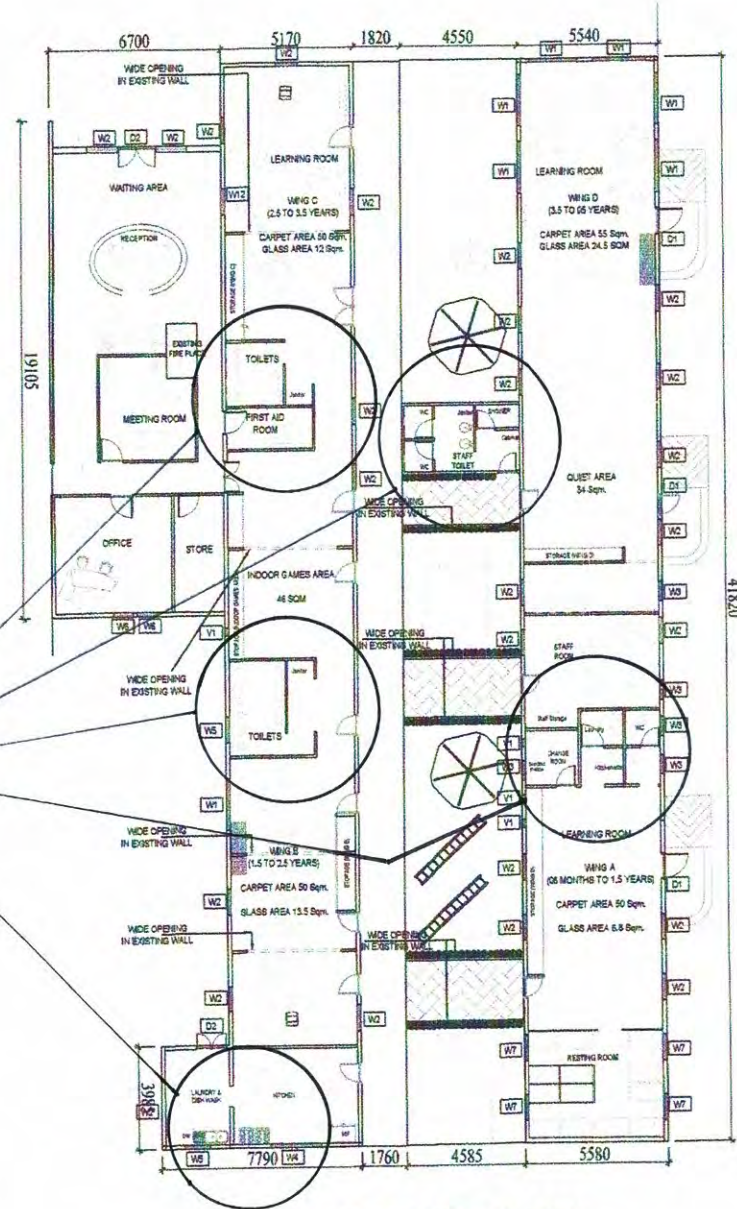
DN40 ID COLD WATER METERED SUPPLY TO BE PROVIDED BY TASWATER AT DEVELOPERS COST.

PROVIDE A NEW 150mm DIAMETER PVC-U SN10 SEWER CONNECTION TO THE EXISTING 150mm DIAMETER TASWATER MAIN IN ACCORDANCE WITH MRWA-S-302, 100mm I.O TO BE RAISED TO SURFACE IN ACCORDANCE WITH FIG 6.1 OF THE TASMANIAN SUPPLEMENT OF WSA-02-2014 -3.1 W.A.S.A GRAVITY SEWER CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES ADDITION.

ALL WORK BY TASWATER AT THE DEVELOPERS COST

THIS DOCUMENT IS IN SUPPORT OF A PLANNING APPLICATION - THE DRAWING IS NOT FOR CONSTRUCTION

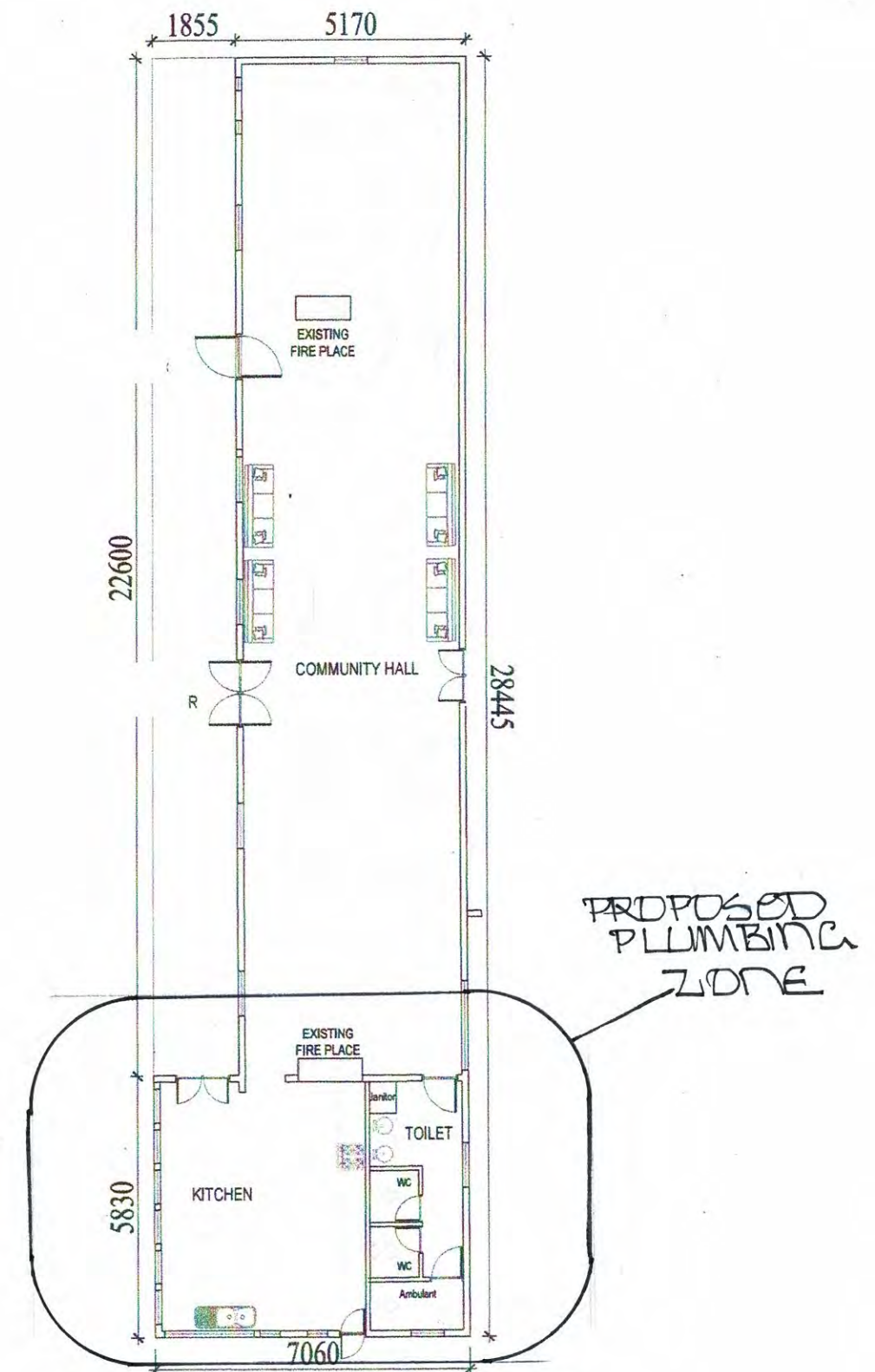
STAGE 1



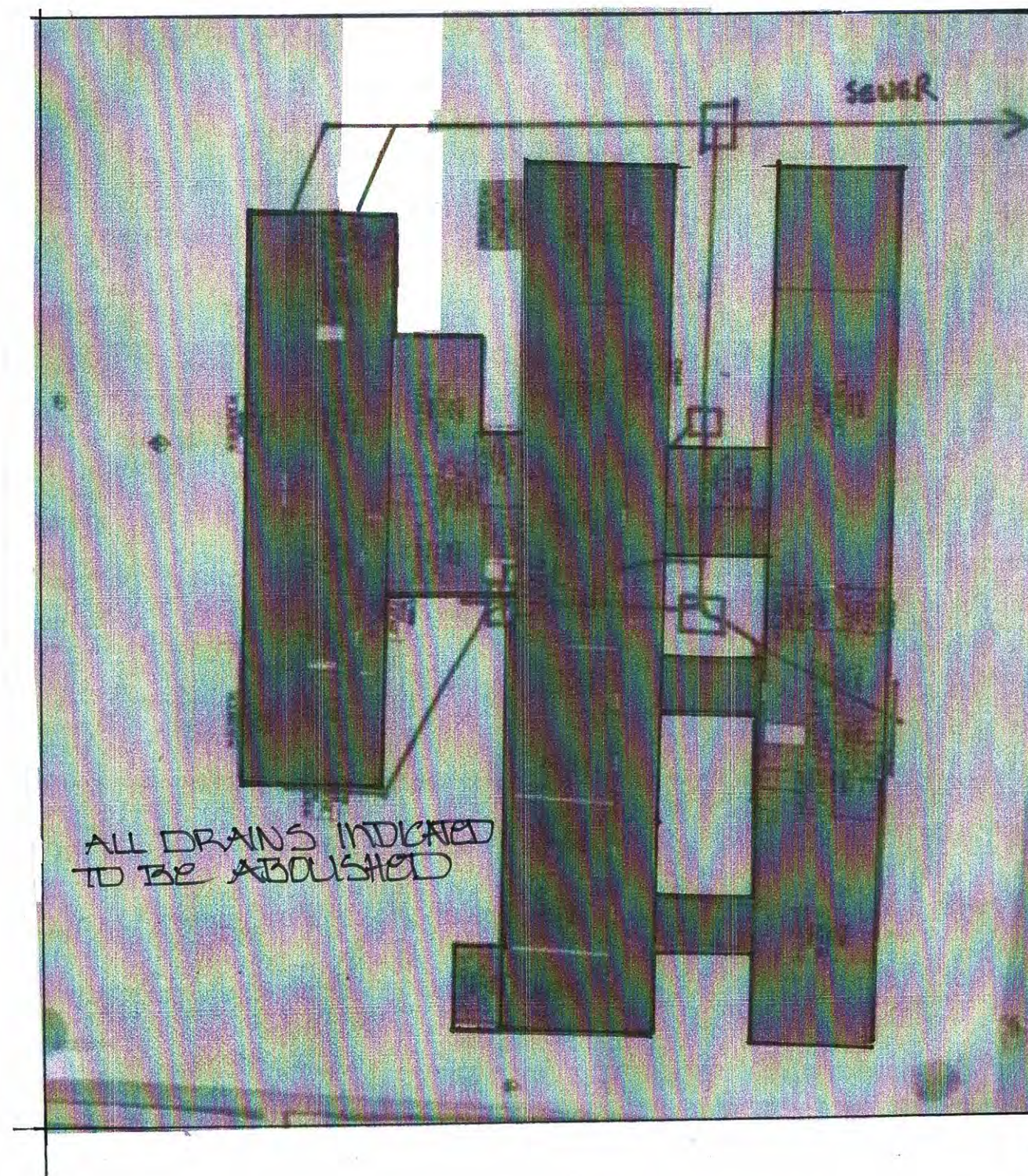
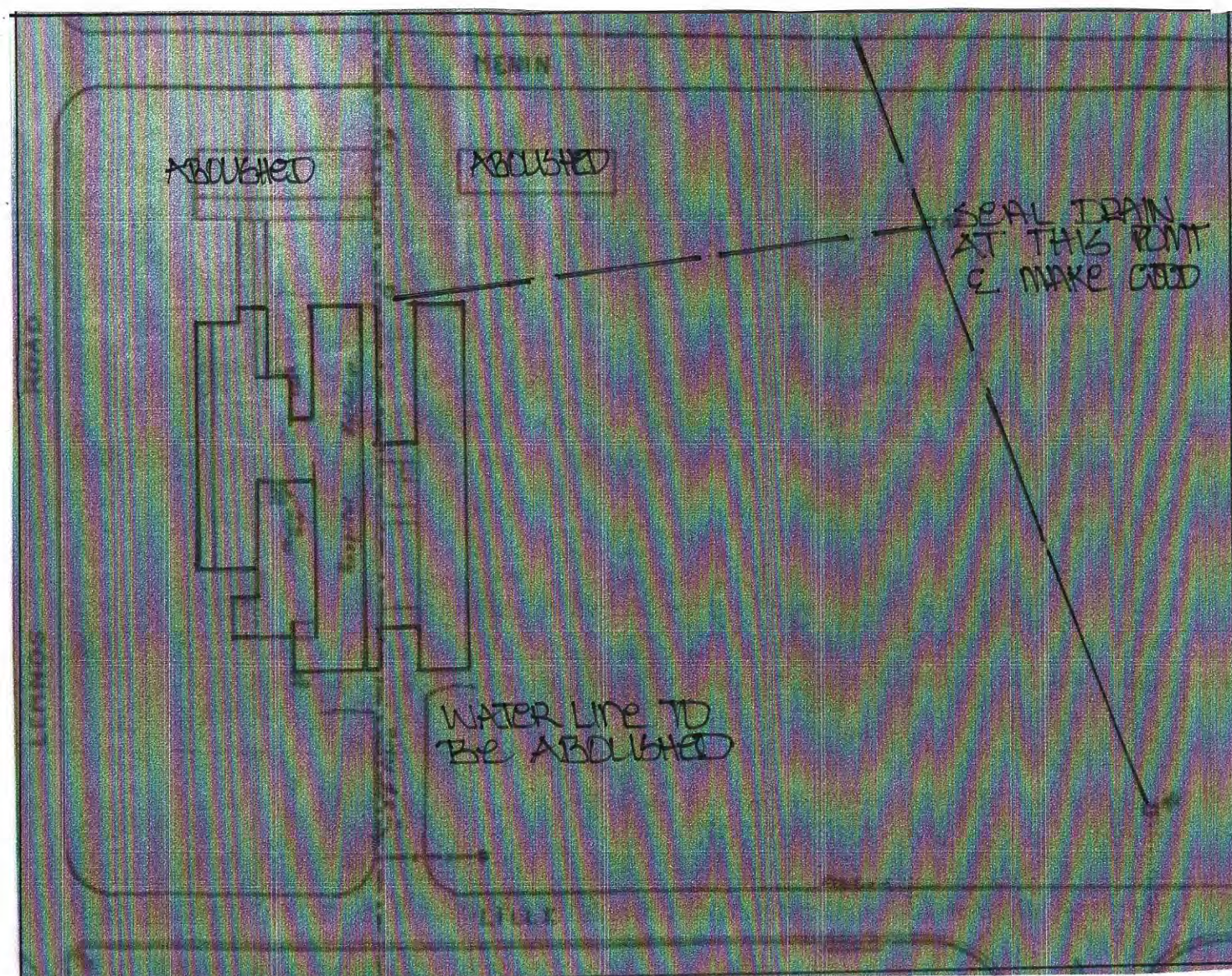
**DIMENSION PLAN
EARLY LEARNING CENTRE
SCALE 1:200**

THIS DOCUMENT IS IN SUPPORT OF A PLANNING APPLICATION – THE DRAWING IS NOT FOR CONSTRUCTION

STAGE 2



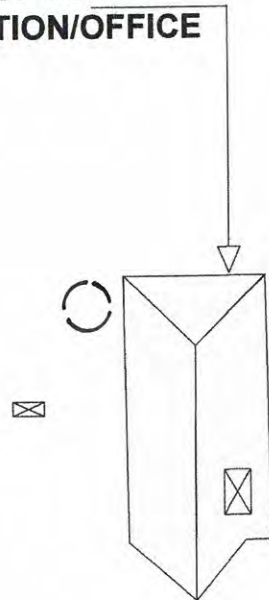
**DIMENSION PLAN COMMUNITY HALL
SCALE 1:100**



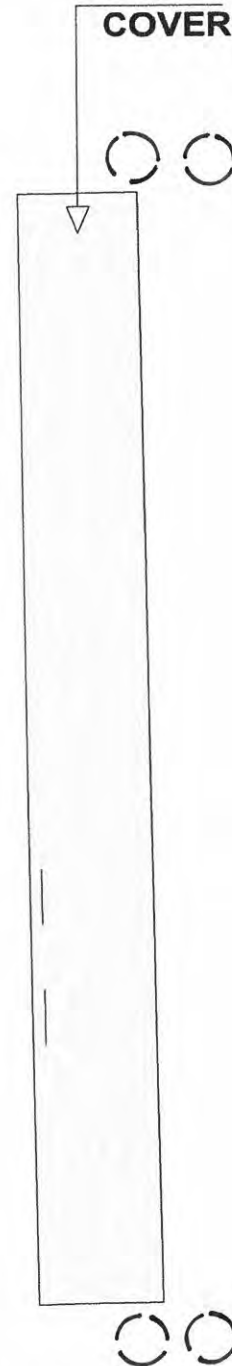
THIS DOCUMENT IS IN SUPPORT OF A PLANNING APPLICATION – THE DRAWING IS NOT FOR CONSTRUCTION

PROJECT TITLE: PROPOSED EARLY LEARNING CENTRE & COMMUNITY HALL DEVELOPMENT AT BRIGHTON

PROPOSED
RECEPTION/OFFICE

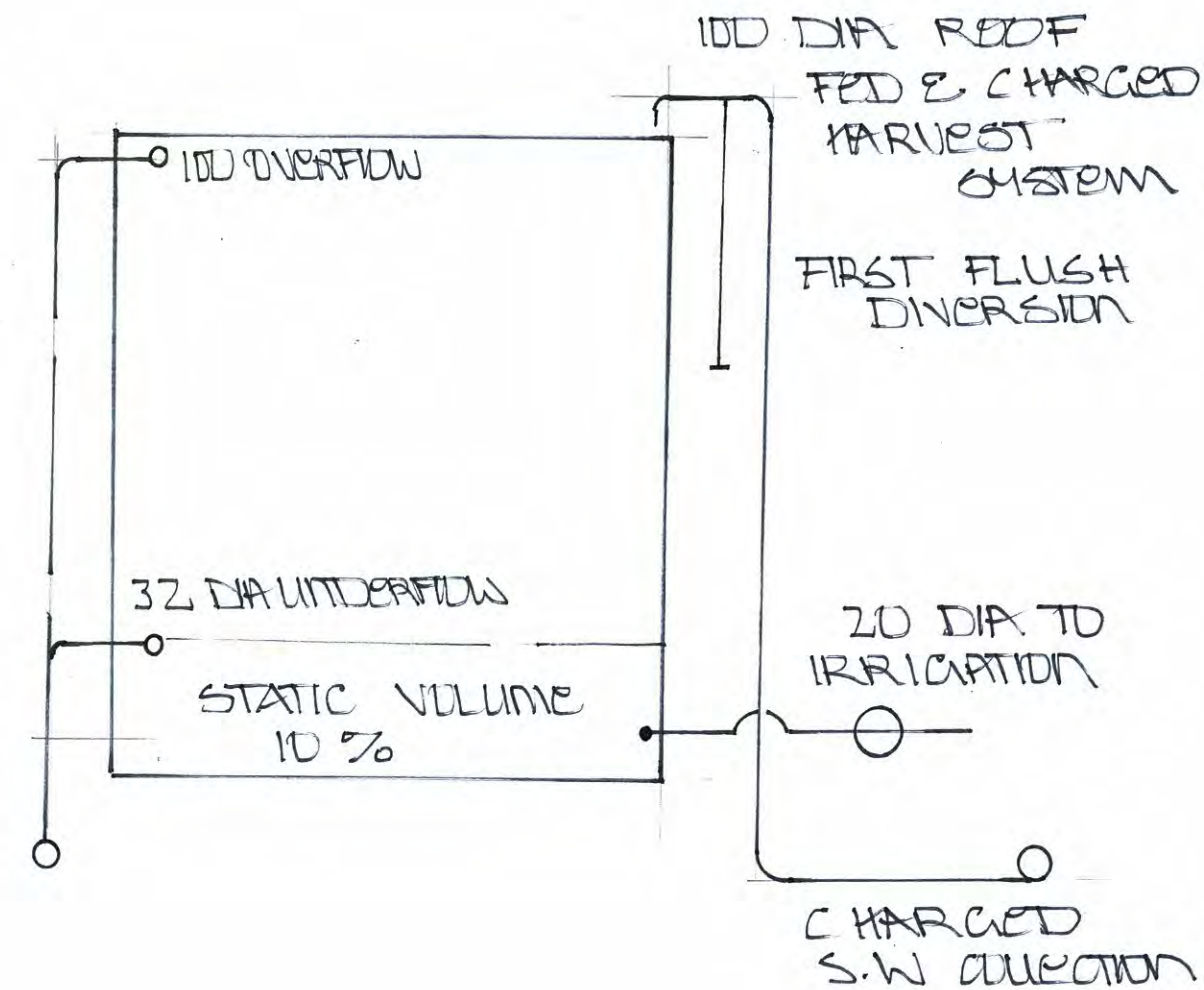


PROPOSED
POLYCARBONATE SHEET
COVERED PLAY AREA



ROOF PLAN
SCALE 1:200

TYPICAL RAINWATER
ROOF HARVEST TANK
DETAIL.



THIS DOCUMENT IS IN SUPPORT OF A PLANNING APPLICATION – THE DRAWING IS NOT FOR
CONSTRUCTION

SANITARY DRAINAGE: FROM AS/NZS 3500:2

STAGE 2				No	TOTAL FU
FIXTURE	FIXTURE UNIT				
SINK	3			2	16
BASIN	1			4	4
WATER CLOSET	6			3	18
FLOOR WASTE	2			4	5
STAGE 1				No	TOTAL FU
FIXTURE	FIXTURE UNIT				
SINK	3			3	9
BASIN	1			8	8
WATER CLOSET	6			15	90
WATER HEATER	N/A				
TROUGH	5			1	5
WASH MACHINE	5			1	5
SHOWER	2			2	4
STAGE 3					
DWELLINGS (X4)	1.03			4	
SINK	3			4	12
BASIN	1			8	8
SHOWER	4			8	32
TROUGH	5			4	20
CWM	5			4	20
DWM	3			4	12
WC	4			8	32
TOTAL					300FU

TOTAL FIXTURE UNITS FOR STAGES 1, 2 & 3 IS 300: THEREFORE A NEW SEWER BRACH OF 150mm DIAMETER IS REQUIRED, THE 150MM DIAMETER UPVC PIPELINE WILL BE REQUIRED TO EXTEND TO A NEW ACCESS SHAFT UPSTREAM OF THE CONNECTION FROM STAGE 3 4 DWELLINGS, BEYOND THAT POINT A 100mm DIAMETER UPVC PRIVATE SEWER WILL BE EXTENDED AT 2.5% GRADE.

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WATER SUPPLY – PROBABLE SIMULTANEOUS FLOW RATE : FROM AS/NZS3500.1.2021 TABLE 3.2.1 TABLE 6.3(A)

STAGE 2					
FIXTURE	FLOW L/S	FLOW L/M	LOADING UNITS	No	PSFR
SINK	0.12	7	3	2	0.20
BASIN	0.10	6	1	4	0.16
WATER CLOSET	0.10	6	2	3	0.20
WATER HEATER	0.20	12	8	1	0.09
STAGE 1					
FIXTURE	FLOW L/S	FLOW L/M	LOADING UNITS	No	PSFR
SINK	0.12	7	3	3	0.20
BASIN	0.10	6	1	8	0.16
WATER CLOSET	0.10	6	2	15	0.20
WATER HEATER	0.20	12	8	3	0.09
TROUGH	0.12	7	2	1	0.09
WASH MACHINE	0.20	12	8	1	0.09
SHOWER	0.10	0.10	6	2	0.12
STAGE 3				No	PSFR
DWELLINGS (X4)	1.03			4	1.03

TOTAL PROBABLE SIMULTANEOUS FLOW RATE FOR COMPLETE DEVELOPMENT (STAGE 1, 2 & 3 IS ESTIMATED TO BE 1.98 L/S AT 350KPA.

HYDRAULIC SERVICES REPORT FOR 23 MENIN DRIVE – DA STAGE

THE PROJECT IS FOR REDVELOPMENT OF AN EXISTING BUILDING COMPLEX LOCATED AT 23 MENIN DRIVE BRIGHTON TASMANIA. THE BUILDINGS HAVE PREVIOUSLY BEEN USED BY THE AUSTRALIAN DEFENCE FORCE AS A COMPONENT OF THE NOW CLOSED MILITARY BASE. THE BUILDINGS HAVE BEEN VACANT FOR SOME TIME AND THE PLAN IS TO REPURPOSE THEM IN A 3 STAGE DEVELOPMENT, STAGES 1 WILL BE AN EARLY LEARNING CENTRE AND STAGE 2 A COMMUNITY HALL

STAGE 3 ON THE SAME SITE IS LIKELY TO BE PRIVATE HOUSING WITH 4 DWELLINGS TO BE THE SUBJECT OF A SEPARATE APPLICATION SOME 8 YEARS FORWARD.

THIS REPORT WILL IDENTIFY LIKELY REQUIREMENTS FOR THE DEVELOPMENT IN RESPECT OF PROVISION OF HYDRAULIC SERVICES.

RECORDS OF EXISTING SERVICES ARE NOT EXTENSIVE, HOWEVER, GIVEN THE AGE AND LIKELY CONDITION OF THOSE SERVICES .THE DEVELOPER HAS DECIDED TO *ABANDON ALL EXISTING WATER, SEWER AND BUILDING STORMWATER INFRASTRUCTURE FOR REPLACEMENT WITH NEW.*

WATER RETICULATION

ALL EXISTING SERVICES WILL BE TERMINATED AND SEALED, ANY WORK TO MAINS CONNECTIONS TO THE SITE BY TASWATER WILL BE UNDERTAKEN BY TASWATER AT THE DEVELOPERS COST. A PRELIMINARY BUILDING SURVEYORS REPORT IDENTIFIES 4 OPERATIONAL EXISTING BALL TYPE FIRE HYDRANTS IN ROAD SPACES ADJACENT TO THE SITE THAT WILL PROVIDE AN ACCEPTABLE LEVEL OF FIRE PROTECTION EXTERNAL TO THE BUILDINGS. BASED ON THE CURRENT DOCUMENTATION THE COMPLEX IS DEEMED TO BE LOW RISK FOR WATER BACKFLOW PROTECTION.

WASTEWATER MANAGEMENT

RECORDS INDICATE THAT EXISTING WASTEWATER FROM THE BUILDINGS DISCHARGES TO A MANHOLE LOCATED ON AN OLD ADF INTERNAL SEWER SYSTEM, THIS BRANCH WILL BE SEALED WITHIN THE EXISTING MANHOLE AND ALL WASTEWATER DRAINAGE ASSOCIATED WITH THE REDEVELOPMENT WILL BE REDIRECTED TO A NEW SEWER BRANCH FROM THE TASWATER MAIN WHICH WILL BE CONSTRUCTED BY TASWATER AT THE DEVELOPERS COST.

INTERNAL FIRE PROTECTION FROM A HYDRAULIC PERSPECTIVE WILL INCLUDE STANDARD 36 METRE FIRE HOSE REELS INSTALLED TO MEET TASFIRE REQUIREMENTS AND N.C.C. THE SITE WATER CONNECTION WILL BE SIZED TO INCLUDE ALLOWANCES FOR FIRE HOSE REELS. NO ALLOWANCE HAS BEEN MADE AT THIS STAGE FOR ANY INTERNAL FIRE SPRINKLERS.

ROOFWATER HARVEST.

THE DEVELOPER WISHES TO INSTALL A SERIES OF RAINWATER TANKS TO COLLECT RAINWATER FROM THE BUILDING ROOFS, THIS HARVESTED WATER WILL BE USED INTERNALLY FOR LANDSCAPE IRRIGATION , THE TANKS WILL PROVIDE A MEASURE OF STORMWATER DETENTION BY VIRTUE OF AN OVERFLOW/UNDERFLOW SYSTEM APPLIED TO THE STORAGE TANKS.

EXISTING STORMWATER

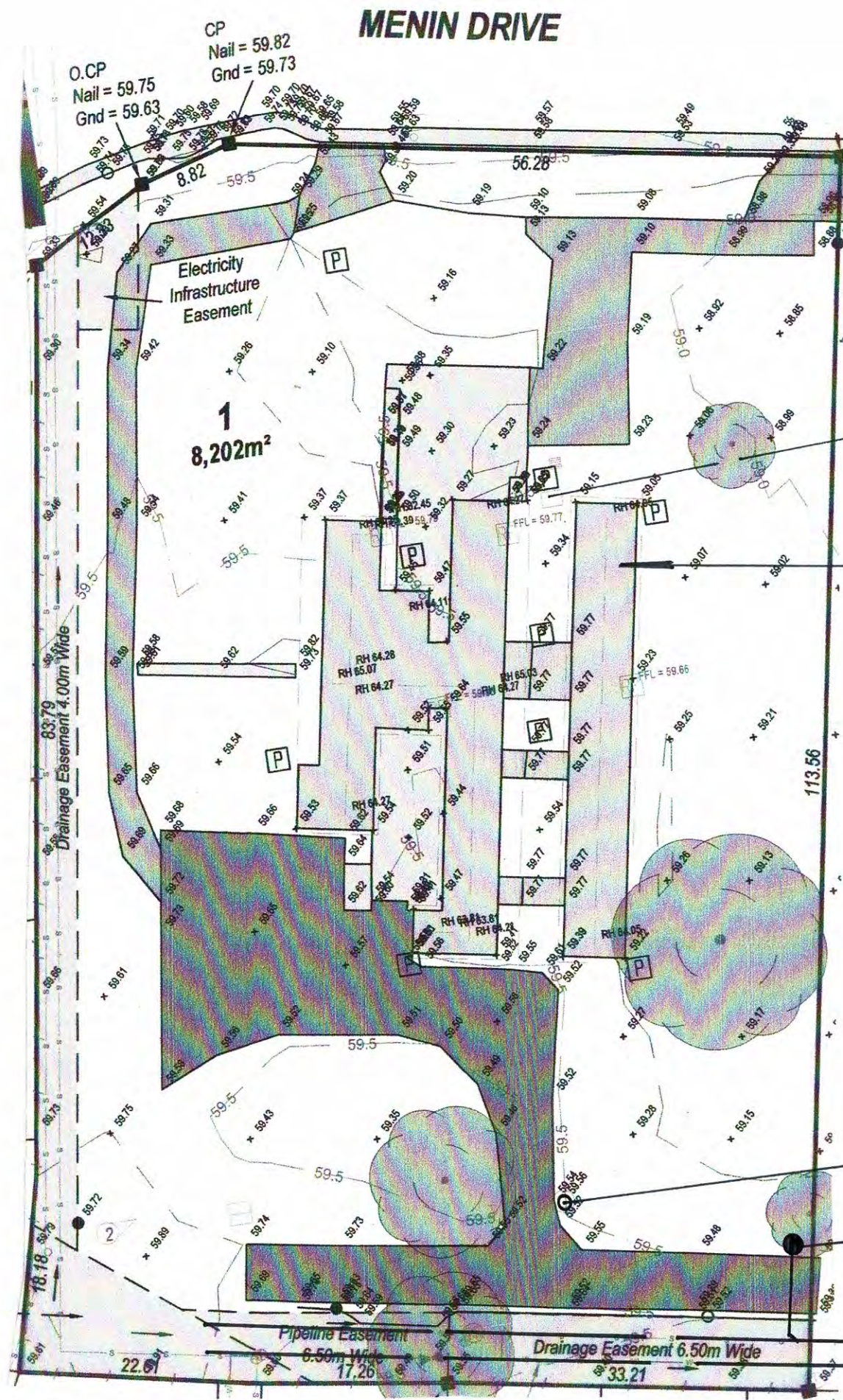
THE SITE HAS AN EXISTING STORMWATER DISPOSAL SYSTEM THAT DISCHARGES TO THE ROADWAY DRAINAGE. THE INTENTION OF THE DEVELOPER IS TO REVIEW THE EXISTING INTERNAL STORMWATER SYSTEM USING CCTV CAMERA, MAKE REPAIRS AND/OR ESSENTIAL REMEDIAL WORK TO THE EXISTING SYSTEM AND ALLOW TO INCLUDE NEW CONNECTIONS TO THE OUTFALL FROM THE PROPOSED ROOFWATER HARVEST TANKS OVERFLOWS. A STATIC VOLUME FOR LANDSCAPE IRRIGATION AND TO PROVIDE A LEVEL OF STORMWATER DETENTION FROM THE SITE TO ENSURE THAT THE DISCHARGE VOLUME WILL BE NO GREATER THAN THAT EXISTING IS INCLUDED.

IT IS ALSO PROPOSED THAT EACH (FUTURE) DWELLINGG IN STAGE 3 WILL DISCHARGE ROOF WATER TO SEPARATE TANKS (1 PER DWELLING) TO BE USED FOR LANDSCAPE IRRIGATION AND LIKLEY WE FLUSHING (THE NCC IS UNDERGOING REVIEW IN THIS RESPECT AND ANY FUTURE TANK SYSTEMS WILL ADDRESS THE CURRENT N.C.C.

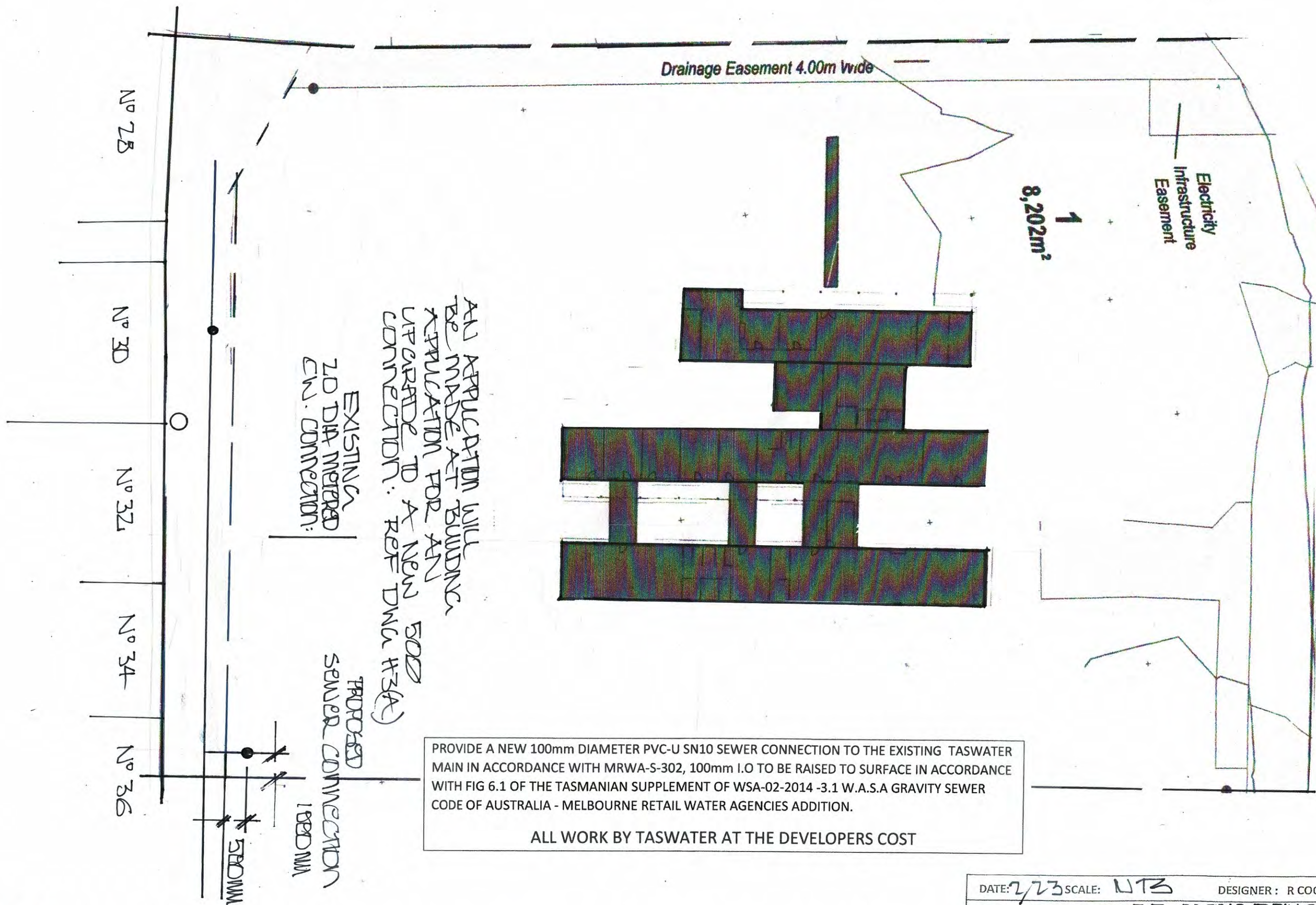
STORMWATER MANAGEMENT – GENERALLY

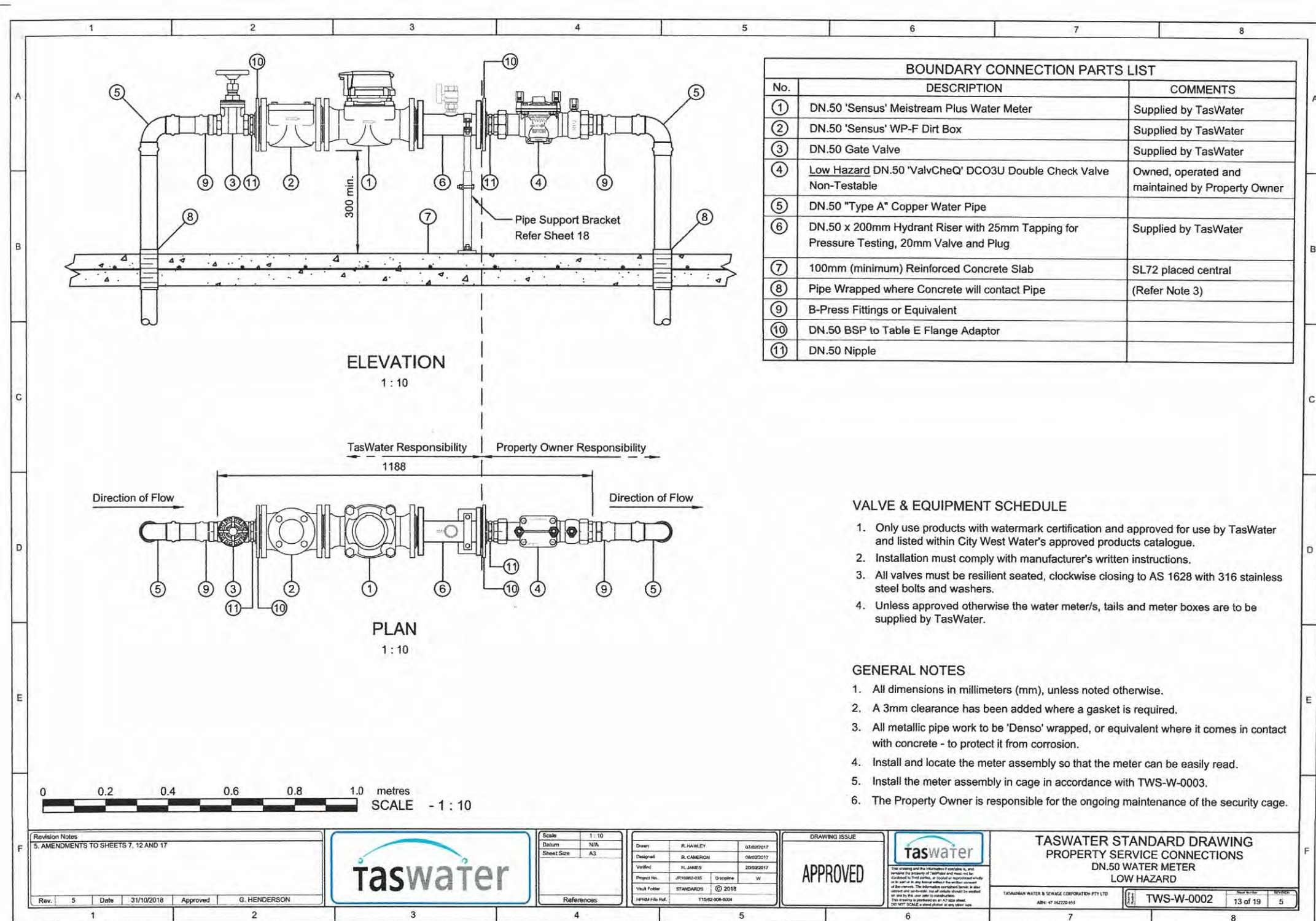
EXISTING STORMWATER PITS IN PAVED AREAS WILL BE UPGRADED TO INCORPORATE INTEGRAL SILT RETENTION FACILITIES, THIS WILL INCLUDE HINGED GRATES FOR EASE OF SILT REMOVAL TO BE LINKED TO THE BUILDING MAINTENANCE PROGRAM.

THIS DOCUMENT IS IN SUPPORT OF A PLANNING APPLICATION – THE DRAWING IS NOT FOR CONSTRUCTION



DATE: 7/23	SCALE: N.T.S.	DESIGNER: R COOPER	ISSUE: A
DWG: H 1	PROJECT: 23 MENIN DRIVE BRANCH		
TITLE:	HYDRAULIC SERVICES	PROJECT NUMBER:	23/12





THE ABOVE PROPERTY CONNECTION DETAIL FOR WATER SUPPLY IS BASED ON DOCUMENTATION AND PROPOSED USES FOR PLANNING APPROVAL.

Single hydrant flow test statement

A flow test undertaken in accordance with AS 2419.1-2005
has been conducted at Flow Testing @ 23 Menin Drive
3 Menin Drive, BRIGHTON, TAS, 7030
on 13/12/2022

Hydrant Logbook Number: Na

Hydrant Type: Groundball

Hydrant Location: Back fence of 40 dollery drive brighton

Test Time: 10:03

Static Pressure: 725 KPa

Residual Pressure: 13.90 L/s @ Unrestricted (Open) Flow
12.10 L/s @ 200KPa
9.80 L/s @ 350KPa

Note: For an interpretation of the results of this flow test, please contact the relevant Water Authority or other competent person.

The hydrant test results appear to meet the requirements of AS2419.1-2005, Table 2.2.

Section E – Services and Equipment

All services are to be designed by licensed services designers

Fire services

The buildings floor area is less than 1,000m² and greater than 500m², therefore the following features are required:

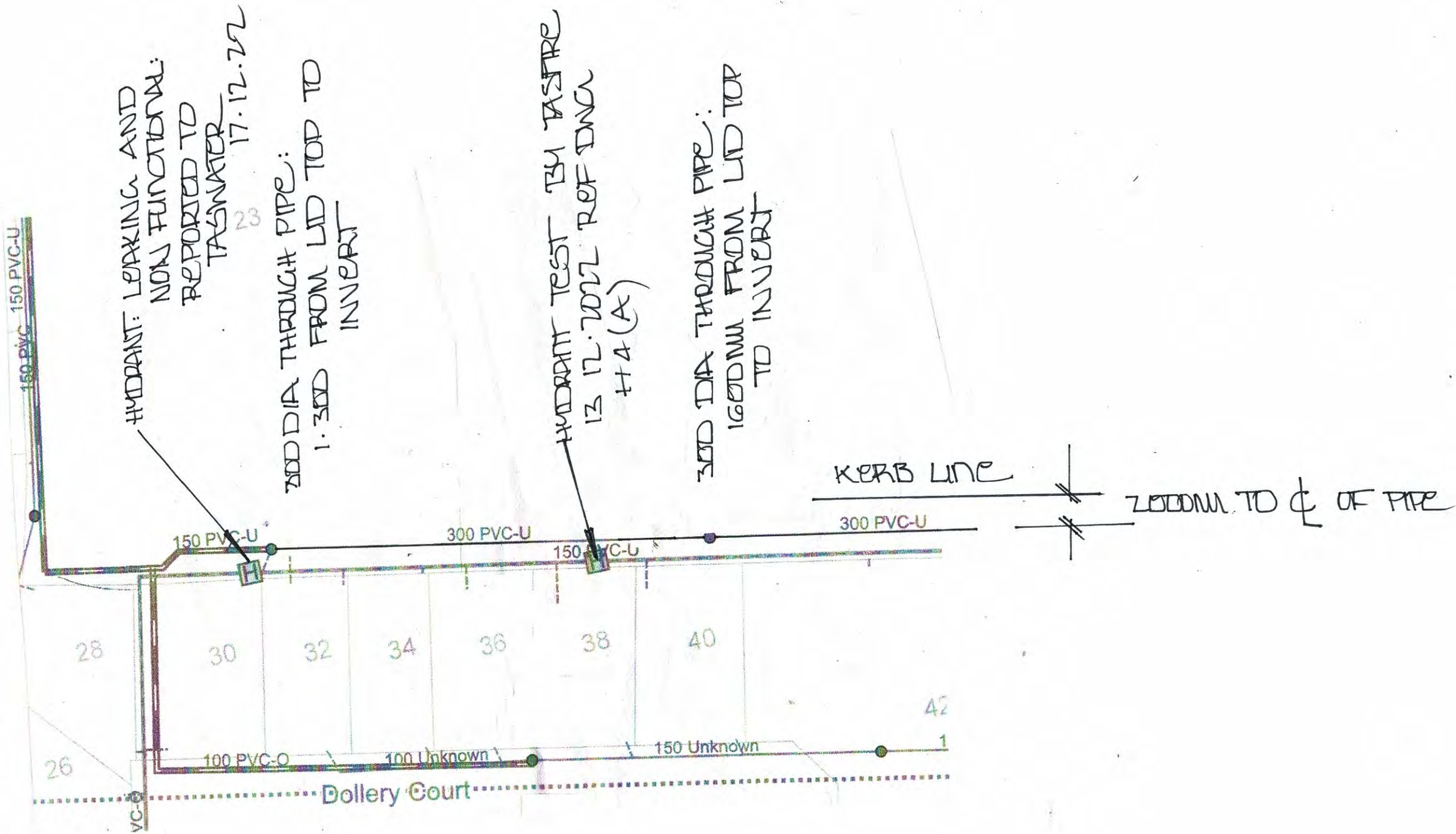
- AS1670.1 – 2018 – Smoke Detection and Alarm throughout
- Fire Hydrants, there are multiple street hydrants surrounding the site on both Menin Drive and to the rear of the site. It is likely that these hydrants will be able to service the site – to be confirmed with accurate to scale hose lay drawings.



- Fire Hose Reels
- Portable Fire Extinguishers (design documentation not required)

BASED ON THE DOCUMENTATION PRESENTED FOR PLANNING APPROVAL AND THE INFORMATION OBTAINED REGARDING THE EXISTING CAPACITY OF THE TASWATER WATER MAIN A PROBABLE SIMULTANEOUS FLOW RATE FOR THE PROJECT IS CALCULATED AS 1.4 L/S @ 350kpa.

LIKELY PRESSURE/FRICTION LOSS ACROSS THE SERVICE BRANCH AND ASSOCIATED EQUIPMENT IS CALCULATED AT 45kpa



RECORD OF FIELD INVESTIGATION DECEMBER 2022, ALL DIMENSIONS TAKEN BY PROTECH AND RECORDED BY REC HYDRAULIC DESIGN CONSULTANTS P/L

23 MENIN DRIVE BRIGHTON TASMANIA

RESPONSE TO TASWATER R.A.I. REF No TWDA2022/00263-BTN

THIS INFORMATION IS PROVIDED IN SUPPORT OF AN APPLICATION FOR DEVELOPMENT APPROVAL ONLY – DETAILED SERVICES DRAWINGS WILL FORM A COMPONENT OF A FUTURE BUILDING APPLICATION.

THE EXISTING BUILDING IS TO BE SEPARATED INTO TWO DISTINCT FUNCTIONAL SPACES, ONE A COMMUNITY HALL AND THE OTHER A COMMUNITY CENTRE, THE EXISTING BUILDING FOOTPRINT IS TO BE RETAINED WITHOUT ANY INCREASE, THE BUILDINGS WILL NOT BE SEPARATED OR SUBDIVIDED IN ANY WAY.

THE EXISTING WASTEWATER DRAINAGE SYSTEM WILL BE ABANDONED, THE DEVELOPMENT WILL CONNECT TO THE NEW PRIVATE SEWER DRAINAGE SYSTEM.

IT IS PROPOSED TO RENOVATE THE BUILDINGS INTERNALLY INCLUSIVE OF ABOLITION OF THE EXISTING WASTEWATER DRAINAGE SYSTEM DATING FROM THE TIME THE BUILDING WAS A MILITARY ESTABLISHMENT, A NEW PRIVATE SEWER SYSTEM WILL BE INSTALLED INTERNALLY TO CONNECT TO A NEW SEWER BRANCH TO BE PROVIDED BY TASWATER AT THE DEVELOPERS COST.

IN TOTAL NEW SANITARY PLUMBING FIXTURES ARE AS LISTED BELOW:

CHILD CARE CENTRE

FIXTURE TYPE	F.U RATING	No OF FIXTURES	TOTAL	OTHER INFORMATION
WATER CLOSET	4	6	24	
HAND BASIN	1	7	7	
SHOWER BAY	2	NIL	NIL	
KITCHEN SINK	3	1	3	DOMESTIC USE
CLEANERS SINK	3	1	3	

COMMUNITY HALL

FIXTURE TYPE	F.U RATING	No OF FIXTURES	TOTAL	OTHER INFORMATION
WATER CLOSET	4	3	12	
HAND BASIN	1	3	3	
SHOWER BAY	2	1	2	
KITCHEN SINK	3	1	3	DOMESTIC USE
CLEANERS SINK	3	1	3	

PRIVATE SEWER DRAINAGE LOADING (REFER AS/NZS3500.2.-2021) BASED ON A PROPOSED DESIGN GRADIENT OF NOT LESS THAN 2.5% FOR A 100mm DIAMETER UPVC DRAINAGE SYSTEM IS NOT TO

EQUIVALENT TENEMENTS – REFERENCE (SECTION 64 DETERMINATION OF EQUIVALENT TENEMENTS)

CATEGORY	STANDARD UNIT	WATER ET	TOTAL	SEWER ET	TOTAL	COMMENT
AMENITIES AND INDOOR FACILITIES	FLOOR AREA 201 SQ METRES	0.02	4.02(ET)	0.02	4.02 (ET)	
CHILD CARE	80 PERSONS	0.06	4.8(ET)	0.10	8.0 (ET)	

100mm DIAMETER SEWER BRANCH TO SERVE THE PROPERTY WITH ALL ASSOCIATED WORK FOR THE BARNCH TO BE BY TASWATER AT THE DEVELOPERS COST.

LOCATION OF TASWATER ASSETS.

TASWATER ASSETS ON THE SITE (WATER MAIN & SEWER MAIN) WERE LOCATED AS FOLLOWS:

SEWER MANHOLE ADJACENT TO 30/32 DOLLERY COURT – MANHOLE LID LIFTED, DEPTH CHECKED, PIPE DIAMETER CHECKED – LID REFITTED & CHECKED.

SEWER MANHOLE ADJACENT TO 40 DOLLERY COURT – MANHOLE LID LIFTED, DEPTH CHECKED, PIPE DIAMETER CHECKED- LID REFITTED AND CHECKED.

TASWATER WATER MAIN PARALLEL TO DOLLERY COURT AND REAR BOUNDARIES OF 30 TO 40 DOLLERY COURT – LINE TAKEN BETWEEN 2 HYDRANTS DEPTH OF PIPE CHECKED TO BE 750mm FROM EXISTING SURFACE LEVEL TO TOP OF PVC- U WATER MAIN.

THE PROPOSED COLD WATER SUPPLY TO THE BUILDING WILL BE INSTALLED UNDERGROUND AT A MAXIMUM OF 600mm, THE BALANCE OF THE INTERNAL WATER RETICULATION WILL BE INSTALLED IN WALL CAVITIES OR CEILING SPACES WITHIN THE BUILDING FOOTPRINT.

PRESSURE AND FLOW TESTS UNDERTAKEN BY TASFIRE 13/12/2022 AS DESCRIBED ON INCLUDED TASFIRE STATEMENT.

REPORT FROM BUILDING SURVEYOR - NIGEL GRICE STATES THAT THERE ARE SUFFICIENT EXISTING FIRE FIGHTING HYDRANTS SURROUNDING THE SITE TO PROVIDE ADEQUATE COVER – NO ADDITIONAL PRIVATE FIRE HYDRANTS WILL BE REQUIRED FOR THE DEVELOPMENT

NOTE THAT THE EXISTING FIRE HYDRANT ADJACENT TO THE REAR BOUNDARY OF NUMBER 30 DOLLERY COURT WAS FOUND TO BE LEAKING BUT NOT FUNCTIONAL – A REQUEST WAS FORWARDED TO TASWATER ON DECEMBER 17TH 2022 FOR REMEDIAL WORK TO BE UNDERTAKEN, AS AT FEBRUARY 3RD 2023 NO CONFIRMATION OF A REPAIR HAS BEEN RECEIVED, FUNCTIONALITY OF THE SUBJECT HYDRANT IS FUNDAMENTAL TO PROVISION OF ADEQUATE FIRE FIGHTING COVERAGE FOR THE DEVELOPMENT.

WATER SUPPLY REQUIREMENTS REFERENCE (AS/NZS3500.1:2021- TABLE 3.2.1)

FIXTURE/APPLIANCE	No	FLOW RATE L/S	FLOW RATE L/M	LOADING UNITS
WATER CLOSET	9	0.10	6.0	18
HAND BASIN	10	0.10	6.0	10
SHOWER BAY	1	0.10	6.0	2
KITCHEN SINK	2	0.10	6.0	12
M.P HOT WATER CYL	3	0.20	12.0	24
HOSE TAP 15mm DIA	4	0.20	12.0	16
TOTAL				82

SITE INVESTIGATION HAS CONFIRMED THE EXISTENCE OF A 20mm DIAMETER METERED COLD WATER SUPPLY ADJACENT TO THE ASPHALT DRIVEWAY ACCESS OPPOSITE THE EASTERN SIDE BOUNDARY OF NUMBER 34 DOLLERY COURT (SHOWN ON DWG 23/12 H1(A). APPLICATION TO TASWATER WILL BE MADE TO UPGRADE THIS CONNECTION TO THE SIZE NOMINATED TO SERVE THE PROPOSED DEVELOPMENT. ALL WORK ASSOCIATED WITH UPGRADING THE COLD WATER CONNECTION WILL BE UNDERTAKEN BY TASWATER AT THE DEVELOPERS COST.

Submission to Planning Authority Notice

Council Planning Permit No.	DA 2022 / 00043		Council notice date	3/03/2022
TasWater details				
TasWater Reference No.	TWDA 2022/00263-BTN		Date of response	21/02/2023
TasWater Contact	Anthony Cengia	Phone No.	0474 933 293	
Response issued to				
Council name	BRIGHTON COUNCIL			
Contact details	development@brighton.tas.gov.au			
Development details				
Address	23 MENIN DRIVE, BRIGHTON		Property ID (PID)	3354429
Description of development	Mixed Use Development (Child Care & Community Hall)			
Schedule of drawings/documents				
Prepared by	Drawing/document No.	Revision No.	Date of Issue	
R.E.C Hydraulic Design Consultant	23/12 Sheets H1 to H6	A	2/23	
Conditions				
<p>SUBMISSION TO PLANNING AUTHORITY NOTICE OF PLANNING APPLICATION REFERRAL>></p> <p>Pursuant to the <i>Water and Sewerage Industry Act 2008</i> (TAS) Section 56P(1) TasWater imposes the following conditions on the permit for this application:</p> <p>CONNECTIONS, METERING & BACKFLOW</p> <ol style="list-style-type: none"> A suitably sized sewerage system and connection to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit. <ol style="list-style-type: none"> The sewerage connection must be a minimum DN150mm supply, not as indicated on R.E.C Hydraulic Design Consultant Design Sheet H6 A suitably sized water supply with metered connection to each lot of the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit. <ol style="list-style-type: none"> The water supply to the development must come from the water main located in MENIN DRIVE, not as shown on R.E.C Hydraulic Design Consultant Design Sheet H2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost. Prior to commencing construction/use of the development, any water connection utilised for construction/the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater. <p>TRADE WASTE</p> <ol style="list-style-type: none"> Prior to the commencement of operation the developer/property owner must obtain Consent to discharge Trade Waste from TasWater. The developer must install appropriately sized and suitable pre-treatment devices prior to gaining Consent to discharge. 				

7. The Developer/property owner must comply with all TasWater conditions prescribed in the Trade Waste Consent.

INFRASTRUCTURE WORKS

8. The developer must take all precautions to protect existing TasWater infrastructure. Any damage caused to existing TasWater infrastructure during the construction period must be promptly reported to TasWater and repaired by TasWater at the developer's cost.
9. Ground levels over the TasWater assets and/or easements must not be altered without the written approval of TasWater.

DEVELOPMENT ASSESSMENT FEES

10. The applicant or landowner as the case may be, must pay a development assessment fee of \$723.84 to TasWater, as approved by the Economic Regulator and the fees will be indexed, until the date paid to TasWater.

The payment is required within 30 days of the issue of an invoice by TasWater.

Advice

Trade Waste

Prior to any Building and/or Plumbing work being undertaken, the applicant will need to make an application to TasWater for a Certificate for Certifiable Work (Building and/or Plumbing). The Certificate for Certifiable Work (Building and/or Plumbing) must accompany all documentation submitted to Council. Documentation must include a floor and site plan with:

Location of all pre-treatment devices i.e. grease arrestor;

Schematic drawings and specification (including the size and type) of any proposed pre-treatment device and drainage design; and

Location of an accessible sampling point in accordance with the TasWater Trade Waste Flow Meter and Sampling Specifications for sampling discharge.

Details of the proposed use of the premises, including the types of food that will be prepared and served; and

The estimated number of patrons and/or meals on a daily basis.

At the time of submitting the Certificate for Certifiable Work (Building and/or Plumbing) a Trade Waste Application form is also required.

If the nature of the business changes or the business is sold, TasWater is required to be informed in order to review the pre-treatment assessment.

The application forms are available at <http://www.taswater.com.au/Customers/Liquid-Trade-waste/Commercial.au>

General

For information on TasWater development standards, please visit <https://www.taswater.com.au/building-and-development/technical-standards>

For application forms please visit <https://www.taswater.com.au/building-and-development/development-application-form>

Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

- (a) A permit is required to work within TasWater's easements or in the vicinity of its infrastructure.

Further information can be obtained from TasWater.

- (b) TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit www.taswater.com.au/Development/Service-location for a list of companies.
- (c) Sewer drainage plans or Inspection Openings (IO) for residential properties are available from your local council.

Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

TasWater Contact Details

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

SEARCH OF TORRENS TITLE

VOLUME 169004	FOLIO 1
EDITION 1	DATE OF ISSUE 26-Feb-2015

SEARCH DATE : 06-Apr-2023

SEARCH TIME : 10.34 AM

DESCRIPTION OF LAND

Town of BRIGHTON

Lot 1 on Sealed Plan 169004

Derivation : Part of 241 Acres Gtd. to Thomas Salmon

Prior CT 168256/200

SCHEDULE 1

M445321 TRANSFER to BRIGHTON COUNCIL Registered 26-Feb-2015
 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

SP169004 EASEMENTS in Schedule of Easements

SP160021, SP162542, SP166272 & SP168256 FENCING COVENANT in
Schedule of EasementsUNREGISTERED DEALINGS AND NOTATIONS

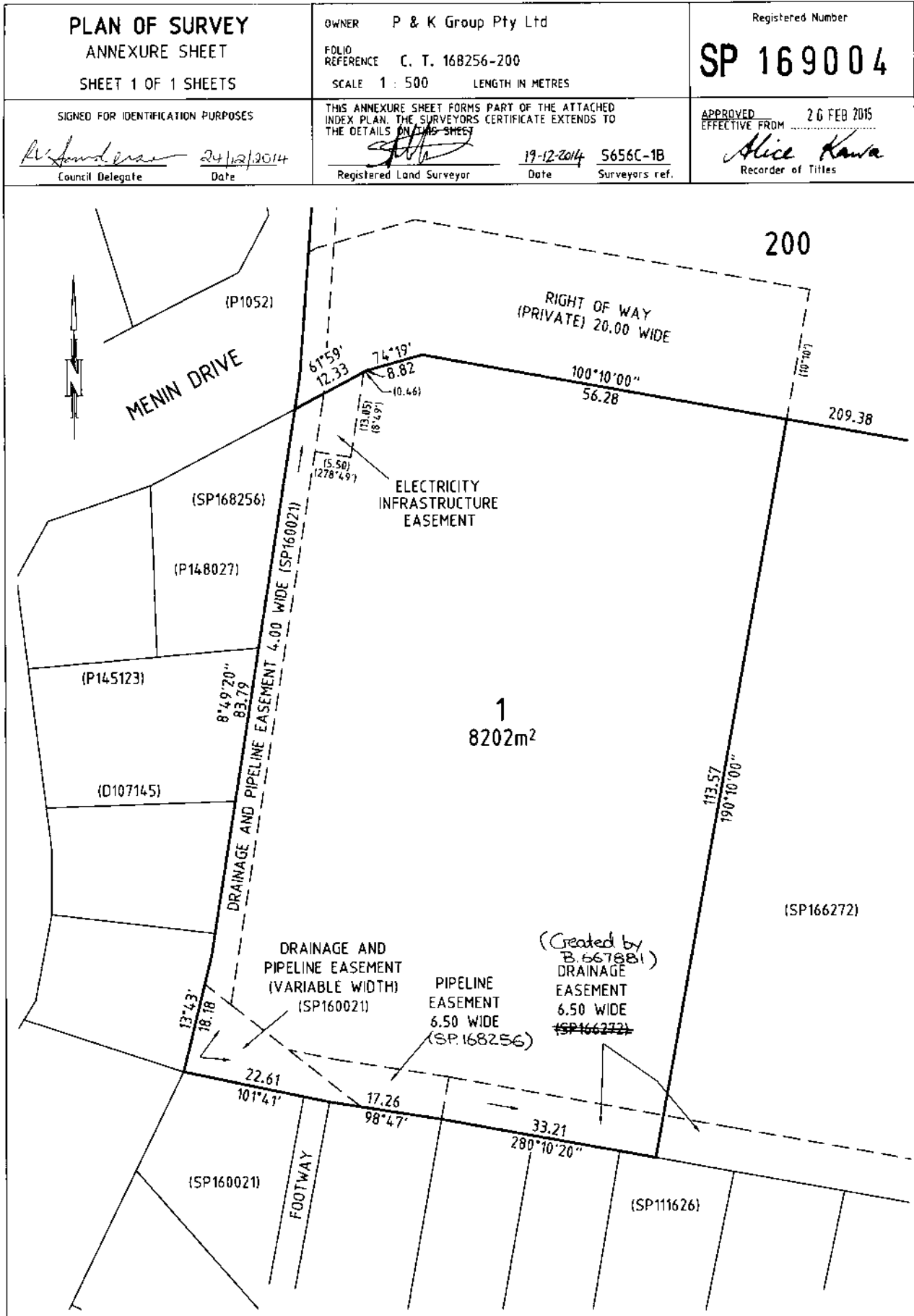
No unregistered dealings or other notations

OWNER P & K Group Pty Ltd FOLIO REFERENCE C. T. 168256-200 GRANTEE Part of 241 acres granted to Thomas Salmon		PLAN OF SURVEY BY SURVEYOR S. Roberts of PDA SURVEYORS 127 BATHURST STREET, HOBART LOCATION Town of BRIGHTON		REGISTERED NUMBER SP169004 APPROVED 26 FEB 2015 EFFECTIVE FROM <i>Alice Kawa</i> Recorder of Titles	
SCALE: 1:4000		LENGTHS IN METRES		SURVEYORS REF: S656C-1B	
MAPSHEET MUNICIPAL CODE No. 102 (5227-41)		LAST UPI No		LAST PLAN No. SP168256	
ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN					

LOT 200 IS COMPILED FROM C. T. 168256-200 & THIS SURVEY.

INDEX PLAN

S. Roberts 24/12/2016
 COUNCIL DELEGATE DATE



SCHEDULE OF EASEMENTS

Registered Number

NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS
& MORTGAGEES OF THE LAND AFFECTED.
SIGNATURES MUST BE ATTESTED.

SP 169004

PAGE 1 OF 4 PAGES

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

BENEFITTING EASEMENT

Lot 1 on the Plan is together with a Right of Carriageway over the area marked "Right of Way (Private) 20.00 Wide" on the Plan.

Lot 200 on the Plan is together with a Drainage Easement over the area marked "Drainage easement 4.57 wide (D52179) created by B.989005" on the Plan.

& shown on the Plan

Lot 200 on the Plan is together with an easement for drainage and sewerage over the Drainage Easement 4.57 wide marked "KL" on Sealed Plan 166272 with a right at all times to enter upon the said land for the purpose of constructing, repairing and cleaning the drain constructed for sewerage purposes from the said land as set forth in Certificate of Title Volume 444 Folio 18.

Lot 200 on the Plan is together with a Drainage Easement over:

- (a) the area marked "Drainage Easement 6.50 wide created by B667881" ~~on Sealed Plan 166272, and so much as passes through Lot 1 on Sealed Plan 166272~~
- ~~(b) the area marked "Drainage Easement 6.50 Wide (SP166272)" on the Plan.~~

BURDENING EASEMENTSEasement

Lot 1 on the Plan is subject to a Drainage and Pipeline Easement in favour of Tasmanian Water and Sewerage Corporation Pty Ltd (ACN 162 220 653) ("TasWater") over the area marked "Drainage and Pipeline Easement 4.00 Wide (SP160021)" so much as passes through Lot 1 on the Plan.

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: P & K GROUP PTY LTD
FOLIO REF: CT 166272/200
SOLICITOR
& REFERENCE: Abetz Curtis - 100237

PLAN SEALED BY: Brighton Council

DATE: 24/12/2014

SUB 2011/55

REF NO.

Council Delegate

NOTE: The Council Delegate must sign the Certificate for the purposes of identification.

**ANNEXURE TO
SCHEDULE OF EASEMENTS**

PAGE 2 OF 4 PAGES

Registered Number

SP 169004

SUBDIVIDER: P & K GROUP PTY LTD

FOLIO REFERENCE: CERTIFICATE OF TITLE VOLUME 166272 FOLIO 200 OF THE REGISTER

PAGE 2 OF 4 PAGES

Easement

Lot 1 on the Plan is subject to a Drainage^a and Pipeline Easement in favour of TasWater over the area marked "Drainage and Pipeline Easement (Variable Width) (SP160021)" so much as passes through Lot 1 on the Plan.

& Drainage and Pipeline Easement 4.00 wide (SP160021)

Lot 1 on the Plan is subject to a Drainage Easement in favour of TasWater over the area marked "Drainage Easement 6.50 Wide (SP166256)" so much as it passes through Lot 1 on the Plan.

Created by B667881

Lot 1 on the Plan is subject to a Pipeline Easement in favour of TasWater over the area marked "Pipeline Easement 6.50 Wide" on the Plan.

Lot 1 on the Plan is subject to an Electricity Infrastructure Easement in favour of TasNetworks over the area marked "Electricity Infrastructure Easement" on the Plan.

(appurtenant to Lot 1)

(Private)

Lot 200 is subject to a Right of Carriageway^a over the area marked "Right of Way 20.00 Wide" so much as it passes through Lot 200 on the Plan.

Lot 200 on the Plan is subject to a Drainage Easement in favour of TasWater and the Brighton Council over the area marked "Drainage Easement 2.50 Wide (SP168256)" so much as it passes through Lot 200 on the Plan.

Easement

Lot 200 on the Plan is subject to a Drainage^a and Pipeline Easement in favour of TasWater and the Brighton Council over the area marked "Drainage and Pipeline Easement 3.50 Wide (SP168256)" so much as passes through Lot 200 on the Plan.

Easement

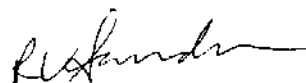
Lot 200 on the Plan is subject to Drainage^a and Pipeline Easements in favour of TasWater over:

~~(a) the area marked "Drainage & Pipeline Easement (Variable Width) (SP160021)" so much as it passes through Lot 200 on the Plan; and~~

(b) the area marked "Drainage & Pipeline Easement 4.00 Wide (SP160021)" so much as it passes through Lot 200 on the Plan.

Lot 200 on the Plan is subject to a Pump Station Easement in favour of TasWater over the area marked "Pump Station Easement (SP160021)" on the Plan.

NOTE: Every annexed sheet must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 3 OF 4 PAGES	Registered Number SP 169004
SUBDIVIDER: P & K GROUP PTY LTD FOLIO REFERENCE: CERTIFICATE OF TITLE VOLUME 166272 FOLIO 200 OF THE REGISTER	

PAGE 3 OF 4 PAGES

DEFINITIONS

'Drainage Easement'	has the meaning given to the words " <i>Right of drainage</i> " in Schedule 8 of the <i>Conveyancing and Law of Property Act 1884</i> (Tas).
'Pipeline Easement'	means (whether used alone in conjunction with other words) the full right and liberty to place an underground water pipeline on the land and to use (including inspection, cleaning, repairing, maintaining, removing and enlarging) such pipes, and valves and related equipment for carrying water and/or sewerage, and for those purposes to enter onto the land and to do any necessary works, causing as little damage as practical and making reasonable compensation for all damage done.
'Pump Station Easement'	means the full right and liberty to erect on the area of the easement such infrastructure and facilities and equipment either above or below the ground as may be desired by the dominant tenement/easement holder for the purpose of assisting or facilitating the pumping of water and/or sewerage from within or outside the area of the easement for running water services or removal of effluent to and from the lots in the plan or other property which may be serviced by the facilities. Ancillary to this right is the right to enter the area of the easement with employees and agents, materials and equipment for the purposes of erecting, installing, maintaining, renewing or removing such facilities, the person exercising such right causing as little damage and inconvenience as reasonably practicable in so doing and making good immediately any damage caused to the servient land.
'Right of Carriageway'	has the meaning given to the words " <i>Right of Carriageway</i> " in Schedule 8 of the <i>Conveyancing and Law of Property Act 1884</i> (Tas).
'Electricity Infrastructure Easement'	means the full right and liberty install electricity infrastructure (including inspection, cleaning, repairing, maintaining, removing and enlarging) such infrastructure, and for those purposes to enter onto the land and to do any necessary works, causing as little damage as practical and making reasonable compensation for all damage done.



NOTE: Every annexed sheet must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

**ANNEXURE TO
SCHEDULE OF EASEMENTS**

PAGE 4 OF 4 PAGES

Registered Number

SP 169004

SUBDIVIDER: P & K GROUP PTY LTD

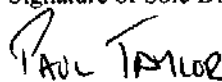
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PAGE 4 OF 4 PAGES

EXECUTED by P & K GROUP PTY LTD
(ACN 101 679 225) as the registered proprietor
of Certificate of Title Volume 166272 Folio 200
of the Register in accordance with section 127
of the *Corporations Act* 2001 (Cth) as follows



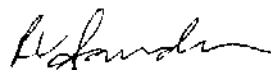
Signature of Sole Director



Name of Sole Director

~~**EXECUTED by TASMANIAN PERPETUAL
TRUSTEES LIMITED (ACN 009 475 629)**~~
~~as Mortgagee of the land in Certificate of Title~~
~~Volume 162542 Folio 200 of the Register pursuant~~
~~to Mortgage number C956144~~

NOTE: Every annexed sheet must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



DESIGN PRINCIPLES

- Maximise passive surveillance by fronting new residential lots onto open space
- Prioritise location of smallest lots adjacent to open space to provide convenient, usable outdoor space for smaller dwellings
- Provide shared path links through new subdivision areas linking residential, to schools, medical, retail and town centre to promote healthier living through exercise
- New areas of subdivision to align with existing cadastral boundaries to facilitate a staged approach to development



DRAWING KEY

- | | |
|---|---|
| Residential Lots: approx 400 - 650sq.m
<i>Opportunity for lots adjoining or adjacent open space and shared pathway link to be 300sq.m</i> | Greenfield Site: No lots shown to allow flexibility for subdivision design. Retain road connection |
| Retail - Potential for large format retail with off street parking and landscape buffer | Proposed Shared Path Links |
| Local Business - Potential site with strong pedestrian connections: 3800sq.m | Potential pedestrian crossing location |
| | Potential bus stop locations |

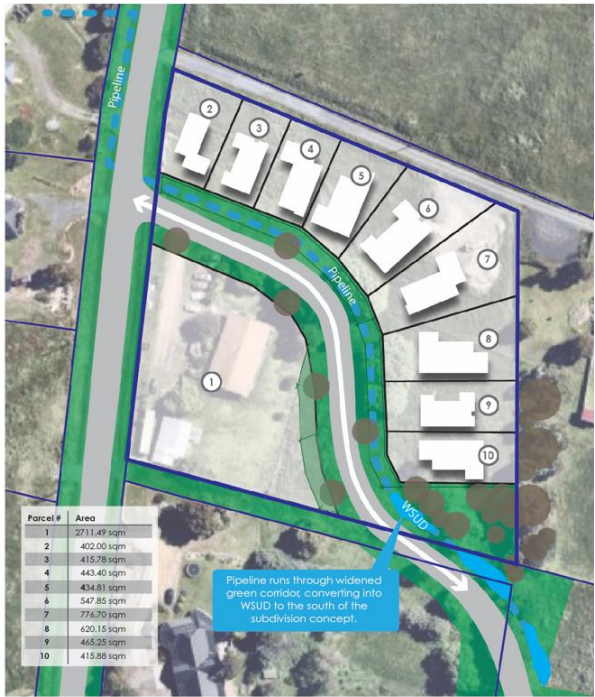
- | | |
|--|---------------------------|
| | Existing Residential Lots |
| | Proposed Residential Lots |
| | Proposed Open Space |

- Potential location of sewer pumping station- requires 30m buffer to residential and to be located outside flood zone
- Final dam layout and capacity is subject to detailed design and risk assessment
- Final road layout in this area is subject to detailed design and risk assessment
- Existing title boundaries shown in red. Potential to reconfigure titles and infill with new residential
- Connection to Jordan River Trail

Attachment B – Proposed alternative layout for 5 Dylan St

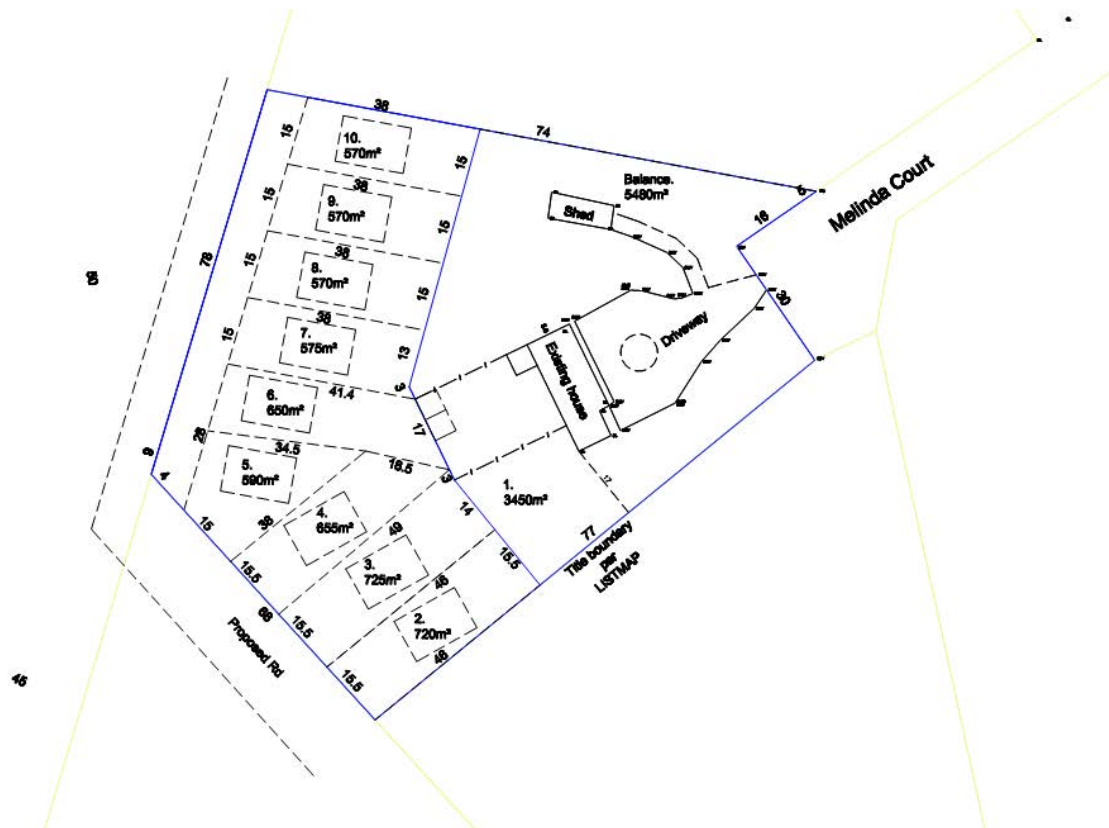


5 Dylan Street, Brighton
Subdivision by GHD Planners



5 Dylan Street, Brighton
Subdivision by GCA

Attachment C – Proposed alternative layout for 9 Melinda Ct





Deputy Premier
Treasurer
Minister for Infrastructure and Transport
Minister for Planning

Level 10, Executive Building, 15 Murray Street, Hobart
Public Buildings, 53 St John Street, Launceston
GPO Box 123, Hobart TAS 7001
Phone: (03) 6165 7754; Email: Michael.Ferguson@dpactas.gov.au

Councillor Leigh Gray
Mayor
Brighton Council
admin@brighton.tas.gov.au

Dear Mayor

**Southern Tasmania Regional Land Use Strategy -
Draft amendment to the Regional Settlement and Residential Development Strategy
(Map 10 and SRD 2.12)**

I refer to the attached documents that form a draft amendment to the Southern Tasmania Regional Land Use Strategy (STRLUS), including background to the draft amendment.

Tasmania is currently experiencing unprecedented pressure for housing. Settlement growth in greater Hobart is managed through the application of the Urban Growth Boundary under the STRLUS Settlement and Residential Development Strategy (settlement strategy).

Significant work has been undertaken in relation to the management of residential growth as part of the Greater Hobart Plan. A foundational outcome of the GHP is to provide for a short to medium term update to the STRLUS UGB where necessary to support future growth objectives.

Early work associated with the GHP identified a number of anomalies and errors associated with the mapped UGB, and some small to moderate sized parcels adjoining UGB that are suitable for urban rezoning without further justification in relation to their impact on the intent of the UGB. The draft amendment to the mapped UGB area captures these issues, as well as the removal of the UGB over the entire Tranmere and Rokeby peninsula to provide for a consistent approach to the application of the UGB across the Clarence municipality.

The second component involves a text change to the settlement strategy's policy SRD 2.12 to allow is to allow a more merit-based approach to planning decisions relating to consideration of land outside, but adjacent to the UGB than what is currently provided under that policy.

As you will be aware, the State Government has committed substantial funds to the comprehensive review of the three regional land use strategies over the next few years. The work forms part of the Phase 2 planning reforms currently underway by the State Planning Office. The Phase 2 work program also anticipates minor updates to the regional land use strategies, as and when required, to address immediate growth pressures prior to the comprehensive review of the RLUSs which will commence after the Tasmanian Planning Policies are made, anticipated for later this year.

In accordance with section 5A(4) of the *Land Use Planning and Approvals Act 1993*, I am required to consult with all councils in the Southern region, State Agencies and the Tasmanian Planning Commission on the proposed amendment to the STRLUS. If you wish to provide any comment, please make a submission by email to yoursay.planning@dpac.tas.gov.au by close of business on Monday 13 June 2022.

If you have any queries on the proposed amendment to the STRLUS, or the broader reforms relating to regional land use strategies, please contact the State Planning Office on 1300 703 977 or by email at stateplanning@dpac.tas.gov.au.

Yours sincerely

A handwritten signature in blue ink that reads "Michael Ferguson".

Michael Ferguson MP

Deputy Premier

Minister for Planning

Attachments:

1. Draft amendment to the STRLUS UGB – identified sites
2. Draft amendment to the STRLUS settlement strategy SRD 2.12
3. Discussion Paper

Discussion Paper

Amendment to the Southern Tasmania Regional Land Use Strategy (STRLUS) Urban Growth Boundary for Greater Hobart



Author:
State Planning Office

Publisher:
Department of Premier and Cabinet

Date:
March 2023

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I Introduction

This paper provides an overview of a proposed amendment to the Urban Growth Boundary (UGB) in the Southern Tasmania Regional Land Use Strategy's (STRLUS) Settlement and Residential Development Strategy (settlement strategy). The amendment is proposed as a short-term change to address current growth pressures in greater Hobart.

The amendment involves both a change to the mapped boundary of the UGB, and a text amendment to policy SRD 2.12, which relates to the consideration of urban zoning for land adjacent to, but beyond the boundary of the UGB. The capacity to rezone land will be subject to all other requirements being met for a planning scheme amendment, including all other applicable STRLUS policies, State Policies and other requirements of the *Land Use Planning and Approvals Act* (LUPA Act).

Significant work is underway to address the management of residential growth in greater Hobart through the preparation of the Greater Hobart Plan (GHP) and its anticipated Settlement Strategy, and through sub-regional residential demand and supply studies. This work will inform the review of the UGB for greater Hobart, which will be considered in more detail as part of the Phase 2 planning reforms.

The Phase 2 planning reforms include the making of the Tasmanian Planning Policies (TPPs), the regional planning framework project and the comprehensive review of the each of the three regional land use strategies (RLUS). The reforms also anticipate short-term updates to the current RLUS, as and when required, to address immediate growth pressures prior to the review work being complete, which is anticipated for 2024.

I.1 Background

Settlement growth in greater Hobart is managed through the application of the UGB provided on Map 10 of the STRLUS and the associated regional policies under SRD 2 of the STRLUS's settlement strategy. The settlement management policies under SRD 2 address issues such as greenfield and infill development, dwelling density, land release staging and the requirements for growth management across the municipalities that contain the UGB.

Regional Policy SRD 2.12 formed an amendment to the STRLUS in 2021. It comprised part of the Stage 2 planning reform agenda and sought to enable a more efficient approach for managing anomalies on the UGB boundary without having to adjust the mapped boundary of the UGB. At the time, the introduction of SRD 2.12 considered the impending work to be undertaken as part of the GHP (formerly MetroPlan).

SRD 2.12 effectively allows parcels beyond, but adjacent to, the UGB to be considered for urban rezoning, where they are not in excess of an area of 2ha, and where they meet the remaining criteria specified in SRD 2.12. As with all planning scheme amendments, rezoning proposals under SRD 2.12 must also be considered in the broader context of the STRLUS policies, along with other considerations such as the State Policies and the objectives of Schedule 1 of the LUPA Act.



1.1.1 Greater Hobart Plan Objectives and Implementation

The GHP sets out broad strategic directions for the growth and development of greater Hobart over the next 30 years. It has recently been endorsed by the Greater Hobart Committee and a draft Implementation Plan is currently being prepared.

A foundational outcome of the GHP is to provide for a short to medium term update to the STRLUS, including any amendments to the UGB which may be necessary to support future growth objectives.

To achieve the above, a Settlement Plan for greater Hobart consistent with the objectives of the Greater Hobart Plan, is being prepared as part of the GHP implementation.

The Settlement Plan will provide the spatial description of where growth should occur (in accordance with the objectives of the GHP and agreed infrastructure and service capacity), and when that growth is likely to occur across each of the municipalities. This in turn will provide the strategic justification and detailed spatial information necessary to support a range of expected medium term updates to the UGB.

1.1.2 Approach to Urban Growth Boundary Amendments

A number of anomalies and errors associated with the mapped UGB were identified during the GHP preparation process, including locations where the boundary is inconsistent with the underlying cadastre, and where the UGB is inconsistent with the urban extent of greater Hobart.

The GHP work also identified a number of small to moderate sized parcels adjoining the current UGB that do not necessarily require further justification in relation impact on the intent of the UGB or the broader STRLUS settlement strategy.

In order to address immediate growth pressures and any constraints to housing supply, a staged approach to managing short to medium term amendments to the UGB in response to the outcomes of the GHP is considered appropriate.

In responding to the current situation, it is important to achieve the appropriate balance between enabling the timely release of residential land for growth without undermining the strategic work to be finalised through the GHP, nor the strategic intent of the STRLUS' settlement strategy.

The current amendment is intended to address the anomalies and errors, and the small to medium sized parcels, as described above, and identified through the GHP preparation process.

At a later stage, it is envisaged that the outcomes of the GHP settlement strategy will inform medium-term updates to the UGB, potentially involving larger and more strategically significant sites required to support future urban growth.

The comprehensive review of the STRLUS to be undertaken after the making of the Tasmanian Planning Policies will provide for the longer-term strategic updates to the UGB and will be informed by GHP settlement strategy regional demand and supply work, and the outcomes of the STRLUS review process.



2 STRLUS amendment to Urban Growth Boundary for Greater Hobart

The proposed draft amendment to the STRLUS consists of two components.

The first component involves mapped changes to the UGB to address the anomalies and errors identified by the GHP preparation process, as outlined in the previous section 1.1.2. It also includes parcels considered errors and anomalies as identified within Brighton and Sorell through a separate process, due to those municipalities being excluded from the GHP area.

The mapped changes also include removal of the UGB over the entire Tranmere/Rokeby peninsula to prove for a consistent approach to the application of the UGB across the Clarence municipality.

The peninsula is currently the only location within STRLUS where the UGB encircles an area, leaving a “hole” within the urban settlement pattern. Removing the UGB from the peninsula is consistent with the UGB in similar locations, including Natone Hill, Gordons Hill, Rosny Hill and the Hobart Domain, as well as the Nyrstar industrial area and the Hobart International Airport.

The second component consists of a text change to policy SRD 2.12 as follows:

Notwithstanding SRD 2.2 and SRD 2.8, and having regard to the strategic intent of the Urban Growth Boundary under SRD 2 to manage and contain growth across greater Hobart, land outside the Urban Growth Boundary shown in Map 10 may be considered for urban development if it:

- (a) shares a common boundary with land zoned for urban development within the Urban Growth Boundary;
- (b) only provides for a minor and logical extension to land for urban development and does not constitute a significant increase in land zoned for urban development in that locality;
- (c) is identified in a settlement strategy or structure plan produced or endorsed by the relevant planning authority; and
- (d) results in minimal potential for land use conflicts with adjoining uses.

The draft amendment to SRD 2.12 removes the 2.0 ha limit on parcels to be considered for urban rezoning, however, introduces a requirement for the land to be considered as part of a settlement strategy or structure plan endorsed by the relevant planning authority.

In this regard, the redrafting of SRD 2.12 allows for the Tasmanian Planning Commission to adopt a more merit-based approach decision-making when considering the rezoning of land beyond the UGB for urban purposes, rather than being constrained by the 2ha requirement.

The approach allows for greater flexibility in the design, layout and response to constraints associated with the development of larger redevelopment or greenfield sites, which provides opportunities to more efficiently release respond to growth demands.



Definitions are not provided for ‘minor’, ‘logical’ or ‘significant increase’. This is to allow for the appropriate professional judgement to be made against the relevant criteria when determining the suitability of any rezoning proposal and removes quantitative limits which are difficult to apply uniformly without arbitrary outcomes. The

Whilst the amendment allows for more flexibility under the settlement strategy to consider urban development beyond the UGB, rezoning proposals will still to must also be considered in accordance with the broader context of the STRLUS policies and other requirements of the Act for a planning scheme amendment. These will include considerations such as the protection of natural and cultural values, management of natural hazards and the provision of physical and social infrastructure, as well as the State Policies and the objectives of Schedule I of the LUPA Act.

3 Compliance with the Land Use Planning and Approvals Act 1993

Section 5A(3A) of the LUPA Act requires the Minister to only declare a regional land use strategy (including an amended strategy) if satisfied that it:

- furthers the Schedule I Objectives of the LUPA Act;
- is consistent with each State Policy; and
- is consistent with the Tasmanian Planning Policies (once made).

The current STRLUS has been declared as furthering the Schedule I Objectives of the Act and being consistent with the State Policies. The proposed amendment seeks to accommodate growth pressures whilst retailing the original intent of the STRLUS settlement strategy and its regional policies. The amendment is considered to be in accordance with the Schedule I Objectives of the LUPA Act and the State Policies.





Tasmanian
Government

Department of Premier and Cabinet
State Planning Office

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Stateplanning@dpac.tas.gov.au

www.planningreform.tas.gov.au

STRLUS UGB Updates - Stage 1: Identified Sites (March 2023)

Site: East Derwent Highway, Risdon	
Municipality	Clarence
Area	74.0 ha (approx.)
Current Zoning(s)	Local Business, Rural Living Zone B, Community Purpose
Current Use(s)	Prison Complex, Tavern, Holiday Cabins, Retirement Living, Vacant Land
Reason for Inclusion within the UGB	Logical extension. The land forms part of the existing urban footprint.



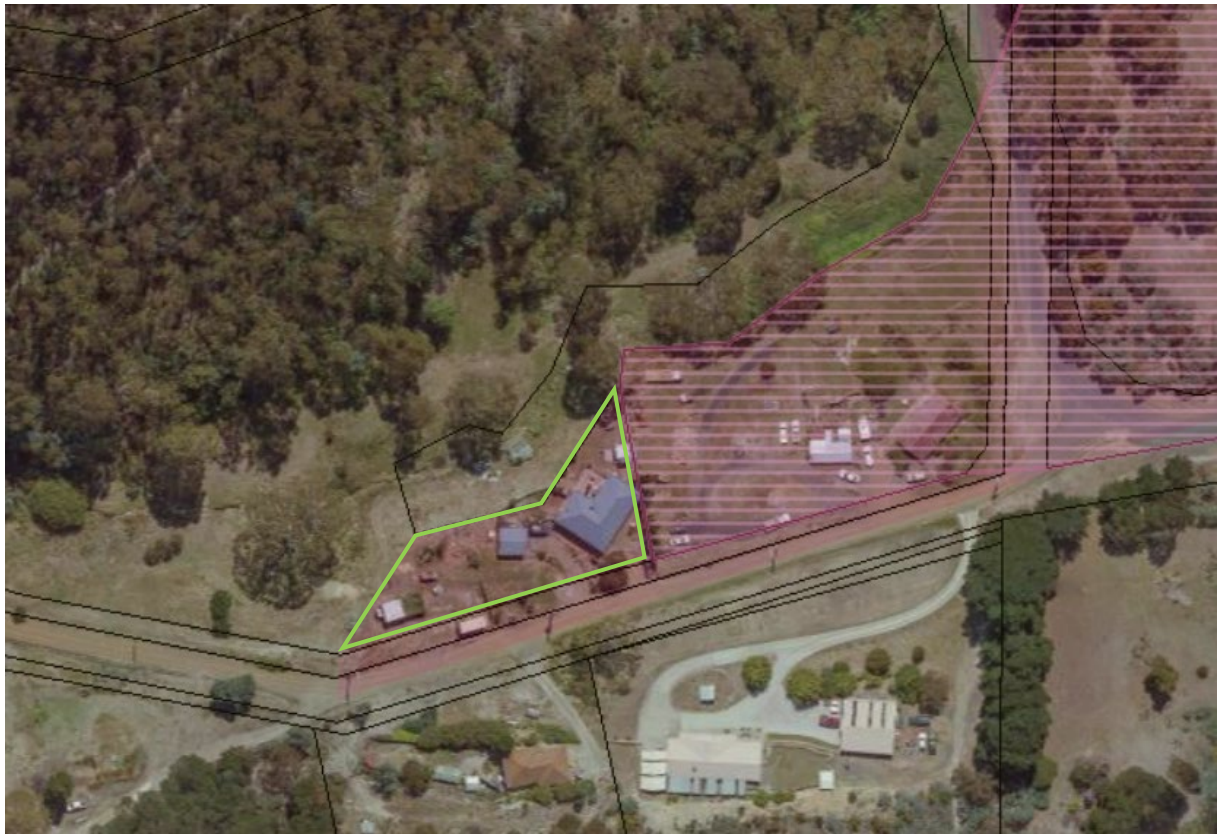
Site: Rokeby Hills – Tranmere - Droughty Point	
Municipality	Clarence
Area	300 ha (approx.)
Current Zoning(s)	Low Density Residential, Open Spcae, Landscape Conservation, Utilities
Current Use(s)	Residential, vacant, water infrastructure
Reason for Inclusion within the UGB	Anomaly. Removal of the UGB is consistant with similar locations within the municipality which are fully surrounded by urban development, namely; Natone Hill, Gordons Hill and Rosny Hill.



Site: Spring Farm Estate, Kingston	
Municipality	Kingborough
Area	8.50 ha
Current Zoning(s)	General Residential
Current Use(s)	Residential
Reason for Inclusion within the UGB	Anomaly/correction. Already zoned for residential use and fully developed.



Site: 240 Abbotsfield Road, Claremont (part)	
Municipality	Glenorchy
Area	0.16 ha
Current Zoning(s)	General Residential
Current Use(s)	House and adjoining land, portion of larger parcel zoned landscape Conservation
Reason for Inclusion within the UGB	Anomaly/correction. Already zoned and used for residential purposes.



Site: 28 Jackson Street, Glenorchy (part)	
Municipality	Glenorchy
Area	0.39 ha
Current Zoning(s)	General Residential
Current Use(s)	Vacant land, residential portion or larger title zoned Landscape Conservation
Reason for Inclusion within the UGB	Anomaly/correction. Already zoned for residential use and connected to existing road network.



Site: 73A, 73B and 73C Russell Road, Claremont	
Municipality	Glenorchy
Area	0.94 ha
Current Zoning(s)	Low Density Residential
Current Use	Residential comprising balance of existing residential properties (73B and 73C) and vacant land (73A)
Reason for Inclusion within the UGB	Anomaly/correction. Already zoned for residential use comprising balance land for existing residential properties.



Site: Main Road and Henry Streets, Sorell	
Municipality	Sorell
Area	1.9 ha
Current Zoning(s)	Light Industrial and Open Space
Current Use	Landscape and rural supplies, construction and open space
Reason for Inclusion within the UGB	Anomaly/correction. Already used for urban purposes and directly adjacent to UGB.



Site: Henry Street, Sorell	
Municipality	Sorell
Area	6.5 ha
Current Zoning(s)	Utilities, Rural and Community Purpose
Current Use	Landscape and rural supplies, construction and open space
Reason for Inclusion within the UGB	Anomaly/correction. Already used for urban purposes and adjacent to UGB.



Site: Tasman Highway, Sorell	
Municipality	Sorell
Area	1.0 ha
Current Zoning(s)	Utilities
Current Use	Former road quarry site.
Reason for Inclusion within the UGB	Anomaly. Privately owned former quarry site. Part of urban footprint.



Site: Tasman Highway, Sorell	
Municipality	Sorell
Area	6.6 ha
Current Zoning(s)	Utilities
Current Use	Road Reserve
Reason for Inclusion within the UGB	Correction. Amendment to reflect recently constructed road infrastructure.



Site: Main Road, Sorell	
Municipality	Sorell
Area	33.5 ha
Current Zoning(s)	Rural
Current Use	Poultry Farm
Reason for Inclusion within the UGB	Logical extension. A change in management practices at the Poultry Farm may allow for areas of the site to be used for industrial or light industrial purposes.



Site: Brighton Road, Brighton	
Municipality	Brighton
Area	62.0 ha
Current Zoning(s)	Industrial, Environmental Management, Rural, Utilities
Current Use	Industrial, Residential, Vacant, Roads
Reason for Inclusion within the UGB	Logical extension. Connects industrial estate with southern edge of existing urban area.



Site: Ashgrove Crescent, Old Beach	
Municipality	Brighton
Area	7.0 ha
Current Zoning(s)	General Residential, Rural Living
Current Use	Residential, vacant land
Reason for Inclusion within the UGB	Logical extension. Already zoned and developed for residential use. Located immediately adjacent to land identified for potential future expansion of the UGB (refer Brighton Structure Plan August 2018 - Site 9: Old Beach Quarry).



Attachment 2**STRLUS UGB Updates – Stage 1: Amendment to SRD 2.12**

Notwithstanding SRD 2.2 and SRD 2.8, and having regard to the strategic intent of the Urban Growth Boundary under SRD 2 to manage and contain growth across greater Hobart, land outside the Urban Growth Boundary shown in Map 10 may be considered for urban development if it:

- a) shares a common boundary with land zoned for urban development within the Urban Growth Boundary; and
- b) does not constitute a significant increase in land zoned for urban development in that locality; and
 - i. is identified in a settlement strategy or structure plan produced or endorsed by the relevant planning authority; or
 - ii. only provides for a minor and logical extension to urban development beyond the Urban Growth Boundary; and
- c) can be supplied with reticulated water, sewerage and stormwater services; and
- d) is aligned with the capacity of transport and road infrastructure and minimises impacts on the efficiency and safety of road networks; and
- e) results in minimal potential for land use conflicts with adjoining uses.